



MEETING AGENDA

The City of Beaufort

PLANNING COMMISSION

1911 Boundary Street, Beaufort, SC 29902

Phone: 843-525-7011 ~ Fax: 843-986-5606

Monday, April 20, 2026, 5:00 P.M.

City Hall, Council Chambers, 2nd Floor – 1911 Boundary Street, Beaufort, SC

Please click the link below to join the webinar:

<https://us02web.zoom.us/j/82491436416?pwd=bPVn4xtLGX9GNmczjkQYhIHh1Wc8Fy.1>

Password: 176000

Meeting ID: 824 9143 6416

Call in Phone #: 1+929-205-6099

STATEMENT OF MEDIA NOTIFICATION: "In accordance with South Carolina Code of Laws, 1976, Section 30-4-80(d), as amended, all local media were duly notified of the time, date, place and agenda of this meeting."

Please note, this meeting will be broadcast via zoom and live-streamed on YouTube. You can view the meeting at the City's page; City Beaufort SC

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- I. Call to Order
 - II. Pledge of Allegiance
 - III. Review of Commission Meeting Minutes:
 - A. February 18, 2026 Worksession Minutes
 - B. March 2, 2026 Worksession Minutes
 - C. March 16, 2026 Worksession Minutes
 - IV. Questions Relating to Military Operations
 - V. New Business:
 - A. Sketch Plan - Beaufort Free-Standing Emergency Department (FSED) – 396 Robert Smalls Parkway. The Applicant, Conor Blaney with Ward Edwards is proposing a commercial development to include a free-standing Emergency Department. The property is further identified as R120 028 000 1248 0000.

VI. Public Hearing:

B. Rezoning – 909 Church Street. On behalf of the applicant, Ms. Kelley Lamsens, the City of Beaufort is requesting approval to align the zoning designation of the property located at 909 Church Street from a split zoning of T4-HN/T4-N (T4-Historic District Neighborhood /T4-Neighborhood) to a single zoning of T4-HN (T4-Historic Neighborhood). The property is further identified as R120 004 000 0275 0000.

C. Beaufort Development Code Changes – Amendment to Chapter 4 regarding Building Design and Infill Standards of the Beaufort Development Code.

VII. Adjournment

Note: If you have special needs due to a physical challenge, please call Julie Bachety at (843) 525-7011.

Meeting Minutes Review

- A. PC Work Session – February 18, 2026**
- B. PC Work Session – March 2, 2026**
- C. PC Work Session – March 16, 2026**



Planning Commission

Work Session Minutes – February 18, 2026

CALL TO ORDER

6:45

A work session of the Planning Commission was called to order by Chairman Mike Tomy and was held in-person in the Planning Conference Room on February 18, 2026 at 1:05 PM. The meeting was recorded and streamed on Facebook in accordance with City Policy.

ATTENDEES

Members in attendance: Mike Tomy, Chairman, Bill Bardenwerper, Vice-Chairman, Libby Anderson, Clinton Hallman, Kim McFann, Benjie Morillo, and Bill Suter.

Staff in attendance: Curt Freese, Community Development Department Director, Christopher Klement, Planner III and Nicolas Navia, Planner I. Meadors Architecture, joined the meeting via Zoom.

DISCUSSION

7:00

Mr. Tomy asked if there were any general comments.

Eric Clausseen, the new Assistant City Manager, introduced himself.

Public Comment

None.

Public comment closed.

Mr. Freese stated that Adam Biery with the Architecture Firm Design Build is here to give the Planning Commission (PC) a presentation in relation to the liner buildings. Mr. Biery introduced himself and said he is working with Paul Trask to evaluate and/or look at the Code definition of liner buildings and the requirement for two stories; how to address the corner. His firm worked with Mr. Trask on the Chipotle and Five Guys Buildings.

CHAPTER 4 REVIEW

21:25

Mr. Freese stated at the last worksession the PC stopped at Section 4.5.12 – Gas/Fuel Stations and Car Washes, page 43 of 72. Mr. Tomy stated that he would like to go back to a detailed discussion after we finish the general discussion. Mr. Freese proposed to rewrite this section, adding the General Architectural Design and Lowcountry Design Elements.

PC agreed to not allow gas stations in the Historic District, referring to the gas station on the corner of Harrison Street and Boundary Street, which is grandfathered, and the gas station on the corner of Boundary Street where Subway used to be.

All were good with the vernacular standards. Mr. Freese referred to Page 46 of 72 and said the sentence *See definition of transparency below* needs to be deleted since it is referenced in another section. Mr. Tomy referred to Section 4.5.13 G.b (page 47 of 72) that says, *blank walls are prohibited along public streets* and feels it needs a definition. Mr. Freese said it is discussed on the previous page under Scale & Proportion. PC agreed to remove the Starbucks picture and the wording below the picture.

Mr. Morillo referred to a few drive-thrus that don't work (Dunkin Donut and Chick-Fil-A) and he proposed prohibiting drive-thrus within 250 feet of major intersections. Mr. Bardenwerper referred to a Stacking Study he did.

Mr. Tomy referred to Section 4.5.13.g, Drive-Thru Facilities Special Site Requirements and proposed adding the sentence, *Stacking study shall be made to substantiate the proposed stacking design*. Mr. Freese will make a note "TIA Required are the discretion of the administrator under 50 trips".

Mr. Freese stated that Jeremy Tate, staff consulting architect, put together the Drive-Thru Facilities Special Architectural Requirements. Mr. Tomy proposed under Section 4.5.13 H.b (page 47 of 72) to strike the words *strongly discouraged* and replace it with *not within the city of*. Mr. Tomy feels this needs to be reworded so that there is a stronger language in the code and that it needs to be consistent and coordinated with Lowcountry Vehicular Architecture. Mr. Freese will work on rewording this.

Mr. Freese stated there are not too many changes in Section 4.5.1.4 Civic Buildings. Mr. Tomy explained that he is an anti-terrorism force protection expert and also was a consultant for NY Yankees, Mets, NFL Stadiums, federal court buildings and does anti-terrorism protection. He said the PC should be aware of when designing a new civic or public buildings there needs to be an installation of something in the design stages relating to the exterior facilities. An example would be an old courthouse that has no room for a metal detector at the entryway. Ms. Anderson suggested this take place within the application requirements. Mr. Freese said there are no changes proposed for this section, but we can add a recommendation.

Mr. Tomy suggesting doing something simple under Civic Buildings & Institutions to say, *in consideration regarding security and anti-terrorism and force protection shall be provided for the proposal*. Mr. Tomy intends to do a presentation to the Fire Department as well as the Police Department.

PC agreed to what was done in the new Section 4.5.14 – Church and Institutional Buildings.

Mr. Freese stated that the PC has not reviewed Section 4.6: Building Design Standards yet. It needs to be clarified when it says, *district* is it referring to the historic district.

Mr. Tomy referred Section 4.6.1.H.3.a and wants to add to it the sentence *only be located on the property and on a public right-of-way*. Ms. Wilson referred to Section H.3.c which doesn't read grammatical right. PC agreed to after add the words *within public* after the words *displayed on*.

Mr. Freese stated that the Fencing in Section 4.6.1.1 and 4.6.1.2 have been moved to Chapter 5 under Landscaping.

Mr. Tomy referred to Section 4.6.3.A.1.b, and Section 4.6.3.A.5.c delete the words *smooth finish*.

Mr. Freese said in Section 4.6.3.A.9.h Faux wood grain textures will be removed.

PC agreed to put back into Section 4.6.2.c *Tabby*.

PC agreed to put back into Section 4.6.3.A.1.f, *Tabby (or stucco with oyster shell aggregate, typ.)*.

PC agreed in Section 4.6.3.A.4.d to remove *PVC*.

PC agreed in Section 4.6.3.A.7 to remove the entire paragraph.

PC agreed in Section 4.6.3.A.9.c to research the new product – do not prohibit.

PC agreed in Section 4.6.3.A.9.b to remove the words *and trim*.

PC agreed in Section 4.6.3.A.9.i to remove the entire sentence.

PC agreed in Section 4.6.3.A.9.l.iii remove entire sentence.

PC agreed in Section 4.6.3.B.ii. – staff will research to match this to other color recommendations in the Code.

Mr. Freese referred to added Section 4.6.4.C.1 – Windows/Doors. All agreed after Fenestration/Transparency to add *With the exception of residential uses*. Also, add at the end of the paragraph, *The Planning Commission may consider design exceptions to this requirement to include spandrel glass and/or higher percentage opacity in cases where the applicant can prove these are critical functional (privacy?) areas*.

PC agreed in Section 4.6.4.A.1.f. to unstricken the sentence *Tabby (or stucco with oyster shell aggregate, typ.)*

PC agreed in Section 4.6.4.A.2.c to unstricken the word *Tabby*.

Mr. Tomy suggested the PC and staff research the term *stucco* to include modern materials.

Mr. Freese went over the three big issues that were not resolved today (garage issue and multi-family). At the next meeting first the PC will go over the major things and then go over the final things. Next worksession can be March 16 in the afternoon since we don't have any agenda items for the regular PC meeting.

Ms. Anderson stated she would not be at the next meeting but will send her comments to Mr. Freese.

Mr. Tomy asked what Mr. Freese has in final or in an update to date form for the Accessory Dwelling Units (ADUs) & Short-Term Rentals (STRs). Mr. Freese said that's going to council next month. Mr. Tomy would like a copy of what was presented to council. Mr. Freese will send it to the PC.

Public Comment

Merritt Patterson spoke. Mr. Patterson stated he does commercial development and is a long-term resident within the city. He does a lot of commercial work, particularly light industrial properties (Beaufort Industrial Village, Commerce Park and the land on Depot Road). He talked about how the Code is trying to group

together light industrial and commercial types buildings as if they are residential looking. The document being reviewed does not address light industrial properties. The industrial properties should have the same rules as the Commerce Park does.

Public comment closed.

The Worksession ended at 3:21 pm.

3:21:33

DRAFT



Planning Commission

Work Session Minutes – March 2, 2026

CALL TO ORDER

0:10

A work session of the Planning Commission was called to order by Chairman Mike Tomy and was held in-person in the Planning Conference Room on March 2, 2026 at 1:00 PM. The meeting was recorded and streamed on Facebook in accordance with City Policy.

ATTENDEES

Members in attendance: Mike Tomy, Chairman, Bill Bardenwerper, Vice-Chairman, Clinton Hallman, Kim McFann, Benjie Morillo, and Bill Suter.

Member Absent: Libby Anderson

Staff in attendance: Curt Freese, Community Development Department Director, Christopher Klement, Planner III and Nicolas Navia, Planner I.

DISCUSSION – CHAPTER 4 REVIEW

0:23

Mr. Tomy asked if there were any initial comments from staff. Mr. Freese referred to his memo that was sent out with the agenda and the email from Mr. Morillo regarding the definition of architectural vernacular. Today the schedule is to go over today garages, patios homes, and affordability for affordable housing and lot diversity.

Mr. Tomy commented there is a discrepancy at the beginning part of Chapter 4 and at the end of Chapter 4 that he would like to discuss.

Public Comment

Paul Trask, 610 Bladen Street, referred to Adam Biery's presentation and hoped it was well received.

Public comment closed.

PC spoke about the Garages (single-family) issues. Mr. Bardenwerper went over his proposed revisions for the garage standards. Mr. Bardenwerper commented that Ms. Anderson felt the current garage standards should be retained, and were misapplied by previous Code Administrators.

Mr. Freese referred to Section 4.5.4.7.b (Garages). Mr. Bardenwerper suggested that the definition of the façade be clarified. Mr. Navia read the term for façade that now reads, *the vertical surface of a building*. Mr.

Freese felt Mr. Bardenwerper's draft was good and that it was a good compromise between the two sides. Mr. Freese added *the five feet and not including the porch*. The garage standards in 2.4.1 are not being done away with.

Public Comment

Paul Trask, 610 Bladen Street, said it's important to keep in mind that if the PC is talking about a large tract-development, the lots have not been configured yet. He referred to DR Horton. For example, a garage can be set back where it still is attached to the house, but its set back enough that one car can still be parked in front of the driveway without being in the garage. Eliminating the porch would be a terrible mistake, it's a critical item if the city is trying to create a walkable neighborhood. "Consider maybe wider sidewalks that encourages people to walk."

Public comment closed.

Mr. Bardenwerper referred to Section 4.5.4.B.2 as he would like his note added at the end of the newly inserted paragraph "2. Housing Model Diversity" to say: *"Building designs shall be considered similar unless they are distinguished by colors as well as some combination of uniquely designed entryways, porches, windows and roof lines, such that they plainly look different one from the other. Square footages (and massing) characteristics do not have to be different."*

Mr. Tomy suggested adding to Mr. Bardenwerper's notes *massing elements of* after the words *as well as*; also adding the word *characteristics* after the word *footages*. To summarize, the added sentence would say: "Building design shall be considered similar unless they are distinguished by colors, massing elements of some combination of uniquely designed entryways, porches, windows and roof lines, such that they plainly look different one from the other. Square footage characteristics do not have to be different."

Mr. Bardenwerper suggested also making the same changes to 4.5. 5.B.4 and 4.5.6.B.4. The PC agreed to Mr. Bardenwerper's suggestion.

PC agreed in Section B. Garages to remove items c, f, g, h, and i.

PC agreed in Section B.a Garages to add a new sentence that says: *"Garages shall be placed behind the rowhomes/townhomes. Apart from occasional on-street parking, all parking, including garages and carports, if any, in new rowhome/townhouse developments shall be located in the rear of the rowhouses/townhouses and be accessed off a rear alleyway."*

PC agreed in Section B.e to add a sentence that says: *"Landscaping or appropriate buffering may be required by the Administrator or Planning Commission to accomplish this requirement."*

By referring to Section 4.5.7 Apartment House, Mr. Freese mentioned that he had previously met with the Housing Trust and discussed options for affordable housing. Recommendations included the following:

- 1) Reducing design standards for qualified workforce or affordable housing.
- 2) Making design standards administrative, instead of discretionary.
- 3) Considering a streamline approach, such as administrative approvals or a more expedient process for affordable/workforce housing.

Mr. Bardenwerper referred to his submitted notes. He also referred to affordable housing submittals. He stated that such specific applicants would present their project(s) as being “affordable” housing but when the tax credits are not received those housing projects would actually not be “affordable” after all.

Public Comment

Paul Trask, 610 Bladen Street, said he’s not sure about the tax credits side but he is familiar with the financing side. When an applicant has a construction loan and reaches a certain percentage of filling it up, they then can roll it into a HUD backed financing model. And the more affordable units that are provided in the development, the lower the HUD rate is going to be fixed for 30 or 40 years. He agreed with Mr. Bardenwerper as there shouldn’t be a compromise. He stated there should be more focus on accessory dwellings.

Public comment closed.

PC took a five-minute break.

Mr. Freese referred to the three handouts (page 23 of 73, page 11 of 77) and Libby Anderson’s email. Mr. Tomy clarified that this would be discussed at the time when PC would proceed with discussing changes to Chapter 2, however, this was not the time to address it yet.

Public Comment

Paul Trask, 610 Bladen Street, felt the code has stepped on the toes of the Milner and the conservation/preservation goals of the Beaufort Historic District in some respects. As far as diversity or housing types in the Historic District, in Mr. Trask’s opinion it’s not a good idea to allow a broad cross-section of these different housing types other than single-family. He referred to the Bladen Street Redevelopment Overlay and the small townhouse project that was being proposed to be built behind the Herban Market. He also referred to the older apartments/townhomes on The Point that seemed to have blended in well over a period of decades. Also, the townhouses on King Street blended in well.

Public comment closed.

After going over the major items, PC agreed to move on to review the rest of Chapter 4. Mr. Tomy commented on the *offsets* that are present throughout the chapter, especially in comparing the very first to the last part. The offset in the first part has been corrected to 18 inches whereas the offsets in the last part of the chapter still are at 24 inches. He prefers 24 inches.

PC agreed in Section 4.3.1.A to change the word *village* to *city/town*.

PC agreed in Section 4.3.1.a.c to add “*respects the historical context and/or*” after “Encourage architecture that”, and before “blends harmoniously with the natural surroundings and neighboring development”.

PC agreed in Section 4.3.1.C to change the title from *Findings* to *Lowcountry Vernacular*.

Mr. Morillo went over his email that discussed the definition of Lowcountry Vernacular in Section 4.3.1.C and suggested adding his information to this section. He said the language highlighted in red was what already existed and the words highlighted in blue represented his suggested changes/additions. The PC agreed.

PC agreed in Section 4.3.2.J.d to add a new paragraph to say, *maximizing air flow through single room wide floor plans*.

Mr. Bardenwerper referred to Section 4.4.A Common Yard and asked if the word *common* is the correct term. PC agreed to change to *Front*.

PC asked staff to go over and come up with better sketches in the tables in Section 4.4 and 4.5

PC agreed in Section 4.5.H – Large Footprint Building, in the chart, to change the footprint size notation back to “greater than 20,000 square feet” for districts T4, T5, RMX, IC, LI.

PC agreed in Section 4.5.J – Gas/Fuel Station, in the chart, to add “& Car Wash” to the rubric title as well as add the specific description “Car washes feature pull-through lanes in which customers may or may not exit their vehicles.”

PC agreed to add to Section 4.5 (Chart): **“K. Drive-Thru Facilities: This is a commercial building type that includes a vehicular lane along a portion of the building to serve the public through a drive-thru window or other area in which customers do not leave their vehicles.”** Applicable to districts T5 and RMX. The addition of a specific Sketch and Plan to be complemented by staff.

PC agreed to add/amend prior addition to Section 4.5 (Chart): **“L. Civic Buildings: This is a civic, institutional or religious building in which program, massing, and height vary. For religious buildings such as churches, heights and frontages are to be determined by the Code Administrator.”** Applicable to districts T3, T3-N, T4, T4-N, T4-HN, T5-UC, T5-DC, RMX, IC.

PC agreed to strike paragraph **“C. Local Design Quality. (...)”** from Section 4.5.1 General.

PC agreed in Section 4.5.3.B – Special Requirements to amend number 2 – Number allowed from “2” to “1” per lot, and to add “an additional ADU may be allowed as a Special Exception (see Section 9.13).” (remove: “except in T3-S where 1 per lot is permitted.”)

PC agreed in Section 4.5.4.B.2 – to include language pertaining to requiring some sort of **“Housing Model/Lot Diversity”**. Final language to be discussed at next PC work-session.

PC agreed in Section 4.5.4.B.3 and B.4 to insert new language pertaining to “General Architectural Design” and “Lowcountry Design Elements”.

PC agreed in Section 4.5.4.B to strike “5. Four Sided Architecture”, but instead and further to insert more detailed requirements regarding “5. Elevation”, “6. Corner Lots”, “7. Garages” (hereby taking over Mr. Bardenwerper’s drafted language without including front porches, and to include a definition of “façade” at the end (excluding a front porch). Lastly, “8. Front Yard Landscaping” concludes the list of extended Special Requirements (all or some to be inserted into Sections 4.5.5 – 2-3 Unit House, Section 4.5.6 – Rowhome/Townhouse, and Section 4.5.7 – Apartment House).

PC agreed to remove the terms *stoop* and *forecourt* from Section 4.5.7.B.1.

PC agreed to remove the terms *arcade* and *forecourt* from 4.5.8.B.1.

PC agreed in Section 4.5.8.B.12 - Wall Articulation to add “*at 24 inches*” after the words “*stepping of the wall plane*” and then to delete the words “*and/or landscaping accomplishing the same effect*”;

PC agreed in Section 4.5.9.B.4 (Liner Buildings) to remove this sentence: “Required in RMX: Attached or detached liner building shall be required in the RMX zoning district”.

Moreover, PC agreed to more detailed “Special Requirements” in Section 4.5.9.B (Liner Buildings), to include “Parking”, “General Architectural Design”, “Lowcountry Design Elements”, “Depth” as well as “Liner Building Special Architectural Requirements”.

PC agreed in Section 4.5.10 – Large Footprint Building to amend and specify in more detail “B. Special Requirements” as including “1. The allowed frontage types”, “2. Parking Location”, “3. Liner Buildings”, “4. Height”, “5. Size”, 6. Entrances”, “7. Location”, “8. General Architectural Design”, “9. Lowcountry Design Elements”, and “10. Large Footprint Buildings Special Architectural Requirements”.

PC agreed in Section 4.5.10.B.8 – General Architectural Design, replace the word *quality* with *Lowcountry Vernacular*.

PC agreed in Section 4.5.9.B.10.a to add after the word *plane*, *at 24 inches*.

PC agreed in Section 4.5.9.B.10.f to add a note after 12’, (*consider different depth*).

PC agreed in Section 4.5.9.B.10.h to remove this paragraph.

PC agreed in Section 4.5.9.B.14 to add a note as item #14, *to allow for side entry on building facing courtyard or similar layouts*.

PC agreed in Section 4.5.10.B.5 – Size, to remove the first sentence.

PC agreed in Section 4.5.12 to delete the word *landscape features* after the word *building*.

PC agreed in Section 4.6.3 to amend the title to “**Specific to All Districts (SEE 4.2.2)**”

PC agreed in Section 4.6.3 – **A. Building Materials and Details** to add language as to exempt “Light Industrial” from these requirements.

Public Comment

Paul Trask, 610 Bladen Street, asked if the two story is still a requirement in significant intersections? This is a problematic issue and has not worked in Beaufort yet. Mr. Trask said, “under two stories the façade facing problem should be activated for pedestrians to include but not limited to an activated plaza 24 feet in depth. He also talked about the location of a patio and entrance ways. He proposed that the city maintain a 75%

frontage buildout but the city might permit a 60% or a 65% frontage buildout if its accompanied by a side patio or a side plaza which would accomplish the same goal. Mr. Trask said he could get with Adam Biery and get a draft of a red line on this and submit it to back to the PC. Mr. Tomy said this would be good and should be submitted to Mr. Freese.

Public comment closed.

PC agreed in Section 4.5.11.C to add a note, *Include Section for Lowcountry Vernacular.*

PC agreed in Section 4.5.11.C.2 add after the words *such as, brick, cast stone.*

The next worksession will be March 16, which is the PC's regular schedule meeting, but there are no agenda items. The worksession will start at 2:00 pm. Next meeting PC would like a clean version and a red lined version.

The Worksession ended at 3:20 pm.

3:25:40

DRAFT



Planning Commission

Work Session Minutes – March 16, 2026

CALL TO ORDER

5:23

A work session of the Planning Commission was called to order by Chairman Mike Tomy and was held in-person in the Planning Conference Room on March 16, 2026 at 2:05 PM. The meeting was recorded and streamed on Facebook in accordance with City Policy.

ATTENDEES

Members in attendance: Mike Tomy, Chairman, Bill Bardenwerper, Vice-Chairman, Libby Anderson, Clinton Hallman, Kim McFann, Benjie Morillo.

Member late: Bill Suter.

Staff in attendance: Curt Freese, Community Development Department Director, Christopher Klement, Planner III, and Nick Navia, Planner I.

DISCUSSION – CHAPTER 2, 3, AND 4 REVIEW

5:55

Public Comment

Paul Trask, 610 Bladen Street, referred to the suggestions he sent regarding Section 2.6.3 (Two-Story Height Requirements) and Section 2.5.1 (Building Placement and Orientation). He said, “the change to frontage build out with patios were not included the draft”. He feels it would be a mistake if there is some provision adjustment in the case of the side; don’t give that flexibility to the developer. Ms. McFann asked Mr. Trask where this information would fit best into the Code. He said, “on page 4.5.9”. Mr. Hallman said the change that Mr. Trask referred to was made in Section 4.5.9.8.2.

Public comment closed.

Mr. Freese started the discussion and went over Chapter 2, Section 2.5.1A.1.d. Alternate Entrances, Section 2.6.3 Two Story Height Requirements, and Section 2.5.9 Lot Diversity and the changes that were made from the last worksession and stated the blue and red colors are new information.

Mr. Freese stated Section 2.6.3.C is new information.

Mr. Tomy referred to Section 2.6.3.A to change word *should* to *shall* in the sentence that says *Buildings*

located near Significant Intersections should that above item #1. The PC agreed to not to change the word should.

Mr. Tomy referred to Section 2.6.3.A.3 and suggested changing the word *Active* to *Activate*.

It was also agreed by the PC in Section 2.6.3.A, under item #4, to *remove* the first paragraph.

Ms. McFann also suggested *putting 2.6.3.B before* the sentence, *Buildings located near Significant Intersections should:*.

Ms. McFann suggested to label the title of 2.6.3.B *Requirements*.

PC agreed in Section 2.6.3.A to change the word *Intent* with *Applicability: Specific to Zones T4-N and T5-UC:* and make the description of *Intent* as a separate paragraph.

Ms. McFann suggested adding the word *Requirements* after the word *Building* in Section 2.6.3.C and then having the sentence "*Buildings locate near Significant intersections should: be on its own line after Section 2.6.3.C.*"

All agreed to remove the words *Specific to Zones T4-N and T5-UC under the new labeled Section 2.6.3.D.*

All agreed in section 2.6.3.C – Building Requirements to *add a #5 and put the information regarding Architectural elements there.*

All agreed in section 2.6.3.C to *add the title 2.6.3.D – 2-Story Building Standards* as its own title.

All agreed to change the title of Section 2.6.3.E back to the original title for *Alternative Single-Story Building Form*.

Ms. Anderson suggesting adding to Section 4.6.1.H – Outdoor Display of Merchandise a new item as:

1. Architectural elements intended solely to simulate an additional story without providing meaningful building depth, usable space, or architectural articulation shall not be used to satisfy the two-story requirement.

Ms. McFann referred to Section 2.6.3.E.7 – Architectural Massing #7 (page 2 of 2) and suggested *removing* the word *steeply*.

Ms. McFann referred to Section 2.6.3.E – Alternative Single-Story Building Form and the word *forecourt* in the first paragraph *should be removed*. Mr. Morillo felt it's okay for the word *forecourt* to be left in this section.

Chapter 4:

Mr. Freese referred to the handout for Section 4.5.9 – Liner Buildings and went over the changes.

All agreed in Section 4.6.1.H – Prohibited Uses to add the word *maintenance* before the words *service functions*.

All agreed after the paragraph under Section 2.6.3.E – Alternative Single-Story Building Form to add the wording *See Specific to Liner Buildings: see Section 4.5.9 for additional requirements*

Section 3.6.2.C – Bed and Breakfast

Mr. Freese went over some potential changes to the Bed and Breakfast ordinance that would make it consistent with the state statute. Numerous citizens believe that the use should be allowed in T-4 (Historic District area). Now it's a Special Exception that would have to be applied and approved by the Zoning Board of Appeals (ZBOA).

Mr. Tomy commented that he heard from a legal standpoint that the City has the right to create its own regulation and the City doesn't have to comply with the State; city could override the State. The PC should be able to decide what's right for them versus the State's requirement. Beaufort has a different tourist component than other places. Ms. McFann stated she had heard various opinions at presentations by citizens at City Council Worksessions arguing about the State Laws and a lot of it had to do with special uses such as events and expanding the concept of Bread and Breakfast as being a hotel. Mr. Bardenwerper referred to the State Statute Sec. 6-29-60. He clarified that the City's regulation did not come in compliance with the State and now the City is trying to bring it into conformance. Mr. Freese referred to the current lawsuit. Staff made a note to clarify owner/innkeeper/proprietor.

Public Comment

Suzanne Ramm said she and her husband are the owners of The Cuthbert House and she they do have a Special Use Permit in the T4-HN District. Our Inn is the only one that has 10 rooms. The Anchorage and the Rett House have more than 10 rooms. Ms. Ramm said, if The Cuthbert house was grandfathered in because the State Statute was created in 1998.

Mr. Tomy asked about the legal issue of whether or not our mandate is compliant with State Law or additional restrictions on what the State Law is. Mr. Tomy asked for clarification on this.

Mr. Scarborough, 1207 Bay Street said he served in the South Carolina General House and referred to the Bed and Breakfast Laws for Charleston, South Carolina. The City of Charleston and Arthur Ravenel asked Lyn McO'Neill's, a Charleston Senator, to write the Bed and Breakfast Laws. He said South Carolina law states that you as a city can be more restrictive but you cannot be less restrictive than what the State Law allows. He referred to the Special Exception clause and asked the PC to consider that when ownership changes that the new owner has to have a new review.

Ms. McFann said this issue can be also handled by getting their Business license because the license cannot be transferred to a new owner.

Public comment closed.

Chapter 2: Lot Diversity

Mr. Freese went over the new section for Lot Diversity and stated from previous discussions that it was agreed to restrict this to just a few districts (T4-N RMX IC T5-UC).

PC agreed to rename the title of this section to *Lot & Development Diversity*.

Mr. Freese he hopes at next PC Worksession to have all of these changes done and then to approve at the next PC's regular meeting. The next PC worksession will be April 6, at 1:00 pm

The Worksession ended at 2:54 pm.

DRAFT

New Business

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396 ROBERT SMALLS PARKWAY

**Sketch Plan approval for development
of a Free-Standing Emergency
Department (FSED)
by Novant Health**



STAFF REPORT: PLANNING COMMISSION (PC) – SKETCH PLAN

396 ROBERT SMALLS PARKWAY

DATE: April 20, 2026

<i>GENERAL INFORMATION</i>		
Applicant:	Conor Blaney with Ward Edwards (Engineer) for Novant Health	
Site Address / Tax PIN:	396 Robert Smalls Pkwy.; R120 028 000 1248 0000	
Applicant's Request:	Applicant is requesting Sketch Plan approval for development of a 11,620 sq. ft. Free-Standing Emergency Department (FSED) as part of a Master Plan for a new Medical Complex. The previously included 5,750 sq. ft. Primary Care Clinic (PCC) has been tabled for this request. The overall area of the Medical Complex tract encompasses 14.4-acres along SC Hwy 170 (Robert Smalls Parkway).	
Current Zoning:	INSTITUTIONAL & CAMPUS DISTRICT (IC)	
Current Land Use:	Undeveloped	
Flood Zone/Base Flood:	Flood Zone: X, Elevations: 19 – 21 ft. (Base-Flood Elevation: > 13 ft.)	
Existing Landmark/Specimen Trees:	Following number of Specimen/Landmark trees may be affected by this request: <ul style="list-style-type: none"> a. Laurel Oaks: 2x b. Live Oaks: 9x c. Loblolly pines: 19x d. Red Maples: 3x e. Southern Magnolias: 12x f. Sweetgum: 1x 	
<i>ZONING DISTRICT INFORMATION</i>		
<u>INSTITUTIONAL & CAMPUS DISTRICT (IC)</u>		
Minimum Lot Width:	50 ft. min	
Minimum Lot Size:	5,000 sq. ft. min	
Max Impervious Lot Coverage:	65%	
Front Setback	10 ft. min. / Max. setback shall not exceed average max setback on same side of block;	
Side Setback	Corner/Alley: 5 ft min Interior: 10 ft min (25 ft. when abutting T3-districts)	
Rear Setback	25 ft min.	
Building Height:	4 stories max	
<i>SURROUNDING ZONING, LAND USE AND REQUIRED BUFFERS</i>		
<u>Adjacent Zoning</u>	<u>Adjacent Land Uses</u>	<u>Required Buffer type(s) - see Sec. 5.4.1; 5.4.2; 5.4.3</u>
North: County	Robert Smalls Leadership Academy (Beaufort County School)	N/A
South: IC	Undeveloped remainder tract adjacent to "Beaufort Station"	Front Buffer Requirement along Robert Smalls Pkwy.: "Type E"
East: County	Unincorporated section assoc. with Robert Smalls Academy / W.K. Alston Drive	N/A

West: IC	4000 Margaret - Multi-family residential	Buffer Type "D"
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Background: The applicant is requesting Sketch Plan approval pursuant to Code Section 7.5.3 for construction of a 11,620 sq. ft. Free-Standing Emergency Department (FSED) as the first part of a future medical & healthcare complex. A previously included 5,750 sq. ft. Primary Care Clinic (PCC) has been removed from this revised Sketch Plan approval request. The proposed development encompasses approx. 6 acres of the overall 14.4-acre tract (~40%). This entails 3.61 ac. in developed area and 1.11 ac. in civic/open space. **The applicant has been to three (3) TRC Meetings (7/01/2025; 8/26/2025; 3/03/2026). Also, the original Sketch Plan approval request was tabled at the regular PC Meeting on 12/15/2025.**

Tree Removal / Mitigation / Canopy Coverage Proposed:

With the original Sketch Plan approval request in December 2025, a total of 55 potential Landmark and Specimen trees have been identified for the overall 14.4-ac. tract. Eight (8) of these have structural, or health issues and thus should not be considered to their full value. The remainder of 46 Specimen/Landmark trees had been identified previously that are subject to Code Section 5.3.1 as a preservable Landmark/Specimen tree species. In December 2025, a conducted Tree Canopy Coverage Assessment showed for a total of 55.72% of the disturbed area will be preserved with mature tree coverage and/or be provided in proposed complementary tree planting (street trees, buffers, open space). The minimum tree canopy coverage requirement in accordance with Code Sec. 5.3.2 for Commercial Developments is: 30%. With this latest submittal, the tree removal may even be less and the tree coverage ratio even higher. However, this is subject to change with subsequent submissions for the overall site.

Required elements of a Sketch Plan to be reviewed and discussed at a public meeting in accordance with Code Sec. 7.5.3.B:

- 1. **Street Plan** – The applicant shall provide a preliminary traffic plan that addresses the following elements:
 - a. **The proposed street network and connectivity to the existing Street network, including all proposed access points.**

Staff analysis:

- **Street access / network connectivity:** The proposed FSED will be accessible by a direct ingress/egress from S.C. Hwy. 170 (Right-in, right-out only - RIRO) as well as be accessible from a newly created connector road on the north-side of the property that will connect between W.K. Alston Drive and the “4000 Margaret” community. Moreover, the Master Plan for future medical office development of the entire site includes a second connector/feeder road as well as a second RIRO access point further west along SC 170. **After the PC meeting in mid-December 2025, the applicant provided staff with a written motion by Beaufort County School District (BCSD) to provide Novant Health with the requested encroachment permit for traversing BCSD property and connecting into WK Alston Drive.**

- **Pedestrian / bicycle accessibility & connectivity:** The applicant has committed to building several multi-use (pedestrian/bicycle) connections between the “4000 Margaret” community and the new northern connection into W.K. Alston Dr. / Robert Smalls Leadership Academy. These sidewalk connections will also interconnect with future proposed medical offices and the FSED to create both pedestrian accessibility to these buildings/offices and proposed community green spaces/greenways.

b. The location and layout of all arterial and collector roads within the development.

Staff analysis:

- The FSED will be directly accessible from S.C. Hwy. 170 (Robert Smalls Pkwy.) as well as from “4000 Margaret” and W.K. Alston Dr. through a new connector road on the north side of the property.
- In accordance with Sec. 7.2.3 – Lot Access Standards, the minimum prescribed separation between curbs is 500 ft.; this separation distance is met between W.K. Alston Drive and the proposed RIRO-ingress/egress.

c. The scope of a traffic impact study prepared by a S.C. licensed traffic engineer which evaluates proposed access points, the existing street system, and any need for any road improvements (including off-site improvements) created by the proposed development.

Staff analysis:

- The Executive Summary of the TIA comes to the following conclusion: *“Based on the results of the traffic analysis, the proposed Beaufort FSED Development is anticipated to have impact on the surrounding road network warranting additional turn lanes as follows:*
 - *A westbound left-turn lane with 200 ft. of storage at the intersection of WK Alston Dr. and Broad River Blvd.*
 - *A northbound left-turn lane with 100 ft. of storage at the intersection of WK Alston Dr. and Site Access #1.”*

2. Community Open Space Plan - The Applicant shall provide a preliminary open space plan that depicts compliance with Section 7.4 of this Code, with the following elements:

a. Proposed Community open space distribution and location, including percentage of open space.

Staff analysis:

- In accordance with **Sec. 5.3.2 – Tree Coverage Requirements**, the minimum tree canopy percentage of 30% for commercial projects is by far exceeded for this primary request of constructing the FSED. Future development will need to ensure that adequate tree canopy coverage is provided preferably through preservation of existing specimen/landmark trees on site and/or by planting of new trees as part of the landscaping plan.
- In accordance with **Sec. 7.4.2 – Community Green Space and Community Open Space Requirement**, the provided Open Space with this development proposal (disturbed area: ~ 6 ac. of overall 14 ac. site) is 1.11

ac., which comes out to 18.5% of the disturbed area. Hence, the required 15% for property sizes of 3 – 10 ac. in the “IC district” is exceeded.

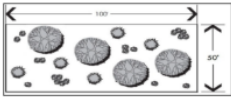
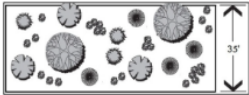
- In accordance with Sec. 7.4.3 – Community Green Space and Community Open Space Types, the overall medial complex development, and particularly in conjunction with future development of the remainder of the parcel, would largely benefit from more community-oriented and active types, such as a “Community Park”, “Greenway”, “Square/Green”, and/or “Plaza(s)”.

b. Compliance with Section 7.4.2, Community Green Space and Community Open Space Requirement.
Yes, complies.

c. Required buffer areas in accordance with Section 5.5.1.

Staff analysis:

- In accordance with **Sec. 5.4.2 – Front Buffer Requirements along Major Corridors**, the applicable location along Robert Smalls Pkwy. (S.C. 170) from Parris Island Gateway to the Broad River Bridge, mandates Buffer Type E. **The applicant has stated that Option 1 of Buffer Type E will be applied** (see chart below).

TYPE E BUFFER	
This buffer provides greater spacing and high-density screening designed to define "green" corridors along major roadways.	
Option 1	 <ul style="list-style-type: none"> • Depth: 50 feet • Overstory trees: 4 every 100 linear feet • Understory trees: 5 every 100 linear feet • Evergreen shrubs: 30 every 100 linear feet and at least 6 feet high at maturity
Option 2	 <ul style="list-style-type: none"> • Depth: 35 feet • Overstory trees: 5 every 100 linear feet • Understory trees: 7 every 100 linear feet • Evergreen shrubs: 35 every 100 linear feet and at least 6 feet high at maturity • At least 50% of all trees must be evergreen

d. Wetland areas and setbacks as determined by SCDES-BCM, if applicable.

Staff analysis:

- A small existing isolated wetland area (~0.049 ac.) on the north of the overall property will be protected by a 10 ft. wetland buffer and thus will not be impacted by the new connector road.

e. Proposed park locations, acreage, and types of parks in accordance with Section 7.4.

Staff analysis:

- Several community green spaces/open space areas are planned to be provided within the Master Plan. This includes aforementioned isolated wetland area as well as a ~16,053 sq. ft. (0.36 ac.) open space area to be preserved at the northwest corner of the overall property adjacent to “4000 Margaret”. Moreover, the provided community green & open spaces are evenly distributed in between the proposed medical office buildings with larger & contiguous spaces planned along the perimeter and integrated into the design of the new connector/feeder roads. Last but not least, street trees and landscaped green spaces will be

provided along all streets, parking lots and internal islands as well as along the front buffer towards Robert Smalls Pkwy.

3. Pedestrian Network

Staff analysis:

- As per latest Master Plan, the developer proposes to create extensive sidewalk connections throughout the entire site with main multi-use pathways provided along both the north and south connector/feeder roads between “4000 Margaret”, W.K. Alston Dr. and the initial Free-Standing Emergency Department facility.
- The main multi-use arterials will be built to accommodate both pedestrian and bicycle traffic.
- Particularly on the southside of the tract adjoining S.C. Hwy. 170 (Robert Smalls Pkwy.), the applicant has revised their master plan to include a multi-use pathway, which is also part of the City’s Civic Master Plan (“*the installation of an off-street multi-use path with a wide landscaped separation from vehicular travel lanes in order to provide a comfortable, safe and beautiful option for pedestrian and cyclists.*”) and also aligns with goals of Beaufort County’s Corridor Plan for S.C. Hwy. 170 to establish a continuous multi-use trail.

4. Zoning/Design

Staff analysis:

- In accordance with **Sec. 2.3.3, the “IC district” is designed to support institutional uses, such as for hospitals, universities, research facilities, and offices in a campus-like setting.** The proposal clearly aligns with this zoning-specific designation.

5. Overall Utility Plan

Staff analysis:

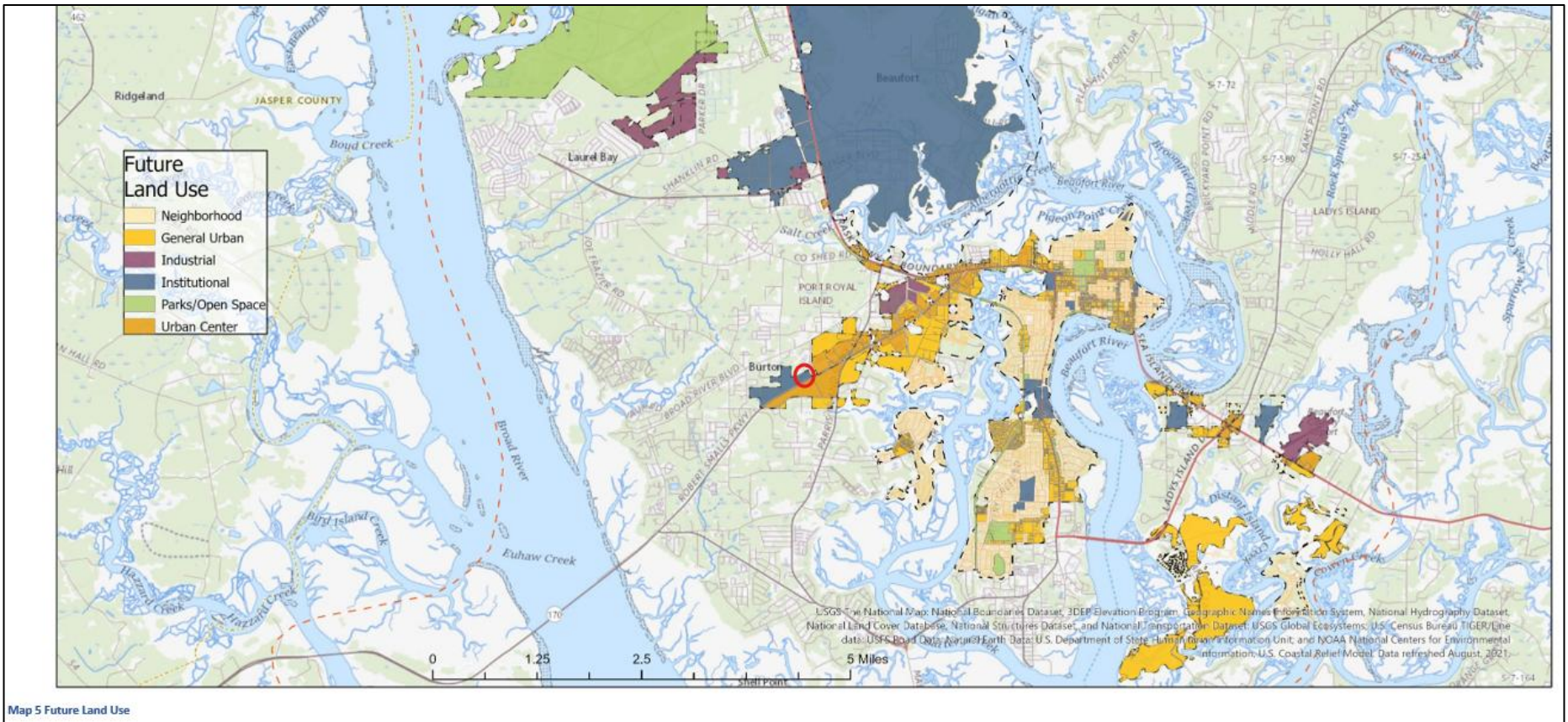
- The applicant has submitted confirmation from “Beaufort Jasper Water & Sewer Authority – BJWSA” attesting to both sufficient water & sewer capacity through tying into an existing 8-inch water main located along WK Alston Dr. and a newly built gravity sewer pump station (SP-75) by the new 4000 Margaret community for this FSED proposal.

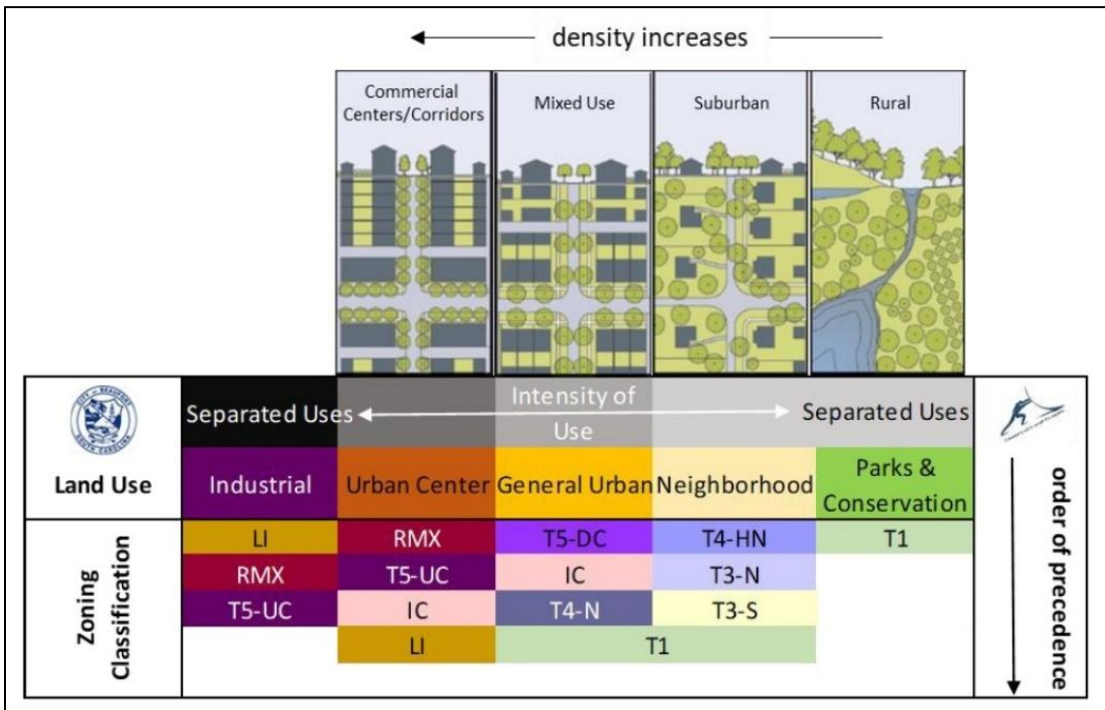
Sketch Plan Review Criteria in accordance with Code Sec. 7.5.3.F:

1. ***“The land use mix within the project conforms to Beaufort’s Zoning District Map and Comprehensive Plan Preferred / Future Land Use Map and furthers the goals and policies of the Comprehensive Plan”:***

Staff analysis:

- As per below Future Land Use Map, the current Beaufort Comprehensive Plan identifies the location of the proposed Novant Health complex, initially consisting of a FSED, as both “Institutional” and part of an “Urban Center”. The current zoning district designation as “IC” is compliant with the goals for future land use development in this location along the Robert Smalls Parkway corridor.

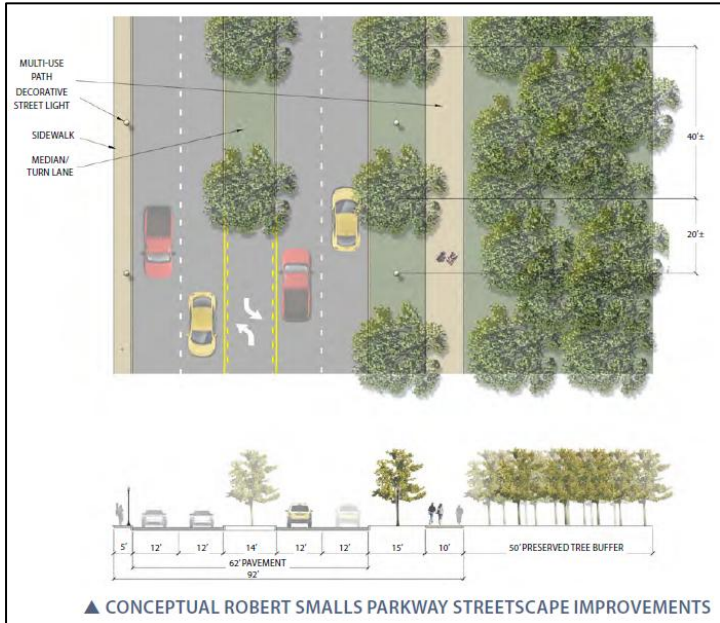




2. *“The Sketch Plan is consistent with the rationale and criteria set forth in this Chapter, the City’s Comprehensive Plan, and the Civic Master Plan as amended”:*

Staff analysis:

- In **Chapter 8.3 of the Civic Master Plan**, Robert Smalls Parkway is identified as a “mixed-use corridor of vibrant activity”. In regard to streetscape improvements, the Civic Plan recommends *“the installation of an off-street multi-use path with a wide landscaped separation from vehicular travel lanes in order to provide a comfortable, safe and beautiful option for pedestrian and cyclists.”* (page 172 et. seq.) – see chart below.



3. *“The preliminary traffic, open space, park, utility, and pedestrian design is adequate and functional given the existing and planned capacities of each system, and meets the standards found in this Code”:*

Staff analysis:

- Vehicular access to the proposed FSED seems to be adequate. **The construction of the proposed connector roads between “4000 Margaret” and W.K. Alston Drive (Robert Smalls Leadership Academy) will provide adequate street network connections between existing developments and provide sufficient alternatives aside from having to access through Robert Smalls Parkway.**
- This includes alternative connections for both pedestrians & cyclists, who will be able to both access the new medial office complex (besides the FSED) from adjacent residential developments. **Further connections to adjoining commercial destinations to the east of WK Alston Drive, i.e., Walmart and Cross Creek Shopping Plaza would be very beneficial to further study and help with implementation.**
- The master plan includes both community green & open spaces as well as the planting of street trees and landscaped parking lots etc. next to a decent number of existing specimen and landmark trees preserved and integrated. In accordance with the Community Green Space and Community Open Space goals, as stated in Sec. 7.4.1 which *“are intended for the use and enjoyment of a development’s residents, employees, or users”*, the master plan for the Novant Health site demonstrably implements this.

4. *“The conceptual design and massing proposed is consistent with the requirements of the Development Code”:*

Staff analysis:

- The height of the proposed FSED building will be no more than 1 story. That underutilizes the permissible building height of 4 stories max.
- Design and massing of the buildings, particularly the building design must comply with the typical Lowcountry vernacular. As of now, the architecture is not reflective of that. This includes such currently included architectural design elements, such as “Bahama shutters”, “reeds”-textured Parklex rainscreen panels, brown and gray colors on the exterior as well as oversized window frames, faux windows, and the application of prefabricated cementitious building wall segments. Building elevations that face the street must have at least 40% of the first-floor wall area consist of windows and/or doors, and 15% of upper floors.
- **Sec. 2.4.2.E - Parking Location:** “Parking areas and drive aisles shall be located in the side or rear yard whenever possible. Parking areas and drive aisles shall not be located within the front setback, but may be placed in front of the building. However head-in rows of parking shall not be permitted to front Primary Streets. Diagonal rows of parking shall not front Primary Streets except as part of a slip or frontage-road design, or when there are less than 10 consecutive spaces. Where 10 or more parking spaces are required, the majority of the parking shall be located in the side or rear of the building.”**The current parking lot between FSED and Robert Smalls is not compliant with this section.**
- **Sec. 4.6.4 - Specific to Conventional Districts - F. Additional Guidelines Specific to IC District:** All buildings constructed as part of a campus may be subject to the guidelines for civic building outlined in Section 4.5.10 or shall be held to standards similar to the requirements in the surrounding context. **The revised building design for the FSED has not been submitted to staff yet, as it is still being reworked.**

➤ **Sec. 5.6.7 – Parking Lot Design – B. Pedestrian Corridors in Parking Lots:**

Parking lots with 40 spaces or greater shall be designed to separate pedestrian travel from vehicles. They shall include designated pedestrian walkways to provide safe access to building entries for pedestrians:

1. Perimeter sidewalks — typically located on public rights-of-way— and/or interior parking lot pedestrian corridors may be utilized to provide the required pedestrian access.
2. Pedestrian pathways (if provided) shall be a minimum 5 feet in width.
3. Where parking is located between a public entrance and the fronting sidewalk, a pedestrian pathway shall be provided, following the shortest practical route across the parking lot between at least 1 such entrance on each side of the building facing a public street.
4. Pedestrian pathways shall be clearly delineated with striping. This may also be accomplished with the use of paving materials that differ from that of vehicular areas, striping or other similar methods.

FINDINGS AND RECOMMENDATIONS

Findings: Refer to sectional Staff Analyses (above).

Staff Recommendation: Approve Sketch Plan with conditions that Preliminary Plat submittal includes the following items to be approved at future PC meeting:

- ❖ Provide final architectural renderings for FSED that demonstrates that proposed building(s) is/are architecturally compliant with the local vernacular of the Lowcountry (see previous architectural staff comments below).
- ❖ Amend site layout for FSED whereas all parking lots are located at either the side and/or the rear of the lot as well as change dumpster location to be located at a more interior/unobtrusive location.
- ❖ Provide final encroachment permit from Beaufort County School District for connecting new road into WK Alston Dr.
- ❖ Provide landscaping plan for FSED specifying street tree planting, parking lot landscaping, and any types of provided community green spaces and open spaces (as required in Sec. 7.4.2: 15% of the area between 3 - 10 ac. shall be provided for community green/open spaces).
- ❖ Provide updated arborist report and/or tree survey for any specimen/landmark trees that may be impacted by proposed FSED (future development).

Appendix: Previous Staff Review Comments - Architecture/Building Design (provided by Meadors Architecture, LLC):

FSED:

- a. It is unclear if any of the windows with Bahama shutters have windows behind them, or if this is a faux window detail. The renderings do not appear to show any windows. Per Section 4.6.4.C.1, "Building elevations that face the street shall have at least 40% of the first-floor wall areas consist of windows and/or doors, and 15% of upper floors." Staff interprets this code section to mean building fenestration that allows visible light to transfer to the interior during the day and light sources from inside to activate the otherwise dark facades at night. Applicant to add windows to meet code compliance. Any instances of faux windows should be removed.
- b. As it is designed, the building's primary façade and entrance face into an internal parking area, while the rear façade and mechanical yard face Robert Smalls Parkway. This rear façade does not appear to meet the 40% fenestration requirement to meet code compliance, and staff believes that placing the mechanical yard on this façade will make it difficult to meet this requirement. Staff believes the building orientation may need to change to better relate to the primary frontage rather than internal, head-in parking. Staff understands the need for quick access from the parking lot to the building entrance, and recommends the applicant study the addition of a second entrance. A potential solution may be to swap the FSER with the PCC on the site plan where the FSER would maintain its current orientation but front to the north access road and the PCC will mirror in plan to front to Robert Smalls Parkway.
- c. Generally, staff does not believe this building is indicative of traditional Lowcountry vernacular architecture and offers the following comments:
 - i. Staff is not supportive of the textured Parklex cementitious rainscreen panel, especially in the "reeds" texture. This is not a common cladding material in Beaufort. Note: Section 4.6.4.A.1.b states "fiber cement board siding, smooth finish." The textured finish "reeds" is not compliant with this section of the code.
 - ii. Bahama shutters are not common in Beaufort and appear more tropical/coastal. Staff recommends they are removed from the project.
 - iii. The window head casings appear to extend past the jamb casings, which is atypical. The sills are not visible in 2D drawings or renderings. These windows should have traditional head and jamb casings with a projecting sill.
 - iv. Staff recommends making the colors lighter, as both the brown and gray appear dark. Staff would be in support of a white or off-white color instead of the gray.
 - v. There are large frame elements (brown) around some of the windows, but not all. Their placement appears arbitrary. Staff believes the intent of these is to break up the façades but believe these may be more successful if designed as actual bays rather than frames applied to the wall.
 - vi. Some of the joints in the panels appear to sit on the outside edges of the windows, while other joints are set a few inches away from the windows.
- d. The use of the protruding brown frames around some of the windows is different from the PCC, but staff still believes its placement is arbitrary. Some of these protrusions also appear to cut off the brick base. This base should continue around the building without interruption, except at the entrances.
- e. The ambulance drive-through bay does not appear to relate or connect well to the rest of the building. If it is possible, staff recommends lowering the roof to align with the main building's roof.
- f. The mechanical screen on top of the roof appears almost as tall as the exterior walls of the building (staff measured the screen wall digitally as approximately 10'-0" tall). To remedy this and to provide better screening, staff recommends converting the flat roof to a sloped roof to conceal the mechanical areas. Staff believes a sloped roof would also create a more Lowcountry approach to this building, as hip/gable roofs are common throughout the area.



MAJOR SUBDIVISION REVIEW

Sketch Plan – Application Form

Community Development Department
1911 Boundary Street, Beaufort, South Carolina 29902
p. (843) 525-7049 / f. (843) 986-5606
Email: development@cityofbeaufort.org / Website: www.cityofbeaufort.org

Application Fee: \$250

OFFICE USE ONLY: Date Filed: Application #: Zoning District:

Approved By: Date:

Pursuant to Section 6-29-1145 of the South Carolina Code of Laws, is this tract or parcel restricted by any recorded covenant that is contrary to, conflicts with, or prohibits the activity described in this application? Yes No

Has Project Attended a TRC Meeting? (Required) Yes No

Submittal Requirement: 7 hard copies and 1 digital copy of all forms and information are required.

Applicant Information:

Applicant Name: Conor Blaney

Applicant Address: PO Box 381, Bluffton, SC 29910

Applicant E-mail: Applicant Phone No.:

Applicant Title: Homeowner Tenant Architect Engineer Developer Contractor

Owner (if other than the Applicant): Novant Health South Carolina Holdings, LLC

Owner Address: 2085 Frontis Plaza Blvd, Winston Salem, NC 27103

Owner E-mail: Owner Phone No.:

Project Information:

Project Name: Beaufort FSED & PCC

Property Address: 396 Robert Smalls Parkway

Property Identification Number(s): R120 028 000 1248 0000

Site Area in Acres: 6

Proposed Area Allocations (in acres and percentage):

- Developed Area: 3.61 / 60
Civic/Open Space: 1.11 / 19



MAJOR SUBDIVISION REVIEW

Sketch Plan – Application Form


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Application Fee: \$250

Brief Project Narrative: (Attach any necessary documentation, spec sheets, pictures, paint swatches, etc.)

The proposed development is to include a new 11,620 SF Free-Standing Emergency Department (FSED) in the City of Beaufort, on a 14.4-acre parcel along Hwy-170. The property is undeveloped, identified as Tax Map Number R120 028 000 1248 0000, and zoned Institutional and Campus (IC) within the City of Beaufort municipal limits. Proposed drives, parking, landscaping, utilities, and other infrastructure will also be constructed in support of the development.

Certification of Correctness: I/we certify that the information in this application is correct.

Applicant's Signature: _____  _____ Date: 2/23/2026

NOTE: See pages 3-4 for the Sketch Plan requirements and checklist.



MAJOR SUBDIVISION REVIEW

Sketch Plan – Application Form

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Application Fee: \$250

Sketch Plan – Requirements and Checklist:

The Sketch Plan shall consist of the following:

- **Street Plan:** A preliminary traffic plan that addresses the following elements:
 - The proposed street network and connectivity to the existing street network, including all proposed access points.
 - The location and layout of all arterial and collector roads within the development.
 - The scope of a traffic impact study prepared by a SC licensed traffic engineer which evaluates proposed access points, the existing street system, and any need for any road improvements (including off-site improvements) created by the proposed development.
- **Community Open Space Plan:** A preliminary open space plan that depicts compliance with Section 7.4 of this Code, with the following elements:
 - Proposed community open space distribution and location, including percentage of open space.
 - Compliance with Section 7.4.2, Community Green Space and Community Open Space Requirement.
 - Required buffer areas in accordance with Section 5.5.1.
 - Wetland areas and setbacks as determined by SCDES-BCM, if applicable.
 - Proposed park locations, acreage, and types of parks in accordance with Section 7.4
- **Pedestrian Network:**
 - Location of all trails within the development, and connection to existing trail networks.
 - Connectivity of sidewalks to the existing pedestrian system, including any off-site sidewalk improvements. This includes planning for a one-quarter mile pedestrian distance.
 - Depiction of any bike lanes or any other multi-modal features.
- **Zoning/Design:**
 - Location of zoning boundaries depicted on the Sketch Plan.
 - Sketch Plan shall show how the development is consistent with the surrounding area, and within the property itself.
 - Conceptual building design and massing.
- **Overall Utility Plan:**
 - A letter from the appropriate utility, confirming the existing capacity of the surrounding utility system, and the future capacity of the utility system for the proposal. Utility plans for the interior of the development (such as water and sewer service lines) are not required as part of this process.
 - Proposed connections to the existing utility system.



MAJOR SUBDIVISION REVIEW

Sketch Plan – Application Form

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p. (843) 525-7049 / f. (843) 986-5606
Email: development@cityofbeaufort.org / Website: www.cityofbeaufort.org

Application Fee: \$250

Sketch Plan – Application Submittal Requirements:

- **Sketch Plan:** The application package shall include the following items:
 - Sketch Plan application form and fee.
 - Name of project.
 - North arrow, scale (not greater than one inch equals 200 feet), and date of preparation.
 - Vicinity map.
 - Legal description.
 - Acreage of property; acreage in each zoning district; acreage in parks; and acreage in open space.
 - USGS topographic contours.
 - Location and approximate acreage of proposed land uses.
 - Existing easements and rights-of-way on or adjacent to the property.
 - Existing streets on or adjacent to the property (show and label street names).
 - Table providing the following information for each proposed land use area: total acreage, proposed density; and proposed number of dwelling units and/or commercial buildings.
 - Location and acreage of proposed open space and parks as per Section 7.4, trails, regional trail connections, playgrounds, schools or other public uses.
 - Proposed street system depicting the location and layout of all arterial and collector roads within the development.
 - The scope of a traffic impact study prepared by a SC licensed traffic engineer which evaluates proposed access points, the existing street system, and any need for any road improvements (including off-site improvements) created by the proposed development.
 - Floodplain boundary with a note regarding the source of information.
 - Zoning on adjoining properties.
 - A letter from the appropriate utility, confirming the existing capacity of the surrounding utility system, and the future capacity of the utility system for the development.
 - Proposed connections to the existing utility system.
 - The location of any proposed or required lift stations.
 - Design rationale – description of how the development is integrated with the surrounding area, how it responds to site features/constraints and how it is consistent with this code.
 - General description of plan for drainage and storm water management, including any regional drainage solutions.
 - Description of how the proposed development complies with the City’s Comprehensive Plan.
 - Concept of structure location, massing and design.



Project: Beaufort FSED
City of Beaufort, SC

Date: February 23rd, 2026

Applicant: Novant Health
Primary Contact: Matthew Stiene
Senior Vice President, Construction & Facility Services
2085 Frontis Plaza Blvd, Winston Salem NC, 27103
E: [REDACTED]
P: [REDACTED]

Agent: Ward Edwards, Inc.
Primary Contact: Conor Blaney, PE
P.O. Box 381
Bluffton, SC 29910
E: [REDACTED]
P: [REDACTED]

Description

The proposed development is to include a new 11,620 SF Free-Standing Emergency Department (FSED) in the City of Beaufort, on a 14.4-acre parcel along Hwy-170. The property is undeveloped, identified as Tax Map Number R120 028 000 1248 0000, and zoned Institutional and Campus (IC) within the City of Beaufort municipal limits. Proposed drives, parking, landscaping, utilities, and other infrastructure will also be constructed in support of the development. The site is relatively flat with elevations running from 21 to 19 and will not require any wetland impacts.

Zoning District

Institutional and Campus (IC)

Design Rationale

The proposed FSED will significantly enhance access to high-quality medical care, support community well-being, and help meet the growing healthcare needs of Beaufort County. The facility aligns with the intent of the IC zoning district, which is designed to accommodate higher-intensity, larger-scale institutional and commercial uses. The operational scale and overall site activity of an FSED is fully compatible with the purpose of the district and the surrounding corridor, providing essential services while integrating seamlessly into an area already characterized by impactful community-oriented and service-based development.

City Comprehensive Plan Compliance

The proposed FSED aligns with several goals of the City of Beaufort Comprehensive Plan. The project helps expand access to everyday and emergency healthcare services, supporting the Plan's focus on maintaining quality community services as the area grows. Locating the facility along Robert Smalls Parkway is consistent with the Plan's guidance for infill and reinvestment along established corridors with existing infrastructure. The project's scale and site design also reflect the Plan's emphasis on ensuring compatibility



with surrounding development. Overall, the proposed use fits within the broader direction outlined in the Comprehensive Plan.

Tree Removal

To support the proposed development, tree removal will be necessary. The final landscape plan will show open space area being planted in accordance with the City of Beaufort municode along with the required street trees and landscape island plantings.

Access

Vehicular access can be made from Highway 170, W.K. Alston Drive and the adjacent multifamily development to the West of the property. These surrounding roads will also serve for fire protection and emergency vehicle access.

Parking

The City of Beaufort municode requires 1 per 2 beds, plus 1 per staff, plus 1 per 4 employees for the FSED. The proposed land plan will exceed this required amount. Additionally, ADA parking stalls will be strategically located near building entrances and exceed the required amount as well.

Utilities

An existing 8-inch water main, owned and maintained by the Beaufort-Jasper Water and Sewer Authority (BJWSA), is available to serve this property and is located along W.K. Alston Drive. Additionally, the adjacent multifamily development has provided an 8-inch stub-out, which the proposed development may utilize as a secondary point of connection. A proposed 8-inch water main will extend from both connection points and tee south through the parking lot to serve the FSED building. Two fire hydrants are proposed to ensure adequate fire protection and domestic service capacity.

To allow for future capacity needs, the adjacent multifamily development also installed a pump station designed to accommodate the proposed development. As a result, no further system modifications or capacity analyses are required. An 8-inch gravity sewer main will connect to this pump station, running east along the access road and through the proposed parking lot at an approximate slope of 0.40% to serve both buildings.

Drainage

Stormwater runoff from the Western portion of the undeveloped site is currently conveyed to a storm drain located Southwest of the property, which discharges into Battery Creek and ultimately into the Beaufort River. Runoff from the eastern portion of the site is directed to an existing stormwater pond Northeast of the property, which also outfalls into Battery Creek, and subsequently to the Beaufort River. Stormwater generated from the proposed development will be collected into storm inlets and routed to onsite stormwater BMPs prior to discharging into Battery Creek. The proposed development will be designed to collect, treat, and discharge the post-development runoff to the same outfall location at a rate less than that of existing conditions to meet local and state stormwater regulations.

Erosion Control

Silt fence, inlet protection, dust control, and temporary/permanent seeding will be used to help control erosion on the site during construction activities. The existing access will be utilized as the construction entrance.



Zoning Classification

CobData

HISTORIC DISTRICT-Preservation Neighborhood

HISTORIC DISTRICT-Conservation Neighborhood

Bladen Street Redevelopment District

Boundary Street Redevelopment District

Retail Overlay (Church & Green Street)

Retail Overlay

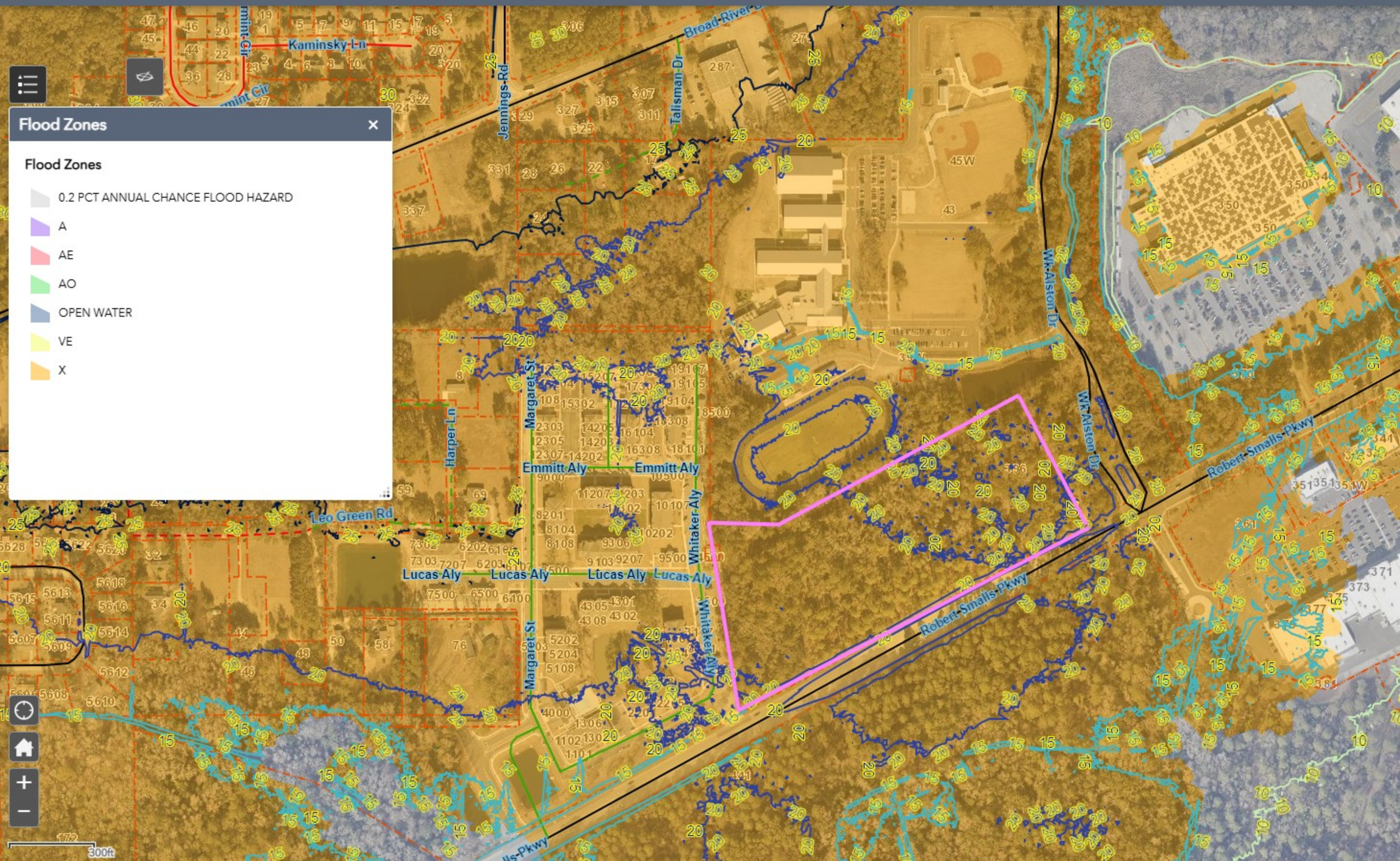
Zoning Districts

LEGACY PUD (LPUD)

- T1
- T3-S
- T3-N
- T4-HN
- T4-N
- T4-NA
- T5-DC
- T5-UC
- T5-UC / RMX
- RMX
- IC
- MH
- LI
- MR



300ft



Flood Zones

- 0.2 PCT ANNUAL CHANCE FLOOD HAZARD
- A
- AE
- AO
- OPEN WATER
- VE
- X



300ft



Broad River Blvd

W K Alston Dr

Beaufort Bull Sharks Wrestling Club

Walmart Supercenter

Claire's Walmart
Book Your Appointment Now

Nail Bar Lounge

Chicken Salad Chick
Chicken Salad Chick

Apartment

PetSmart Dog Training
Extra 20% Off Online Only

T.J. Maxx

Fifth Third Bank & ATM

Christ Our Lord Episcopal Church

Ulta Beauty
Shop Body Wash Brands

PetSmart

PetSmart Grooming

4000 Margaret
1-3 Bedroom Apartments

Robert Smalls Pkwy

Parris Island Gateway

US Hwy 21

Old Jeric

SHEET 1

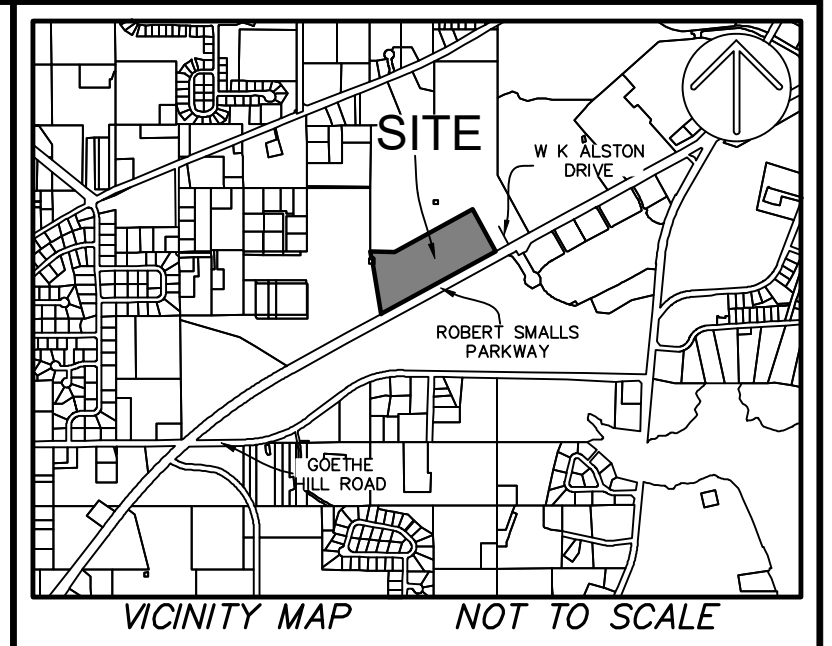
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SHEET 3

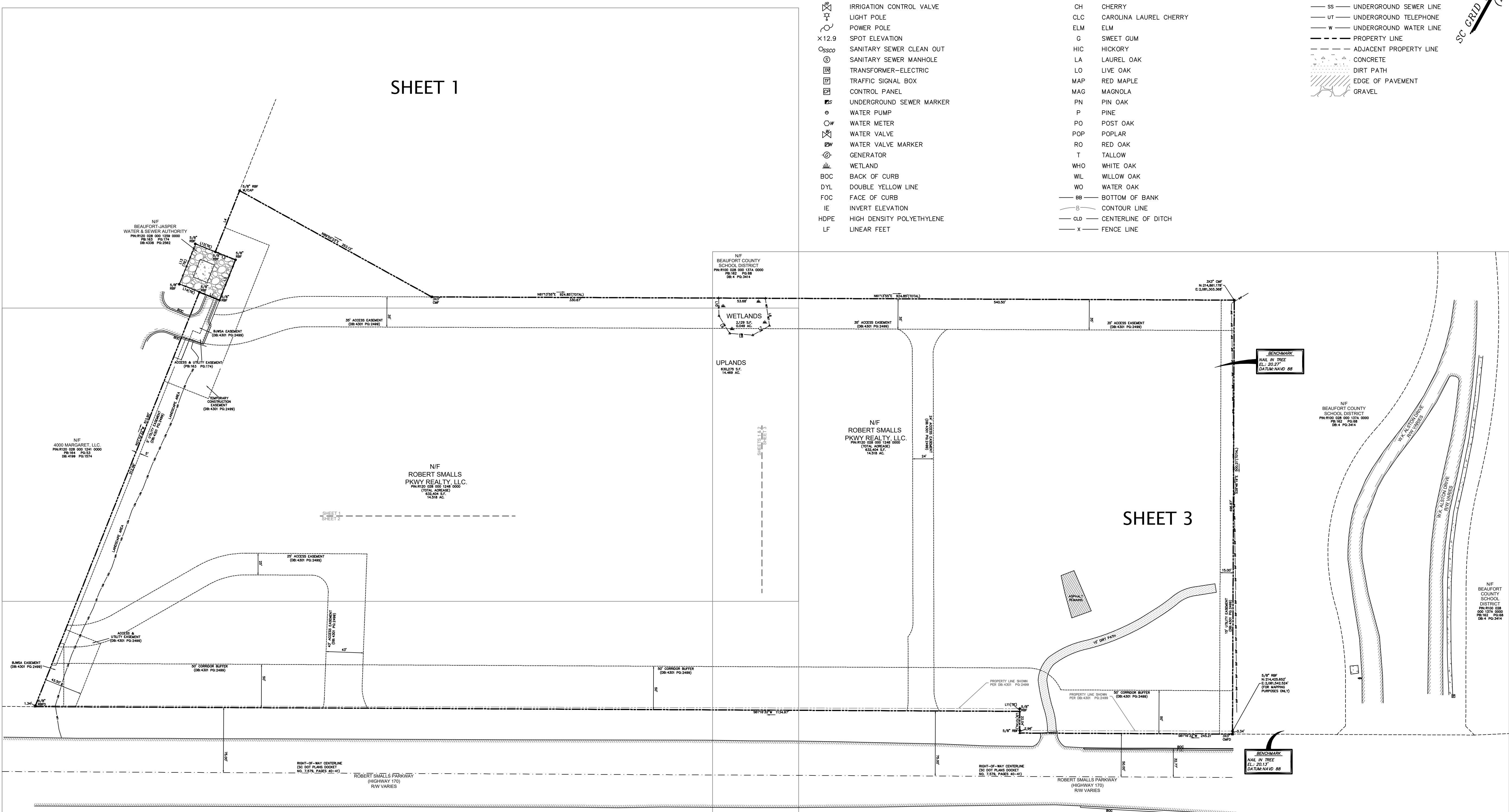
- LEGEND
- ▲ CALC POINT - CORNER NOT SET
 - CONC. MONUMENT FOUND
 - CMFD CONC. MONUMENT FOUND DISTURBED
 - IRON REBAR FOUND
 - RBF IRON REBAR FOUND DISTURBED
 - ⊗ AIR RELEASE VALVE
 - ⊕ FIRE HYDRANT
 - ⊠ FIBER OPTIC BOX
 - × GUY WIRE
 - ⊗ IRRIGATION CONTROL VALVE
 - ⊕ LIGHT POLE
 - ⊕ POWER POLE
 - ⊕ SPOT ELEVATION
 - ⊕ X12.9 SPOT ELEVATION
 - SANITARY SEWER CLEAN OUT
 - SANITARY SEWER MANHOLE
 - ⊠ TRANSFORMER-ELECTRIC
 - ⊠ TRAFFIC SIGNAL BOX
 - ⊠ CONTROL PANEL
 - ⊕ UNDERGROUND SEWER MARKER
 - WATER PUMP
 - WATER METER
 - ⊗ WATER VALVE
 - ⊗ WATER VALVE MARKER
 - ⊕ GENERATOR
 - ⊕ WETLAND
 - ⊕ BOC BACK OF CURB
 - ⊕ DYL DOUBLE YELLOW LINE
 - ⊕ FOC FACE OF CURB
 - ⊕ IE INVERT ELEVATION
 - ⊕ HDPE HIGH DENSITY POLYETHYLENE
 - ⊕ LF LINEAR FEET

- LEGEND
- PD PIPE DIRECTION
 - PIN PARCEL IDENTIFICATION NUMBER
 - PVC POLYVINYL CHLORIDE PIPE
 - RCP REINFORCED CONCRETE PIPE
 - SWL SINGLE WHITE LINE
 - SDWL SINGLE DASHED WHITE LINE
 - ASH ASH
 - BJO BLACKJACK OAK
 - BP BRADFORD PEAR
 - CH CHERRY
 - CLC CAROLINA LAUREL CHERRY
 - ELM ELM
 - G SWEET GUM
 - HIC HICKORY
 - LA LAUREL OAK
 - LO LIVE OAK
 - MAP RED MAPLE
 - MAG MAGNOLA
 - PN PIN OAK
 - P PINE
 - PO POST OAK
 - POP POPLAR
 - RO RED OAK
 - T TALLOW
 - WHO WHITE OAK
 - WL WILLOW OAK
 - WO WATER OAK
 - BB BOTTOM OF BANK
 - CONTOUR LINE
 - CLD CENTERLINE OF DITCH
 - X FENCE LINE

- LEGEND
- FM FORCEMAIN
 - OHP OVERHEAD POWER LINE
 - WET WETLAND LINE
 - LSA LANDSCAPE AREA LINE
 - TB TOP OF BANK
 - SD UNDERGROUND DRAINAGE LINE
 - UE UNDERGROUND ELECTRIC LINE
 - UFO UNDERGROUND FIBER OPTICS LINE
 - UG UNDERGROUND GAS LINE
 - SS UNDERGROUND SEWER LINE
 - UT UNDERGROUND TELEPHONE
 - W UNDERGROUND WATER LINE
 - PROPERTY LINE
 - - - ADJACENT PROPERTY LINE
 - CONCRETE
 - DIRT PATH
 - EDGE OF PAVEMENT
 - GRAVEL

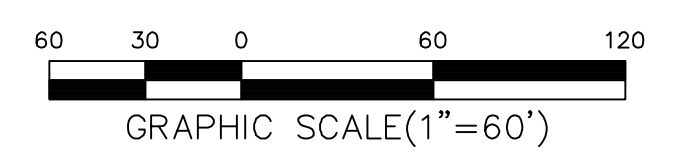


SC GRID (NAD 83) NORTH
(2011)



- NOTES
- FULL LEGEND & LINE TABLE ARE LOCATED ON COVER SHEET.
 - THIS PARCEL APPEARS TO LIE IN FLOOD ZONE X, COMMUNITY 450026, MAP NUMBER 45013C0161G.
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- REFERENCES
- PB-163 PG:174
 - PB-103 PG:81
 - PB-77 PG:153
 - DB-4338 PG:2565
 - SC DOT PLANS, DOCKET NO. 7.579, PAGES 40-41



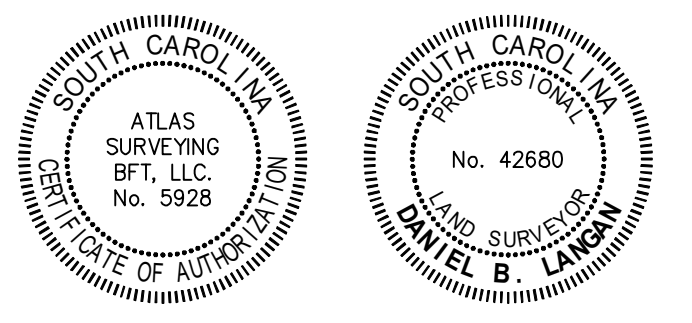
PREPARED FOR:
MEADOWS & OHLY
AN AS-BUILT/TREE AND TOPOGRAPHIC SURVEY OF
#396 ROBERT SMALLS PARKWAY
PIN:R120 028 000 1248 0000

BURTON
BEAUFORT COUNTY, SOUTH CAROLINA
FIELD WORK: SAD
FIELD CHECK: DBL
DRAWN BY: DTJ
FIELD DATE: 04-01-2025
PLAT DATE: 04-21-2025
SCALE: 07-1-60'
PROJECT NO.: 87-25063
FILE: BFT-25063 T1.000

COVER SHEET

ATLAS SURVEYING, INC.

168 BOARDWALK DRIVE, SUITE A.
RIDGELAND, SC 29936.
PHONE: (843) 645-9277
WEBSITE: WWW.ATLASSURVEYING.COM

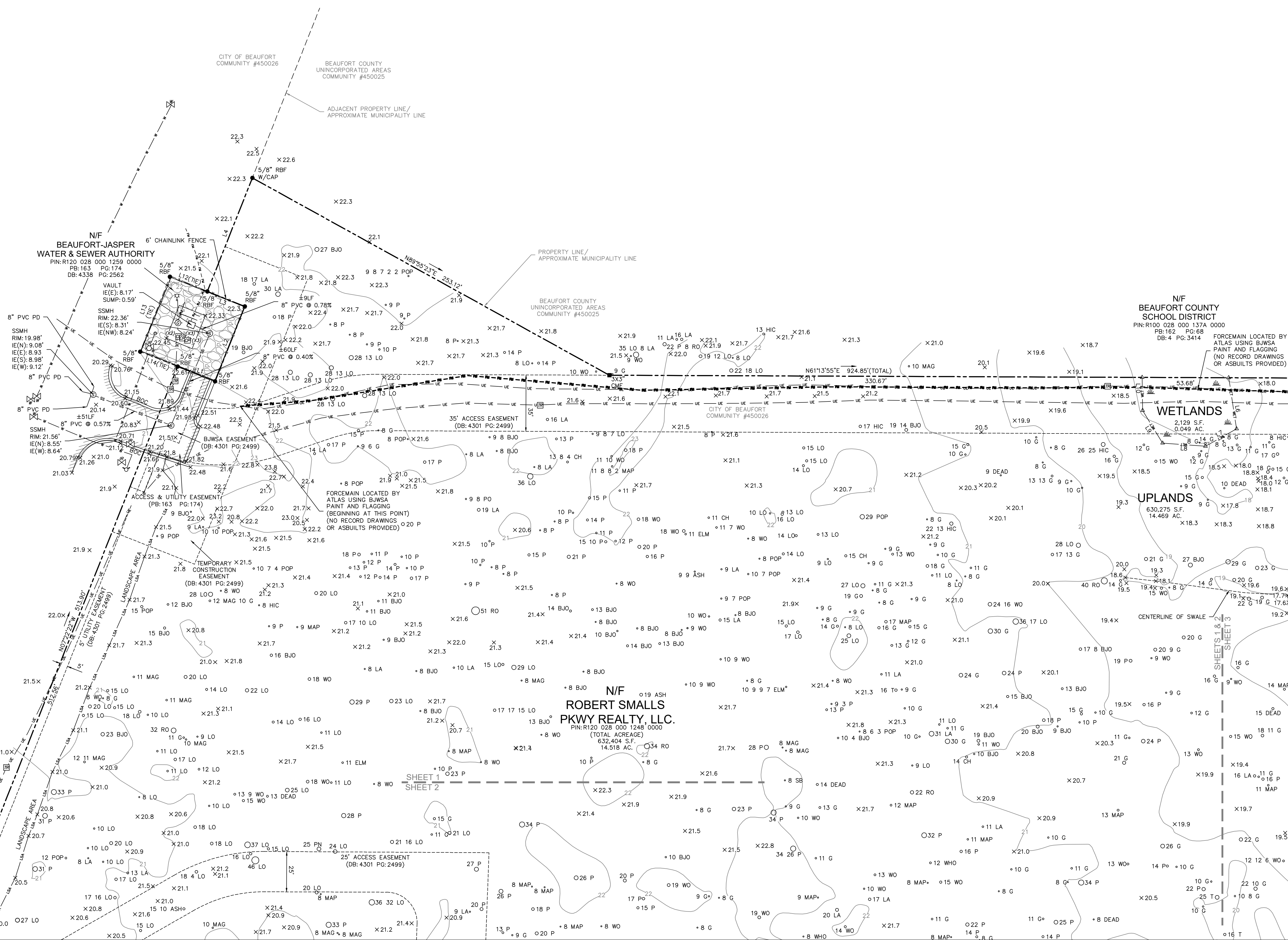
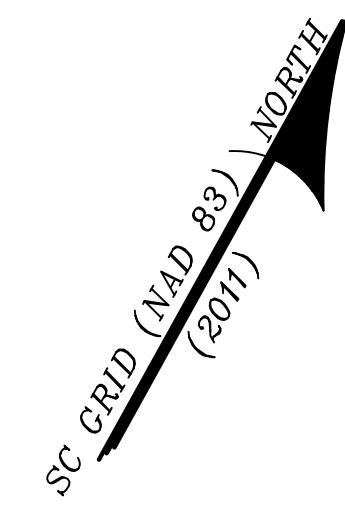
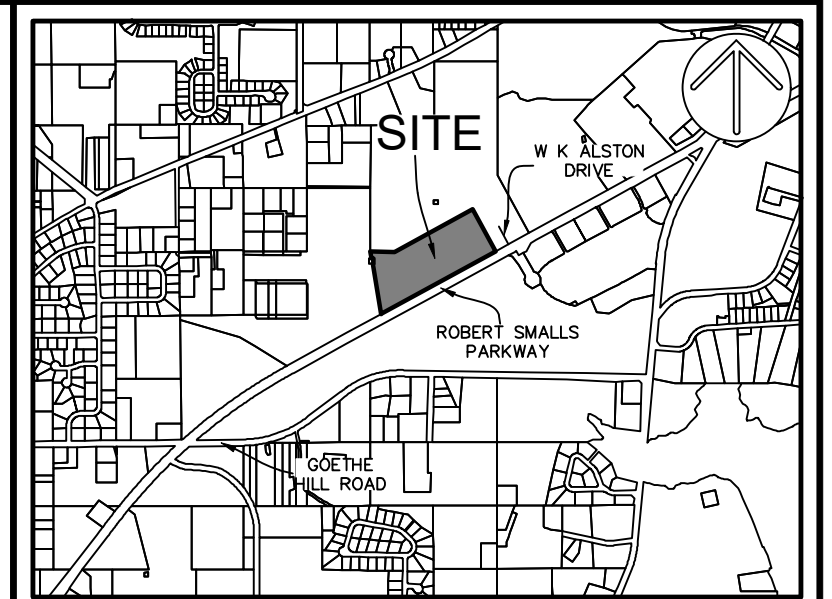


LINE TABLE

LABEL	BEARING	DISTANCE
L1	N82°39'09"E	24.97'
L2	N07°21'03"W	50.00'
L3	S82°43'31"W	25.02'
L4	N07°21'59"W	75.83'
L5	N29°00'24"W	25.00'
L6	S37°43'23"E	32.05'
L7	S31°35'23"W	22.08'
L8	S65°17'06"W	26.73'
L9	N60°31'10"W	23.72'
L10	N29°39'54"W	20.53'
L11	N27°59'49"W	3.04'
L12	S82°31'27"W	24.99'
L13	S07°23'22"E	49.99'
L14	S82°36'21"W	25.00'

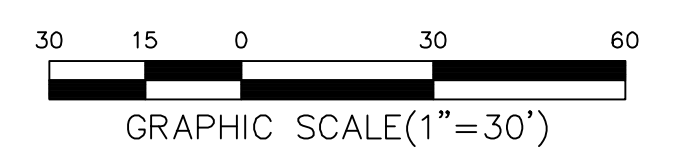
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DANIEL B. LANGAN
S.C.P.L.S. No. 42680
NOT VALID UNLESS CROMPED WITH SEAL



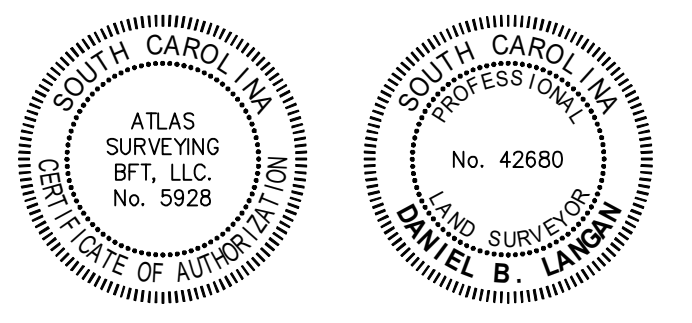
- NOTES
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- REFERENCES
1. PB:163 PG:174
 2. PB:103 PG:81
 3. PB:77 PG:153
 4. DB:4338 PG:2565
 5. SC DOT PLANS, DOCKET NO. 7.579, PAGES 40-41



PREPARED FOR:
MEADOWS & OHLY
AN AS-BUILT/TREE AND TOPOGRAPHIC SURVEY OF
#396 ROBERT SMALLS PARKWAY
PIN:R120 028 000 1248 0000
BURTON
BEAUFORT COUNTY, SOUTH CAROLINA

ATLAS SURVEYING, INC.
168 BOARDWALK DRIVE, SUITE A.
RIDGELAND, SC 29936.
PHONE: (843) 645-9277
WEBSITE: WWW.ATLASSURVEYING.COM



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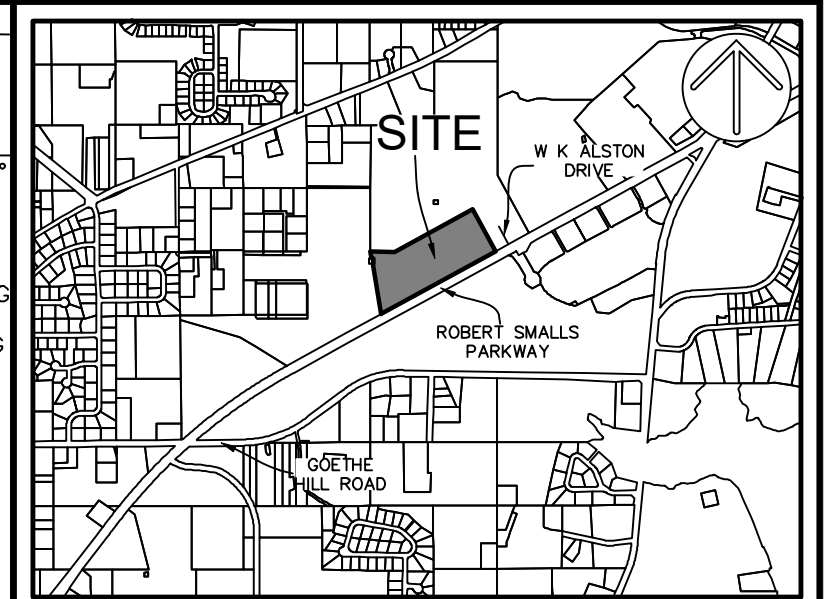
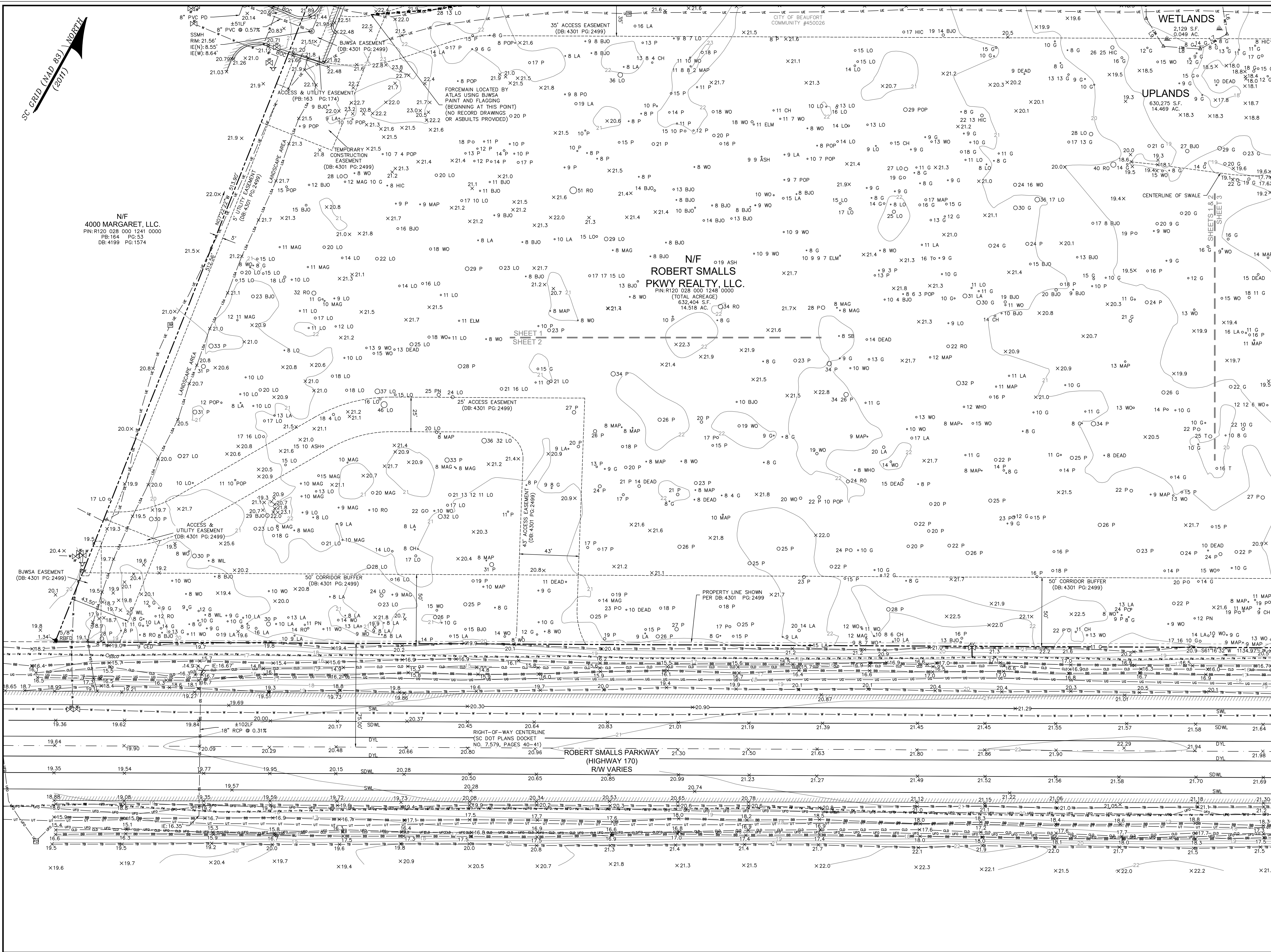
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N/F
4000 MARGARET, LLC.
PIN:R120 028 000 1241 0000
PB:164 PG:53
DB:4199 PG:1574

N/F
ROBERT SMALLS
PKWY REALTY, LLC.
PIN:R120 028 000 1248 0000
(TOTAL ACRES)
632,404 S.F.
14,518 AC.

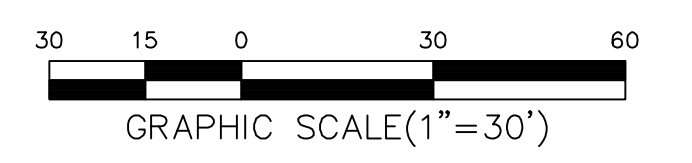
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SHEET 2

SC CRD (NAD 83) NORTH (2011)



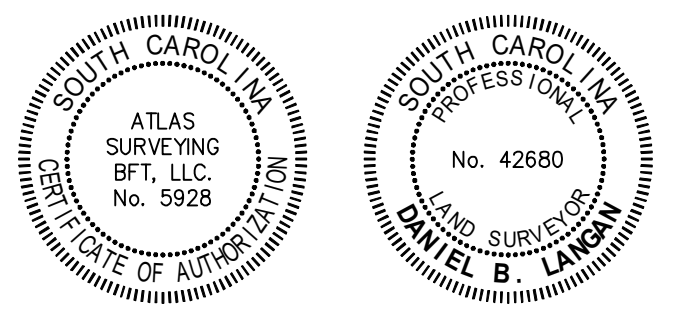
- NOTES
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- REFERENCES
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 2. PB:103 PG:81
 3. PB:7 PG:153
 4. DB:4338 PG:2565
 5. SC DOT PLANS, DOCKET NO. 7.579, PAGES 40-41)



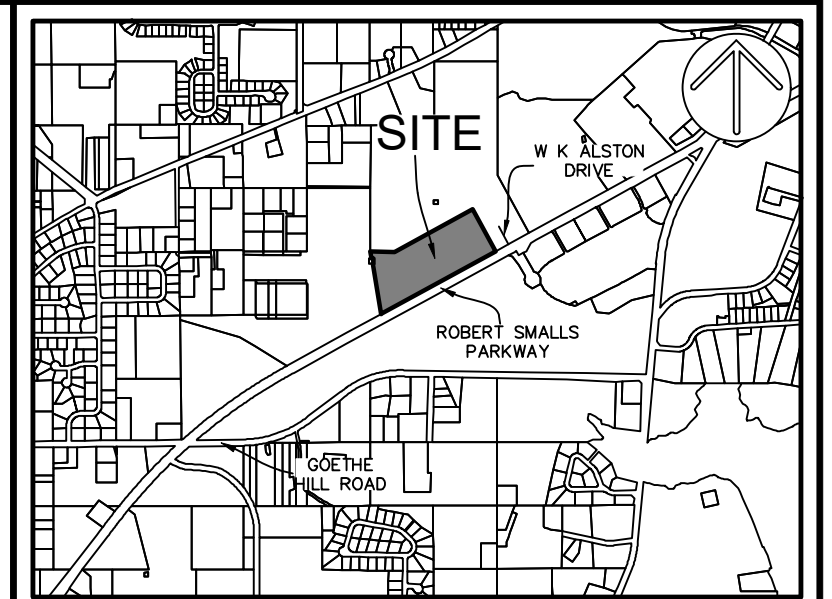
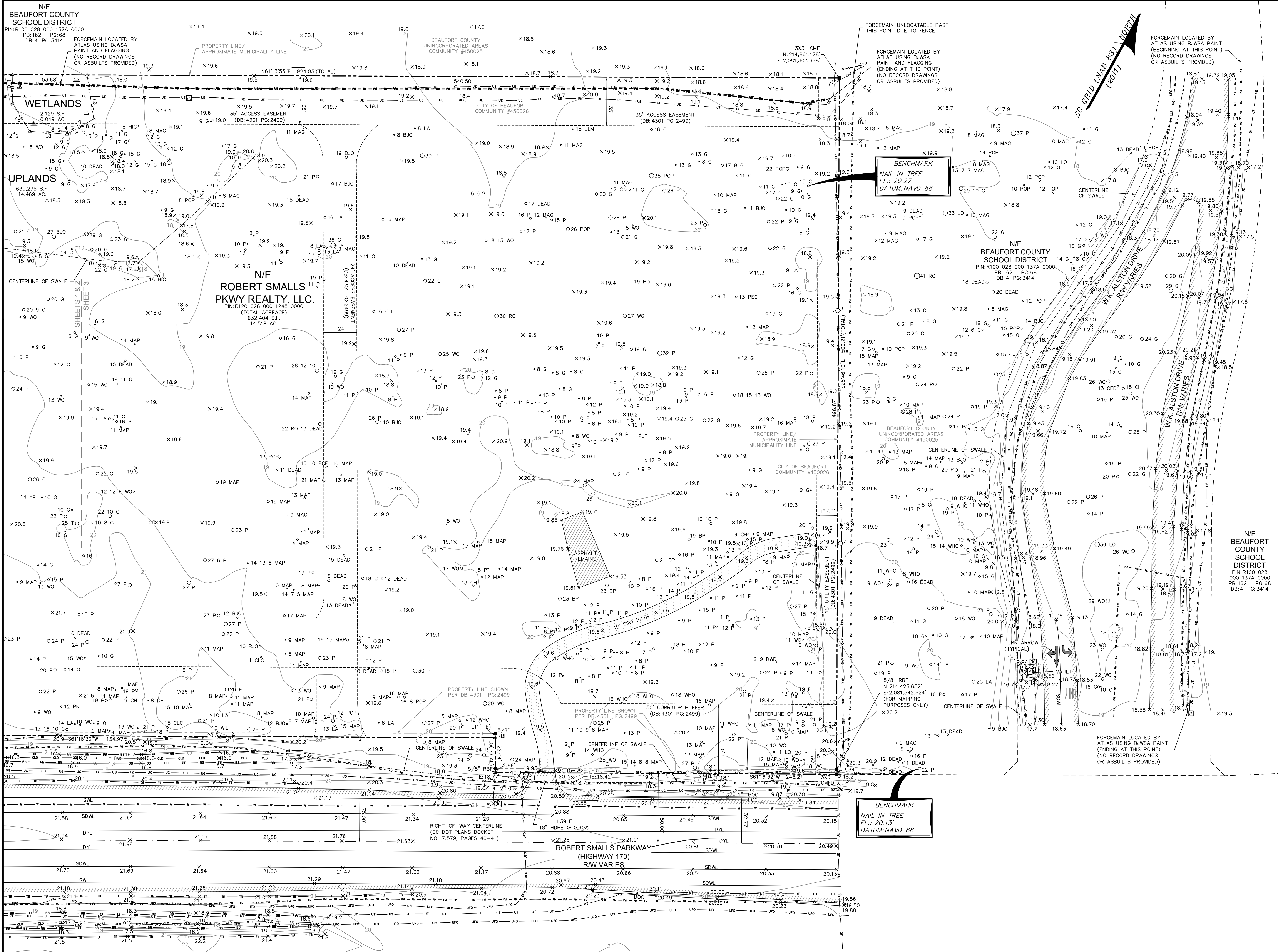
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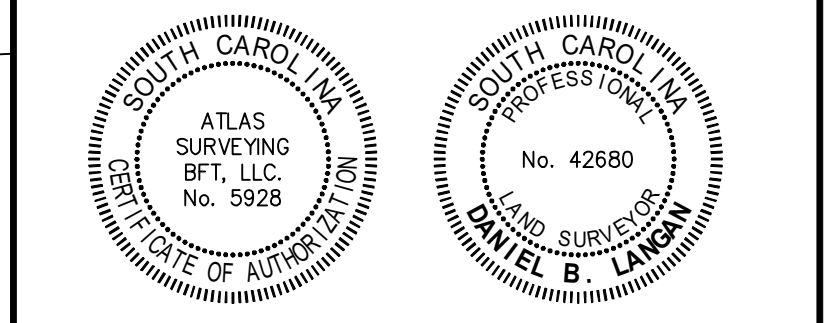
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PROJECT LOCATION



P.O. BOX 381, BLUFFTON, SOUTH CAROLINA 29910
 PH (843) 837-5250 / FAX (843) 837-2558
 WWW.WARDEDWARDS.COM

VICINITY MAP

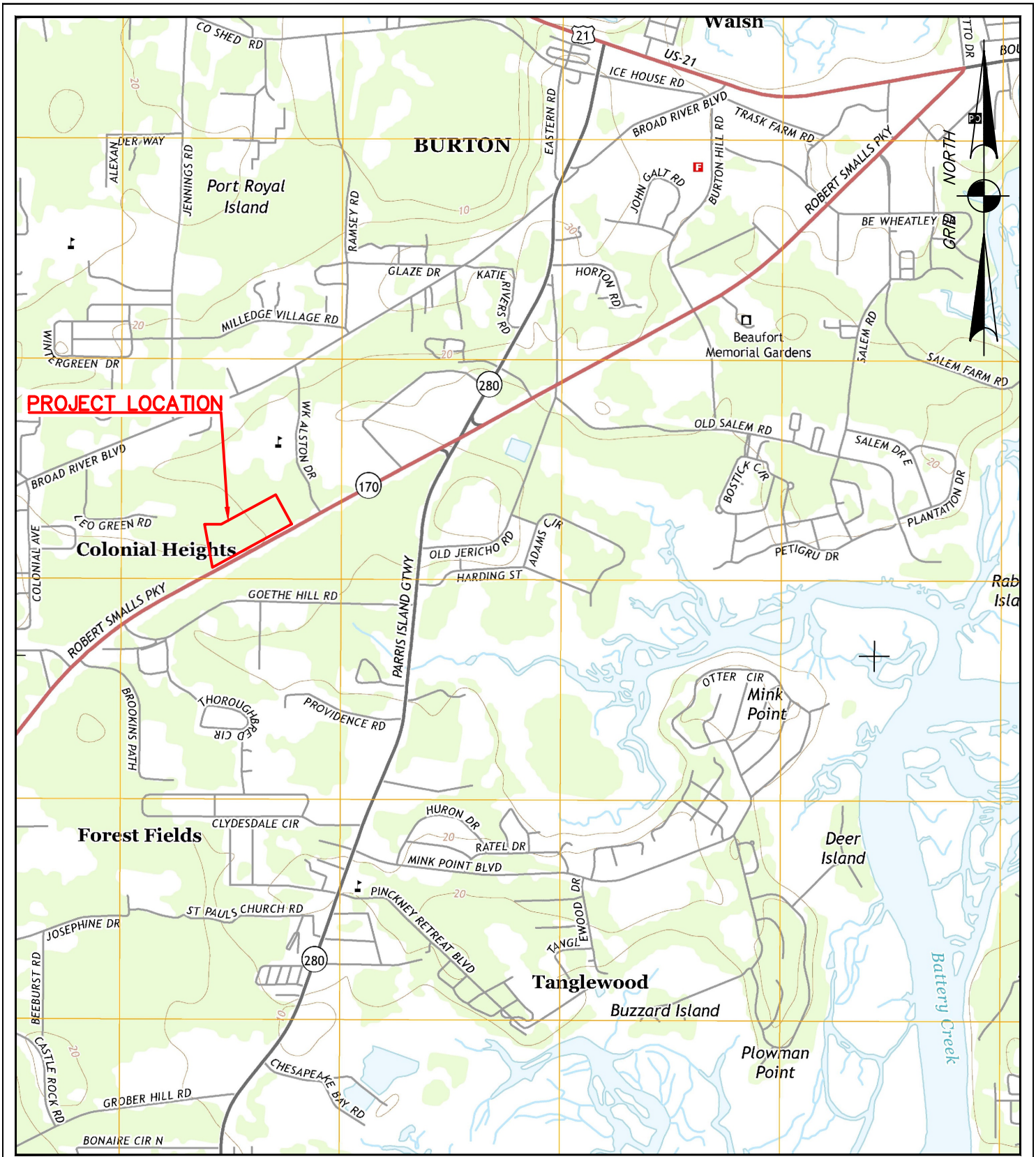
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LOCATION: CITY OF BEAUFORT, SC

DATE: 03/25/2025

PROJECT #: 230643

SCALE: 1"=500'



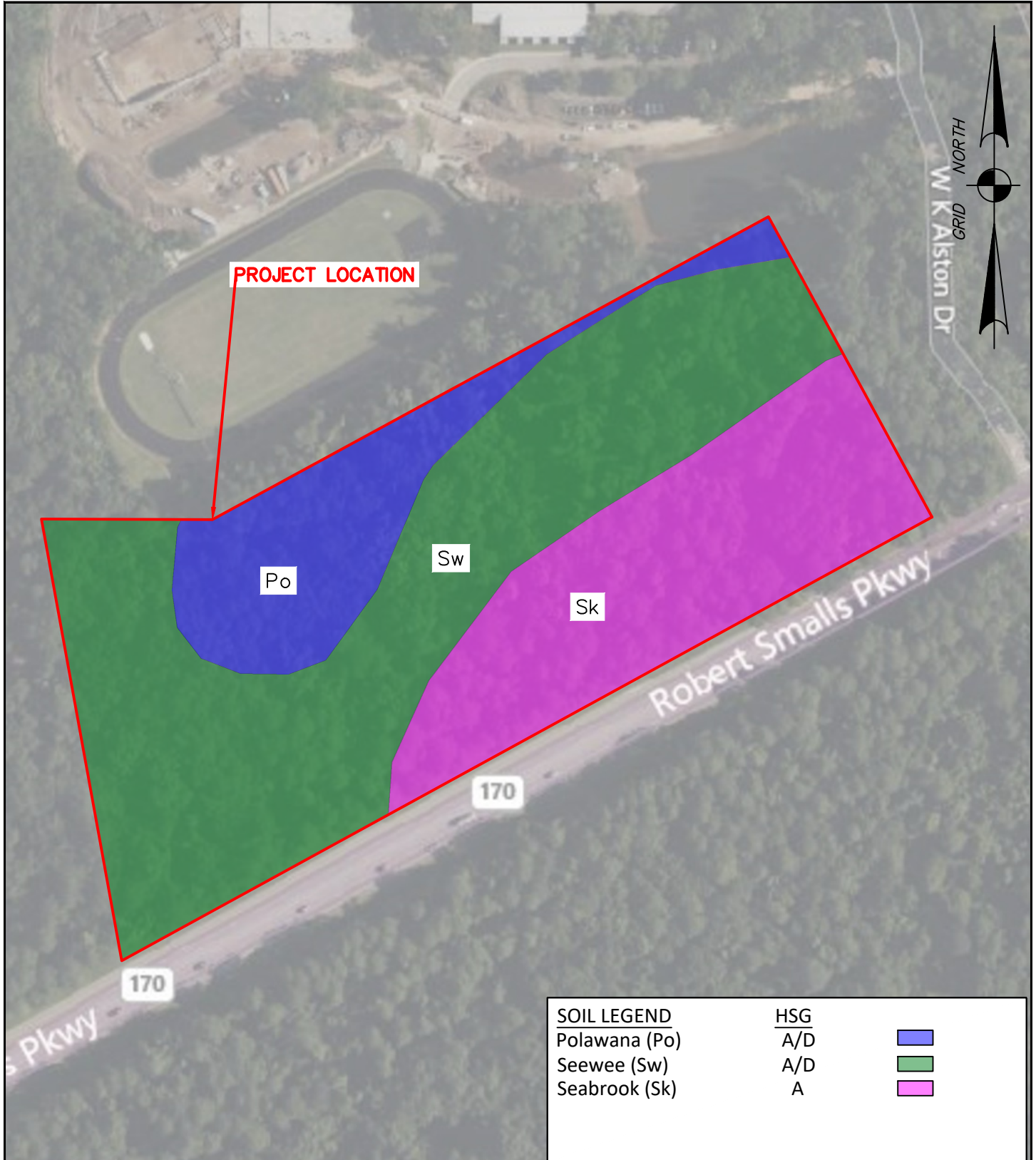
**Ward
Edwards**
ENGINEERING
P.O. BOX 381, BLUFFTON, SOUTH CAROLINA 29910
PH (843) 837-5250 / FAX (843) 837-2558
WWW.WARDEDWARDS.COM

QUAD MAP

TMS #R120 028 000 1248 0000

LOCATION: CITY OF BEAUFORT, SC
DATE: 03/25/2025
PROJECT #: 230643

SCALE: 1"=2000'



SOIL LEGEND		HSG
Polawana (Po)		A/D
Seewee (Sw)		A/D
Seabrook (Sk)		A



Ward Edwards
ENGINEERING

P.O. BOX 381, BLUFFTON, SOUTH CAROLINA 29910
PH (843) 837-5250 / FAX (843) 837-2558
WWW.WARDEDWARDS.COM

SOILS MAP

TMS #R120 028 000 1248 0000

LOCATION: CITY OF BEAUFORT, SC
DATE: 03/25/2025
PROJECT #: 230643

SCALE: 1"=200'

NOTES TO USERS

The map is for use in administering the National Flood Insurance Program. It does not necessarily identify all areas subject to flooding, particularly from local drainage sources of small size. The community map repository should be consulted for possible updated or additional information.

To obtain more detailed information in areas where Base Flood Elevations (BFEs) and/or floodways have been determined, users are encouraged to consult the Flood Profiles and Floodway Data and/or Summary of Stillwater Elevations tables contained within the Flood Insurance Study (FIS) report that accompanies this FIRM. Users should be aware that BFEs shown on the FIRM represent rounded whole-foot elevations. These BFEs are intended for flood insurance rating purposes only and should not be used as the sole source of flood elevation information. Accordingly, flood elevation data presented in the FIS report should be utilized in conjunction with the FIRM for purposes of construction and/or floodplain management.

Coastal Base Flood Elevations (CBFEs) shown on this map apply only to landward of 0.9 North American Vertical Datum of 1988 (NAVD 88). Users of this FIRM should be aware that coastal flood elevations are also provided in the Summary of Stillwater Elevations table in the Flood Insurance Study report for the jurisdiction. Elevations shown in the Summary of Stillwater Elevations table should be used for construction and/or floodplain management purposes when they are higher than the elevations shown on this FIRM.

Boundaries of the floodways were computed at cross sections and interpolated between cross sections. The floodways were based on hydraulic considerations with regard to requirements of the National Flood Insurance Program. Floodway widths and other pertinent floodway data are provided in the Flood Insurance Study report for this jurisdiction.

Certain areas not in Special Flood Hazard Areas may be protected by flood control structures. Refer to Section 2.4 "Flood Protection Measures" of the Flood Insurance Study report for information on flood control structures for this jurisdiction.

The projection used in the preparation of this map was State Plane South Carolina 1983-2000. The horizontal datum was NAD83 (1983) spheroid. Differences in datum, spheroid, projection or State Plane zones used in the production of FIRMs for adjacent jurisdictions may result in slight positional differences in map features across jurisdiction boundaries. These differences do not affect the accuracy of this FIRM.

Point elevations on this map are referenced to the North American Vertical Datum of 1988. These flood elevations must be compared to structure and ground elevations referenced to the same vertical datum. For information regarding conversion between the National Geodetic Vertical Datum of 1929 and the North American Vertical Datum of 1988, visit the National Geodetic Survey website at <http://www.ngs.noaa.gov> or contact the National Geodetic Survey at the following address:

NGS Information Services
NOMA, NANS312
National Geodetic Survey
SSMARC #9022
1315 East-West Highway
Silver Spring, Maryland 20910-3282
(301) 713-3242

To obtain current elevation, description, and/or location information for bench marks shown on this map, please contact the Information Services Branch of the National Geodetic Survey at (301) 713-3242 or visit its website at <http://www.ngs.noaa.gov>.

Base map information shown on this FIRM was provided in digital format by the Beaufort County GIS Department dated 2016.

This map reflects more detailed and up-to-date stream channel configurations than those shown on the previous FIRM for this jurisdiction. The floodplains and floodways that were transferred from the previous FIRM have been adjusted to conform to the new stream channel configurations. As a result, the Flood Profiles and Floodway Data tables in the Flood Insurance Study report (which contains authoritative hydraulic data) may reflect stream channel distances that differ from what is shown on this map.

Corporate limits shown on this map are based on the best data available at the time of publication. Because changes due to annexations or de-annexations may have occurred after this map was published, map users should contact appropriate community officials to verify current corporate limit locations.

Please refer to the separately printed Map Index for an overview map of the county showing the layout of map panels, community map repository addresses, and a Listing of Communities table containing National Flood Insurance Program dates for each community as well as a listing of the panels on which each community is located.

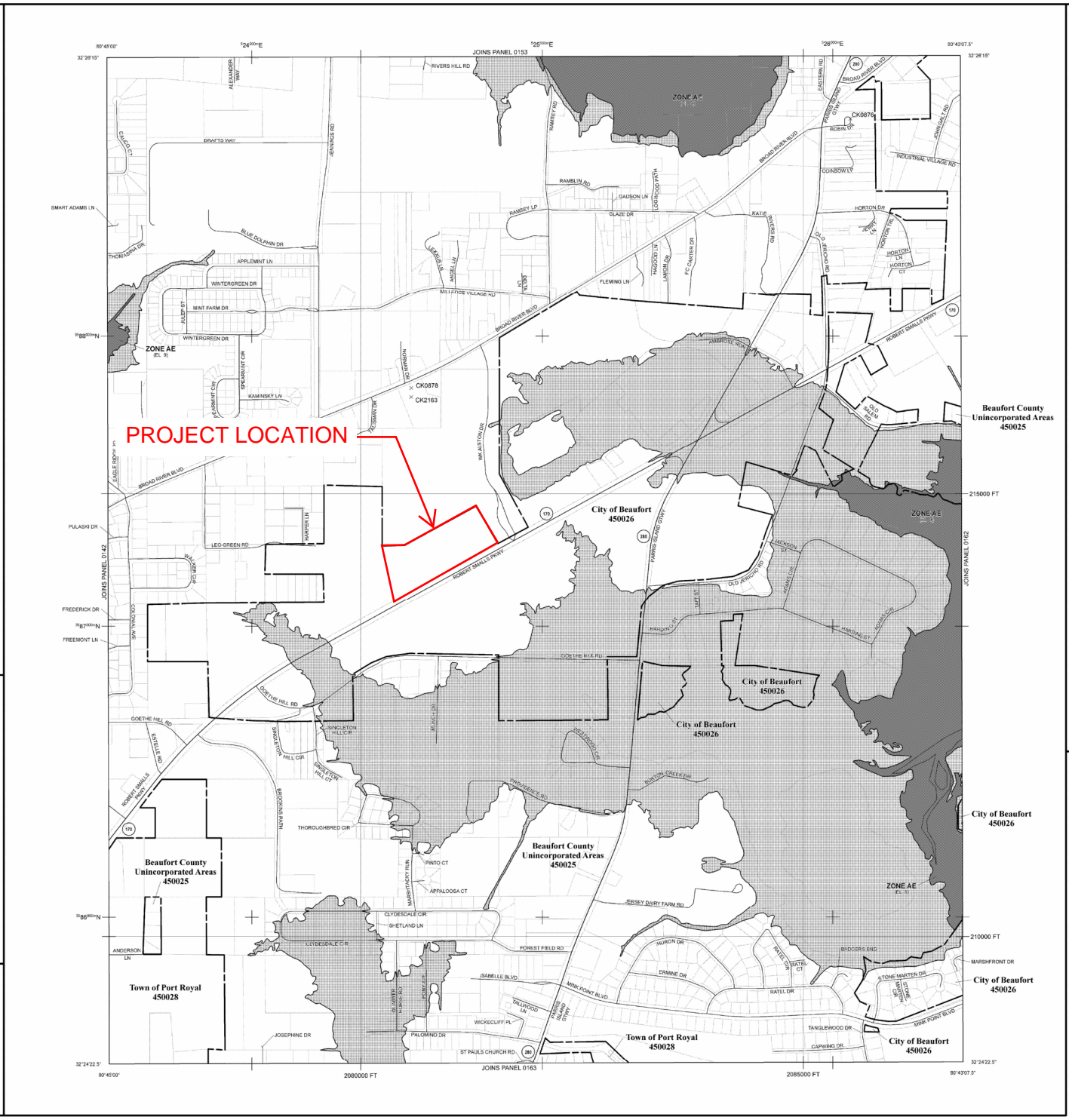
For information and questions about this map, available products associated with this FIRM including historic versions of this FIRM, how to order products or the National Flood Insurance Program in general, please call the FEMA Mapping and Insurance eXchange at 1-877-FEMA-MAP (1-877-336-2627) or visit the FEMA Map Service website at <http://www.fema.gov>. Available products may include previously issued Letters of Map Change, a Flood Insurance Report, and/or digital versions of this map. Many of these products can be ordered or obtained directly from the website. Users may determine the current map date for each FIRM panel by visiting the FEMA Map Service Center website or by calling the FEMA Mapping and Insurance eXchange.

The "profile base lines" depicted on this map represent the hydraulic modeling channels that match the flood profiles in the FIS report. As a result of improved topographic data, the "profile base line", in some cases, may deviate slightly from the channel centerline or appear outside the SFHA.



This digital Flood Insurance Rate Map (FIRM) was produced through a unique cooperative partnership between the State of South Carolina and the Federal Emergency Management Agency (FEMA). The State of South Carolina has implemented a long term program of floodplain management to decrease the costs associated with flooding. This is demonstrated by the state's commitment to map floodplain areas at the local level. As a part of this effort, the state of South Carolina has joined in a Cooperative Technical State agreement with FEMA to produce and maintain this digital FIRM.

<http://www.dnr.state.sc.us/>



LEGEND

SPECIAL FLOOD HAZARD AREAS SUBJECT TO INUNDATION BY THE 1% ANNUAL CHANCE FLOOD

The 1% annual chance flood (100-year flood), also known as the base flood, is the flood that has a 1% chance of being equaled or exceeded in any given year. The Special Flood Hazard Area is the area subject to flooding by the 1% annual chance flood. Areas of Special Flood Hazard include Zones A, AE, AH, AO, A99, V, VE, and VE1. The Base Flood Elevation is the water-surface elevation of the 1% annual chance flood.

- ZONE A** No Base Flood Elevations determined.
- ZONE AE** Base Flood Elevations determined.
- ZONE AH** Flood depths of 1 to 3 feet (usually areas of ponding); Base Flood Elevations determined.
- ZONE AO** Flood depths of 1 to 3 feet (usually sheet flow on sloping terrain); average depths determined. For areas of alluvial fan flooding, velocities also determined.
- ZONE AA** Special Flood Hazard Areas formerly protected from the 1% annual chance flood by a flood control system that was subsequently abandoned. Zone AA indicates that the former flood control system is being restored to provide protection from the 1% annual chance or greater flood.
- ZONE A99** Areas to be protected from 1% annual chance flood event by a Federal flood protection system under construction; no Base Flood Elevations determined.
- ZONE V** Coastal flood zone with velocity hazard (wave action); no Base Flood Elevations determined.
- ZONE VE** Coastal flood zone with velocity hazard (wave action); Base Flood Elevations determined.

FLOODWAY AREAS IN ZONE AE

The floodway is the channel of a stream plus any adjacent floodplain areas that must be kept free of encroachments so that the 1% annual chance flood can be carried without substantial increases in flood heights.

OTHER FLOOD AREAS

- ZONE X** Areas of 0.2% annual chance flood; areas of 1% annual chance flood with average depths of less than 1 foot or with drainage areas less than 1 square mile; areas are protected by levees from 1% annual chance flood.

OTHER AREAS

- ZONE D** Areas determined to be outside the 0.2% annual chance floodplains.
- ZONE U** Areas in which flood hazards are undetermined, but possible.

BOUNDARIES

- Floodplain boundary
- Zone D boundary
- Boundary dividing Special Flood Hazard Area Zones and different Base Flood Elevations, flood depths, or flood velocities
- Base Flood Elevation line and value; elevation in feet*
- Base Flood Elevation value where uniform within zone; elevation in feet*

* Referenced to the North American Vertical Datum of 1988

TRANSFERS

- Geographic coordinates referenced to the North American Datum of 1983 (NAD 83); Western Hemisphere
- 1000-meter Universal Transverse Mercator grid ticks, zone 17
- 3000-foot grid values; South Carolina State plane coordinate system (SPSCONE = 3900); Lambert projection
- Bench mark (see explanation in notes to users section of this FIRM panel)
- M 1.5
- River Mile

MAP REPOSITORIES

Refer to Map Repository table on Map Index

EFFECTIVE DATE OF COUNTWIDE FLOOD INSURANCE RATE MAP
March 23, 2021

EFFECTIVE DATES OF REVISIONS TO THIS PANEL

For community map revision history prior to countywide mapping, refer to the Community Map History table located in the Flood Insurance Study report for this jurisdiction.

To determine if flood insurance is available in this community, contact your insurance agent or call the National Flood Insurance Program at 1-800-438-6620.

MAP SCALE 1" = 500'

0 250 500 750 1,000 FEET
0 150 300 METERS

NFIP PANEL 0161G

FIRM

FLOOD INSURANCE RATE MAP

BEAUFORT COUNTY, SOUTH CAROLINA AND INCORPORATED AREAS

PANEL 161 OF 506
(SEE MAP INDEX FOR FIRM PANEL LAYOUT)

CONTAINS:

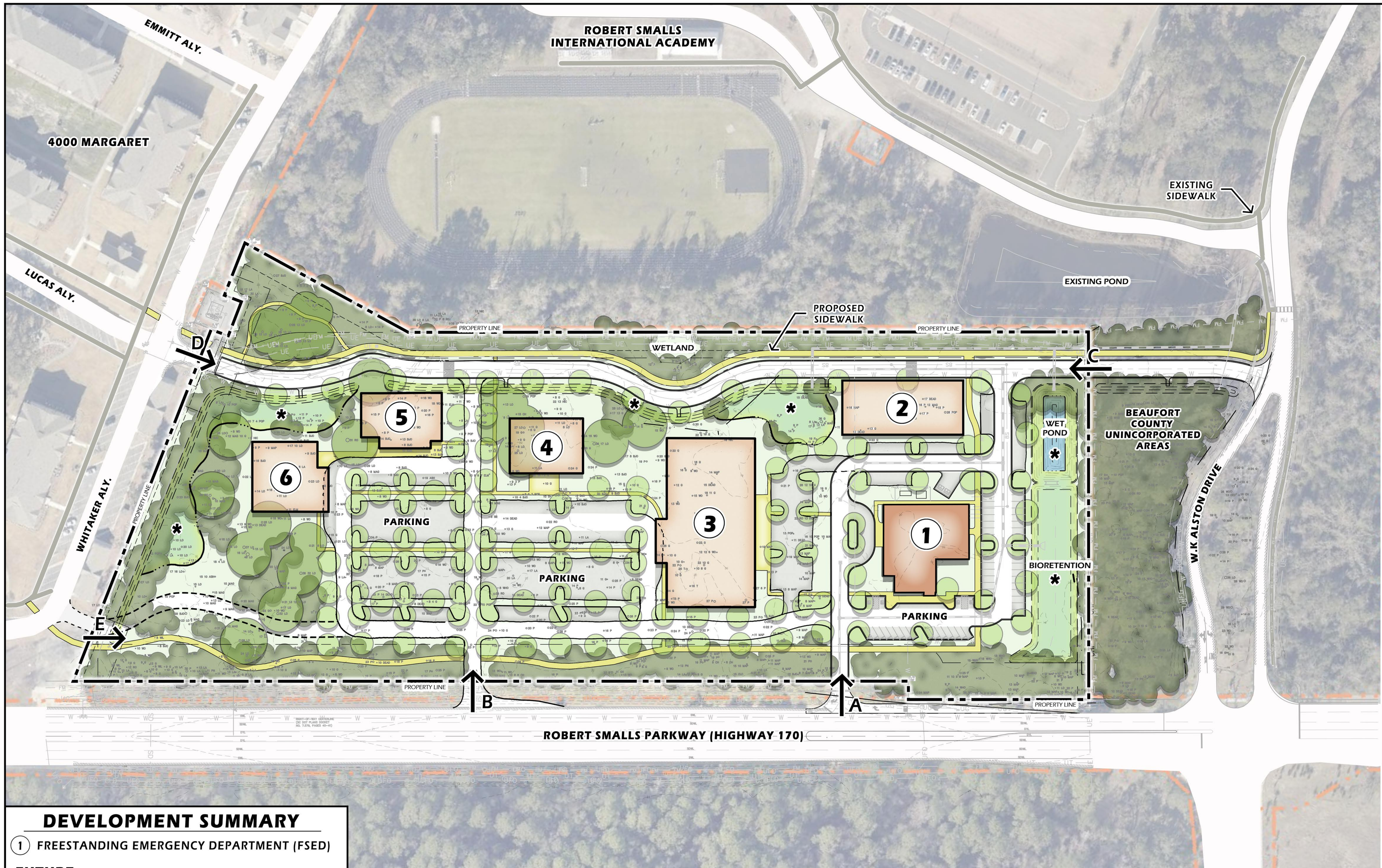
COMMUNITY	NUMBER	PANEL	SUFFIX
BEAUFORT COUNTY	450025	0161	G
BEAUFORT CITY OF	450026	0161	G
PORT ROYAL TOWN OF	450023	0161	G

NOTICE TO USER: The Map Number shown below should be used only when making map orders. The Community Number shown above should be used on insurance applications for the subject community.

MAP NUMBER
4501300161G

EFFECTIVE DATE
MARCH 23, 2021

Federal Emergency Management Agency



DEVELOPMENT SUMMARY

① FREESTANDING EMERGENCY DEPARTMENT (FSED)

FUTURE

② MEDICAL/OFFICE

③ MEDICAL/OFFICE ±60,000 SF (2 FLOORS)

④ MEDICAL/OFFICE ±5,400 SF

⑤ MEDICAL/OFFICE ±5,400 SF

⑥ MEDICAL/OFFICE ±7,200 SF

* STORMWATER MANAGEMENT BMP

Winters Jones Keefe
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 landscape architecture
 land planning
 www.wjkltd.com
 28 Promenade Street, Suite 201 | Burlington, South Carolina | 29910 | ph 843.797.7411

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 THIS SHEET TO SCALE AT: 24"X36"

SITE DEVELOPMENT PLANS
 FOR
NOVANT BEAUFORT FSED
 396 ROBERT SMALLS PARKWAY
 BEAUFORT, SC

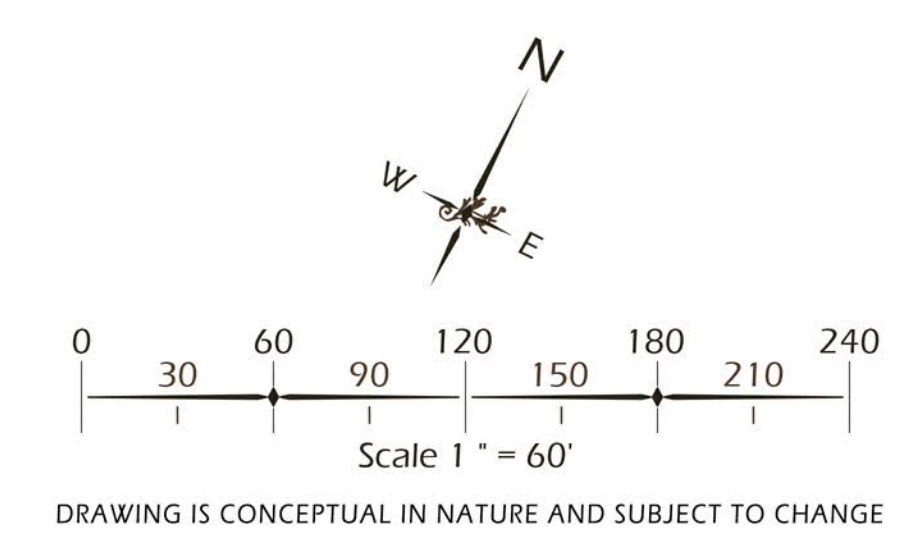
DATE: MAR. 18, 2026
 PROJECT NO.: XXXXX.XX
 DRAWN BY: ER
 CHECKED BY: DK

PRELIMINARY
 SUBMITTAL PLAN,
 NOT FOR
 CONSTRUCTION

REVISIONS:

DRAWING TITLE
 SKETCH MASTER PLAN

DRAWING NUMBER
L13



SITE DEVELOPMENT PLANS
 FOR
NOVANT BEAUFORT FSED
 396 ROBERT SMALLS PARKWAY
 BEAUFORT, SC

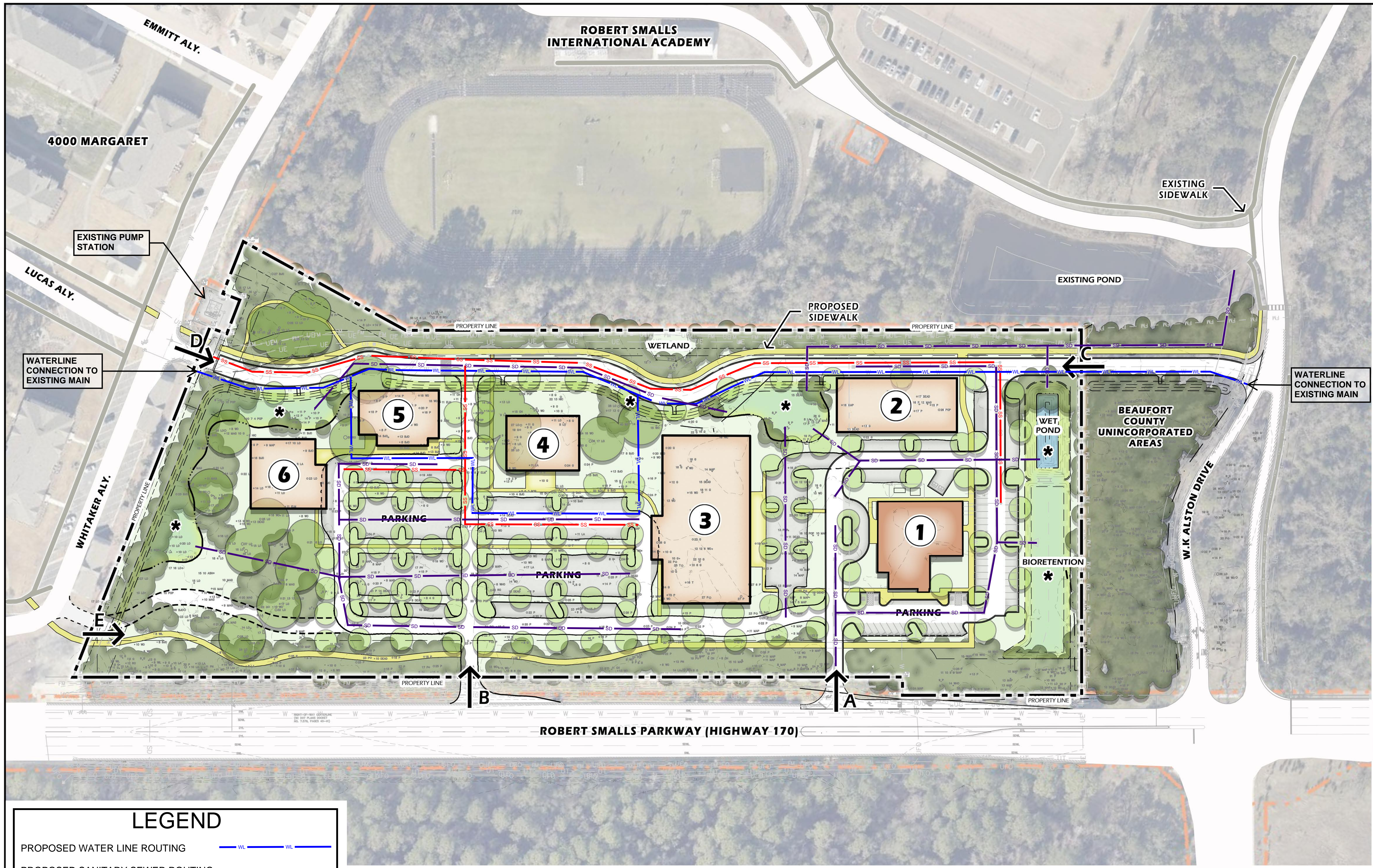
DATE: MAR. 18, 2026
 PROJECT NO.: XXXXXX
 DRAWN BY: ER
 CHECKED BY: DK

**PRELIMINARY
 SUBMITTAL PLAN,
 NOT FOR
 CONSTRUCTION**

REVISIONS:

DRAWING TITLE
SKETCH MASTER PLAN

DRAWING NUMBER
L13



LEGEND

PROPOSED WATER LINE ROUTING — WL — WL —

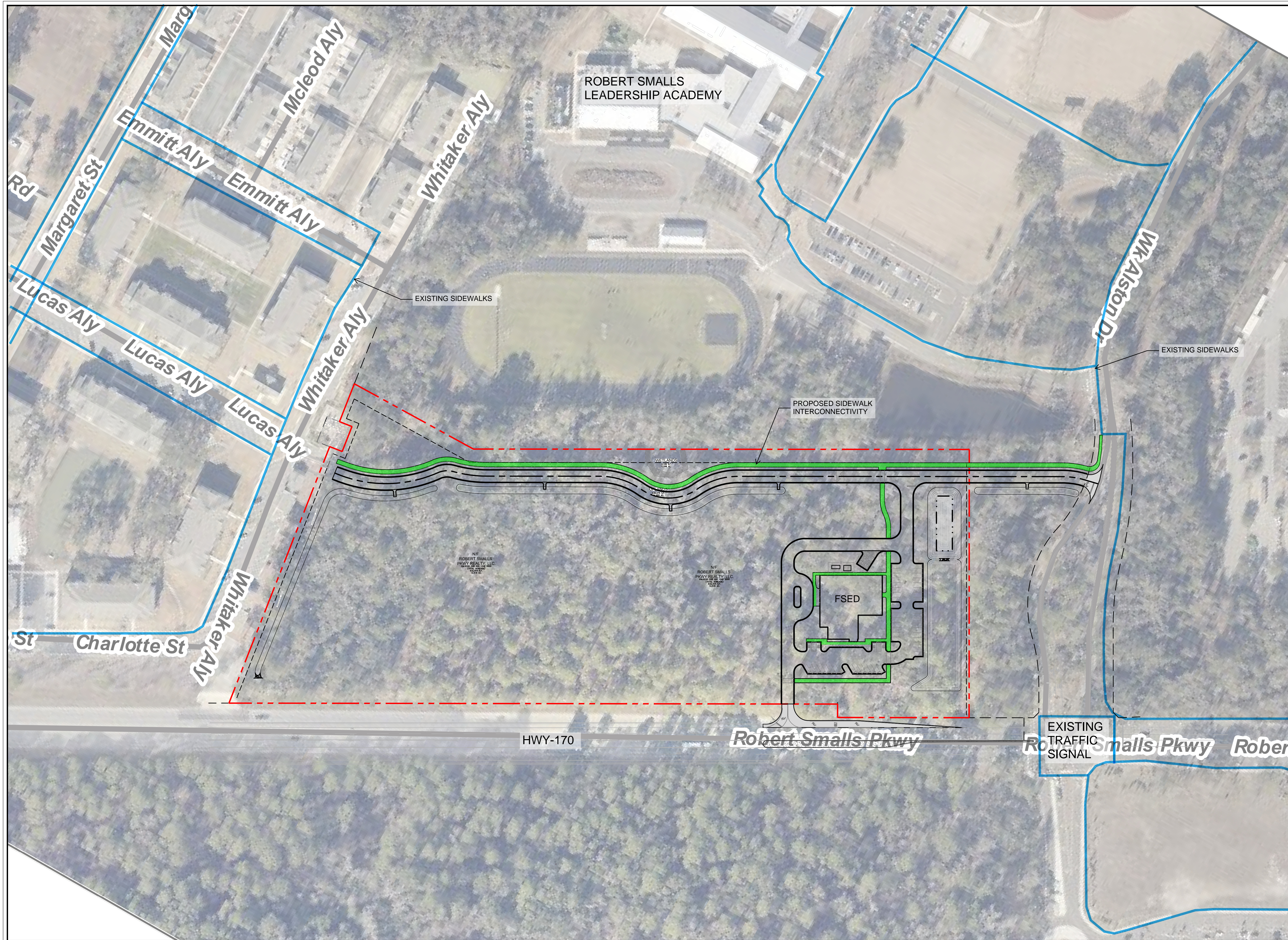
PROPOSED SANITARY SEWER ROUTING — SS — SS —

PROPOSED STORM DRAIN ROUTING — SD — SD —

Scale 1" = 60'

0 30 60 90 120 150 180 210 240

DRAWING IS CONCEPTUAL IN NATURE AND SUBJECT TO CHANGE



VICINITY MAP
Not To Scale

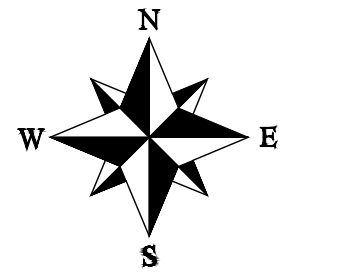
OVERALL EXHIBIT

BEAUFORT FSED

CITY OF BEAUFORT, SC

PREPARED FOR:
NOVANT HEALTH

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Scale: 1" = 80' Feet

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WWW.WARDEDWARDS.COM

PROJECT #:	230643
DATE:	03/10/26
PREPARED BY:	LYJ
SHEET NUMBER:	1 OF 1

RE: BJWSA #2026-004 - Beaufort FSED and PCC Capacity Confirmation

From Matthew Michaels <[redacted]>
Date Mon 10/27/2025 1:38 PM
To Leah Johnston <[redacted]>
Cc Conor Blaney <[redacted]>; Debbie Findley <[redacted]>; Sheila Sulak <[redacted]>; Jason Quick <[redacted]>; Development <[redacted]>

Good Afternoon,

This site will discharge its gravity sewer to the existing SP-75 pump station that was built by the new 4000 Margaret apartments. The pump station was built to handle both the apartments and the property FSED is proposed on. SP-75 has the capacity for FSED. As for the water, based on what Ward Edwards has provided in their calculations, it appears the existing water system can provide the required flows and pressures for the site.

Please let me know if you have any questions.
Thanks,

Matthew Michaels, PE
Development Program Manager

BJWSA
6 Snake Road
Okatie, SC 29909
Phone: (843) 987-8094
Fax: (843) 548-4168
<http://www.bjwsa.org>

Our Mission:

Provide quality water and wastewater services to our current and future customers in the Lowcountry.

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From: Leah Johnston <[redacted]>
Sent: Wednesday, October 22, 2025 2:15 PM
To: Development <[redacted]>; Jason Quick <[redacted]>; Matthew Michaels <[redacted]>; Marquea Beaton <[redacted]>; Mahathi Bhooshi <[redacted]>; Jordan Silva <[redacted]>
Cc: Conor Blaney <[redacted]>; Debbie Findley <[redacted]>; Sheila Sulak <[redacted]>

1/26/2026

Subject: BJWSA #2026-004 - Beaufort FSED and PCC Capacity Confirmation

This message originated from an **External Source**. Please use proper judgment and caution when opening attachments, clicking links, or responding to this email.

Good afternoon,

The City of Beaufort has requested verification from BJWSA confirming that sufficient water and sewer capacity is currently available to serve the Beaufort FSED & PCC project located at 396 Robert Smalls Parkway. They noted that this verification needs to confirm actual capacity rather than be in the form of a standard "will serve" letter.

Could you please provide documentation or correspondence confirming that the necessary infrastructure capacity exists to support this project?

Thanks so much,
Leah Johnston



Leah Johnston
EIT, CEPSCI
Designer
m.johnston@wardedwards.com



Agenda

BEAUFORT COUNTY BOARD OF EDUCATION

District Educational Services Center

Media Center

February 17, 2026

5:00 – 9:15 pm Public Session

Hybrid Video Conferencing

This meeting will be broadcast by the County Channel

NOTICE

In accordance with new safety and security guidelines, bags, backpacks and purses are prohibited by the public at Beaufort County Board of Education Meetings.

Request for Public Comments participation will be accepted between 4:00pm–4:30pm by sending an email with your name, phone #, and topic to robyn.cushingberry@beaufort.k12.sc.us.

You will receive a phone call during Public Comments where you will be able to speak and address the Board for a maximum of three minutes on issues within the Board’s domain. The forum will be limited to 30 minutes.

If you are in person, you may address the Board for a maximum of three minutes on issues within the Board’s domain. Each speaker must fill out a Public Comment Card. Please see the Board Clerk for a card.

Request for the second Public Comments will be accepted between 7:00 – 7:30pm in the same manner as above. The second Public Comments will take place prior to adjournment no later than 9:00pm. The second forum will be limited to 15 minutes.

In the event the Board has not finished addressing all items listed on the February 17, 2026, Agenda, the Board will reconvene at 5:00 pm on February 18, 2026, the next business day to finish the agenda items.

<i>Mission: The Beaufort County School District, through a personalized learning approach, will prepare graduates who compete and succeed in an ever-changing global society and career marketplace.</i>	Coherent Governance Reference
5:00 p.m. Public Session Call to Order	Policy GC-2.20
Approval of Agenda	
Pledge of Allegiance, Moment of Silence, Statement of Media Notification	
First Public Comments - See information above	Policy GC-2. 23.1.IV
Chair’s Report - Board Development and Work Session	
Board Business Action N/A	
Committee Reports <ul style="list-style-type: none"> • Academics • Finance • Operations <ul style="list-style-type: none"> ➤ Recommended motion for the full Board approval for the Superintendent to negotiate an easement agreement with Novant Health to include teacher housing, teaching partnership and to return to the Board with the final agreement for approval. 	
Superintendent’s Report Goal #1 Student Achievement <ul style="list-style-type: none"> • Local Board Approved Courses 	

Goal #2 Highly Qualified Teachers and Administrators N/A	
Goal #3 Fiscal Responsibility N/A	
Goal #4 Policy Development and Implementation N/A	
Goal #5 Supervision and Evaluation of the Superintendent N/A	
Other Matters N/A	
Points of Celebration <ul style="list-style-type: none"> • Student of the Month for Northern & Southern Beaufort County <ul style="list-style-type: none"> ➤ October – Robert Smalls Leadership Academy, 8th grade student Mahki Jacobs ➤ December - Lady’s Island Middle School, 6th grade student Cole Porter Okatie Elementary School, 4th grade student Cristian Alveranga ➤ January – Whale Branch Middle School, 7th grade student Princeton Henry Okatie Elementary School, Kindergarten student Wren Thrasher • Blue Ribbon Affiliate – BCSD Parents as Teachers Program 	
Consent Agenda <ul style="list-style-type: none"> • OE-8 Facilities • OE-18 Monitoring Report Food Service 	
Future Agenda Topics	
Announcements	
Second Public Comments – See information above.	Policy GC-2. 23.I.IV
Executive Session <ul style="list-style-type: none"> • Discussion of negotiations incident to proposed Amendment #1 with Thompson Turner Construction Co., Inc. for Lady’s Island Middle School Early Release Package for Site Work 2023 Referendum Scope Guaranteed maximum Price (GMP) Proposal pursuant to S.C. Code Ann. 30-4-70(a)(2). • Receipt of legal advice relating to pending legal claims covered by the attorney client privilege pursuant to S.C. Code Ann. Section 30-4-70(a)(2). 	
Public Session Action as Necessary or Appropriate Regarding Matters Discussed in Executive Session	
Adjournment Adjournment until next business day: if the Board has not finished addressing all items listed on the February 17, 2026, Agenda, the Board will adjourn at 9:15 p.m. and will reconvene at 5:00 p.m. on February 18,2026.	

The Beaufort County Board of Education and the Beaufort County School District can be accessed at: beaufortschools.net

Next Scheduled School Board Meeting – March 3 2026, Board Meeting. This may be subject to change so please check our website for the most updated information.

BCTV can be found on cable channels Sparklight (formerly Hargray) - channel 9 and 417; Comcast - channel 2; Spectrum - channel 1304 BCTV also streams live on www.BeaufortCounty.tv and through the FREE BCTV APP available on ROKU, Apple TV, Android TV and Amazon Fire.

Beaufort FSED & PCC

Traffic Impact Analysis

Beaufort, South Carolina

Prepared for

Ward Edwards Engineering

Prepared by

Kimley»Horn

January 2026

© Kimley-Horn and Associates, Inc.

Beaufort FSED & PCC

Traffic Impact Analysis

Beaufort, South Carolina

Prepared for

Ward Edwards Engineering

Prepared by

Kimley»Horn

January 2026

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1080 Morrison Drive, Suite 240
Charleston, South Carolina, 29403

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- B – Traffic Volume Development Worksheets
- C – Capacity Analysis Worksheets
- D – Turn Lane Warrant Analysis Worksheets

Executive Summary

The purpose of this Traffic Impact Analysis (TIA) is to review vehicular traffic impacts as a result of the proposed Beaufort FSED development. The development is proposed to be located at the northwest quadrant of the intersection of SC 170 (Robert Smalls Parkway) at WK Alston Drive and is planned to consist of the following:

- 96,200 SF of Medical-Office Building Space

The development is anticipated to be completed in 2030. Based on the site layout, the proposed development will be accessed via the following site accesses:

- One full-movement driveway with one ingress and one egress lane as the west leg of the proposed intersection of WK Alston Drive at Site Access #1
- One right-in/right-out access only driveway with one ingress and one egress lane as the north leg of the proposed intersection of SC 170 (Robert Smalls Parkway) at Site Access #2
- One right-in/right-out access only driveway with one ingress and one egress lane as the north leg of the proposed intersection of SC 170 (Robert Smalls Parkway) at Site Access #3
- One full-movement driveway with one ingress and one egress lane as the proposed east leg of the intersection of Whitaker Aly at Lucas Aly and Site Access #4

This TIA evaluates the traffic operations under 2025 Existing conditions, 2030 No-Build conditions, and 2030 Build conditions during the AM and PM peak hours at the following intersections:

- Whitaker Aly at Lucas Aly and Site Access #4
- SC 170 (Robert Smalls Parkway) at WK Alston Drive
- WK Alston Drive at RSLA Bus Access
- WK Alston Drive at RSLA Full-Movement Access
- WK Alston Drive at RSLA Northern Access
- WK Alston Drive at Broad River Boulevard
- WK Alston Drive at Site Access #1
- SC 170 (Robert Smalls Parkway) at Site Access #2
- SC 170 (Robert Smalls Parkway) at Site Access #3

Based on the results of the traffic analysis, the proposed Beaufort FSED Development is anticipated to have impact on the surrounding road network warranting additional turn lanes as follows:

- A westbound left-turn lane with 200 feet of storage at the intersection of WK Alston Drive and Broad River Boulevard
- A northbound left-turn lane with 100 feet of storage at the intersection of WK Alston Drive and Site Access #1

1 Introduction

The purpose of this TIA is to review vehicular traffic impacts as a result of the proposed Beaufort FSED development. The development is proposed to be located at the northwest quadrant of the intersection of SC 170 (Robert Smalls Parkway) at WK Alston Drive and is planned to consist of the following:

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- One right-in/right-out access only driveway with one ingress and one egress lane as the north leg of the proposed intersection of SC 170 (Robert Smalls Parkway) at Site Access #3
- One full-movement driveway with one ingress and one egress lane as the proposed east leg of the intersection of Whitaker Aly at Lucas Aly and Site Access #4

The conceptual site plan can be seen in **Figure 1**.

This TIA evaluates the traffic operations under 2025 Existing conditions, 2030 No-Build conditions, and 2030 Build conditions during the AM and PM peak hours at the following intersections and is illustrated in **Figure 2**.

- Whitaker Aly at Lucas Aly and Site Access #4
- SC 170 (Robert Smalls Parkway) at WK Alston Drive
- WK Alston Drive at RSLA Bus Access
- WK Alston Drive at RSLA Full-Movement Access
- WK Alston Drive at RSLA Northern Access
- WK Alston Drive at Broad River Boulevard
- WK Alston Drive at Site Access #1
- SC 170 (Robert Smalls Parkway) at Site Access #2
- SC 170 (Robert Smalls Parkway) at Site Access #3

As part of the *Watercrest Apartments (Kimley-Horn 2025)* study and other development in the area, improvements were recommended along WK Alston Drive. These improvements are included in the 2030 No-Build condition and 2030 Build condition analysis and are not the responsibility of the FSED site and are listed here:

WK Alston Drive at RSLA Northern Access / Watercrest Apartments Access

- A northbound right-turn lane with 100 feet of full width storage and a 100-foot taper
- A southbound right-turn lane with 100 feet of full width storage and a 100-foot taper

- The proposed Watercrest Access was analyzed with a single ingress and a single egress lane

WK Alston Drive at RSLA Full-Movement Access

- A northbound left-turn lane with 100 feet of full width storage and a 100-foot taper

WK Alston Drive at Broad River Boulevard

- An eastbound right-turn lane with 200 feet of full width storage and a 100-foot taper

1.1 Existing Conditions

The primary roadways in the vicinity of the site are SC 170 (Robert Smalls Parkway), WK Alston Drive, and Broad River Boulevard.

SC 170 (Robert Smalls Parkway) is a five-lane, undivided local road with a posted speed limit of 45 mph in the vicinity of the FSED site. SC 170 (Robert Smalls Parkway) had an Annual Average Daily Traffic (AADT) of 20,800 vehicles per day in 2024 per SCDOT count station 07-0175.

WK Alston Drive is a two-lane, undivided minor arterial with a posted speed limit of 35 mph in the vicinity of the FSED site. SCDOT does not provide AADT data for WK Alston Drive.

Broad River Boulevard is a two-lane, undivided major collector with a posted speed limit of 45 mph in the vicinity of the FSED site. Broad River Boulevard had an AADT of 7,200 vehicles per day in 2024 per SCDOT count station 07-0251.

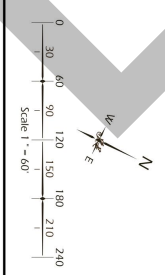
The existing geometry and traffic control for the study area is illustrated in **Figure 3**.

Figure 1 - Conceptual Site Plan

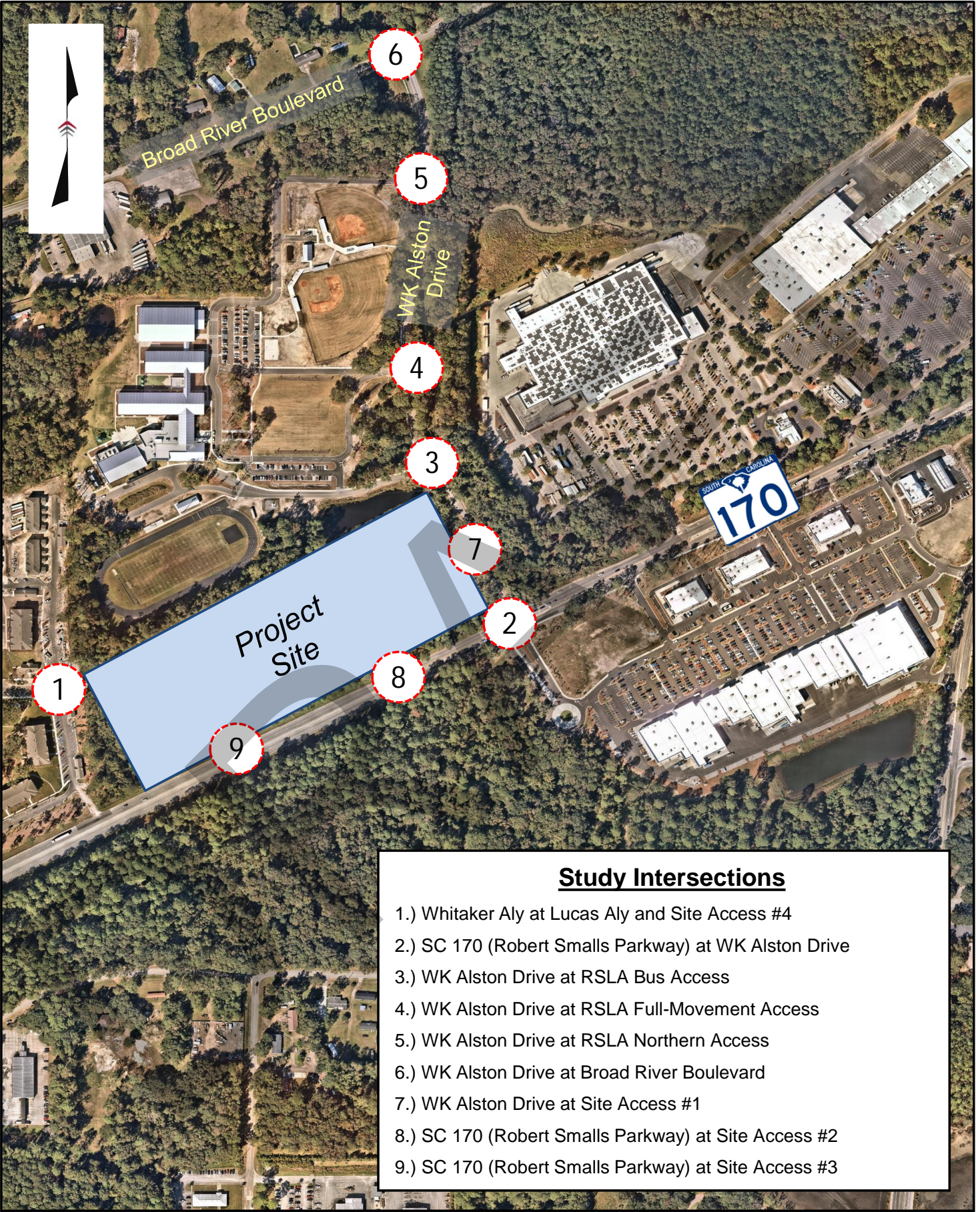


- DEVELOPMENT SUMMARY**
- 1. FSED
 - 2. PCC
 - 3. MEDICAL/OFFICE +/- 60,000 SF (12 FLOORS)
 - 4. MEDICAL/OFFICE +/- 5,400 SF
 - 5. MEDICAL/OFFICE +/- 5,400 SF
 - 6. MEDICAL/OFFICE +/- 8,000 SF
- *STORMWATER MANAGEMENT BMP

DRAFT

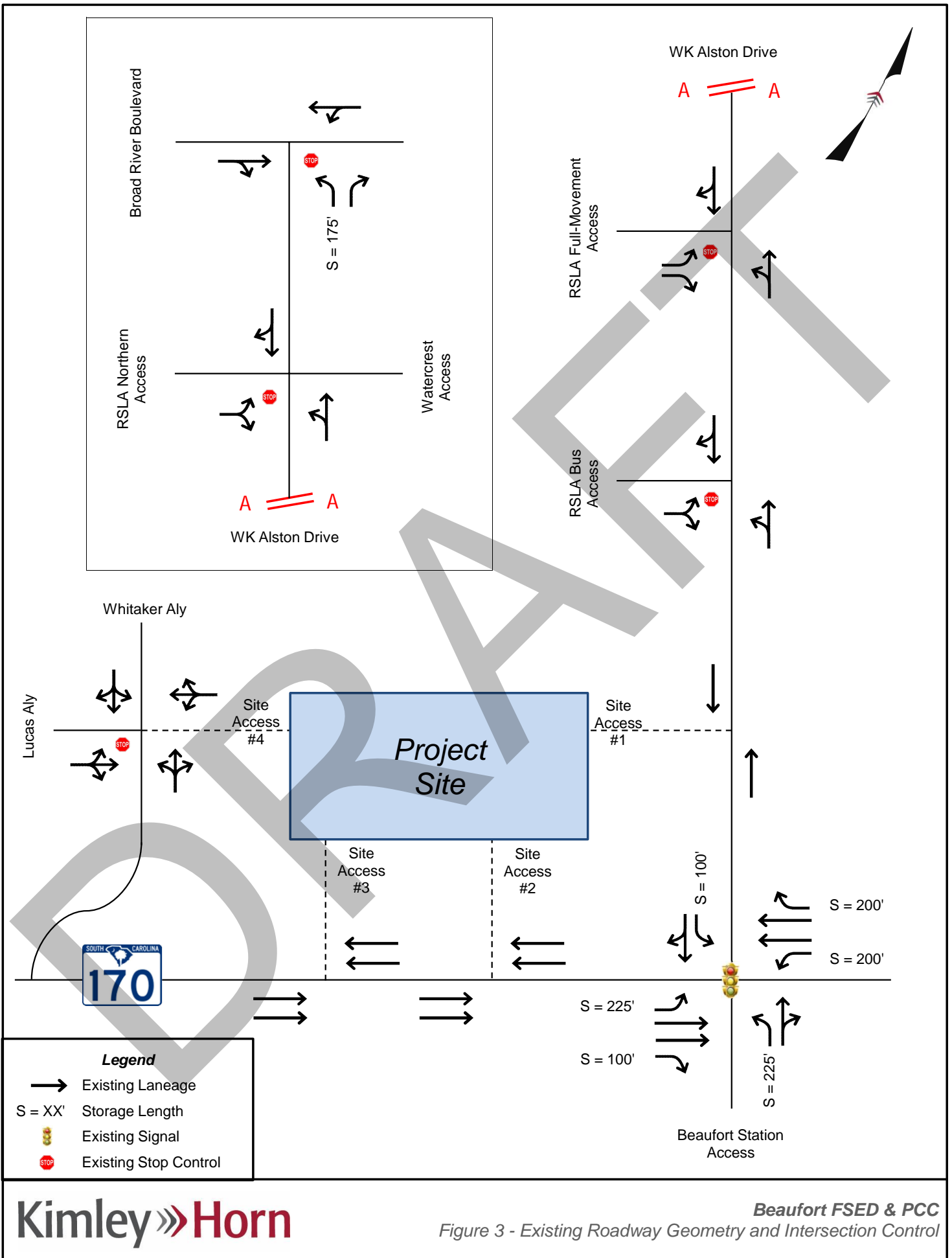


<p>DATE: JAN. 15, 2026 PROJECT NO.: 2509791 DRAWN BY: ER CHECKED BY: CK</p>	<p>PRELIMINARY SUBMITTAL PLAN, NOT FOR CONSTRUCTION</p>	<p>REVISIONS:</p>	<p>DRAWING NUMBER L13</p>	<p>©2025 WJKLD DESIGN: CONCEPT, DRAWING, SPECIES, MATERIALS, LAYOUT, AND FINISHES. VISUALIZATION AND RENDERING ARE NOT TO BE USED IN ANY FORM WITHOUT THE WRITTEN CONSENT OF WJKLD. CONSENT OF WJKLD: 01/15/2026</p>	<p>Witmer Jones Keeler Ltd. landscape architecture land planning www.wjkld.com</p>	<p>82 Promenade Street, Suite 201 Bluffton, South Carolina 29910 ph 843.757.7411</p>



Study Intersections

- 1.) Whitaker Aly at Lucas Aly and Site Access #4
- 2.) SC 170 (Robert Smalls Parkway) at WK Alston Drive
- 3.) WK Alston Drive at RSLA Bus Access
- 4.) WK Alston Drive at RSLA Full-Movement Access
- 5.) WK Alston Drive at RSLA Northern Access
- 6.) WK Alston Drive at Broad River Boulevard
- 7.) WK Alston Drive at Site Access #1
- 8.) SC 170 (Robert Smalls Parkway) at Site Access #2
- 9.) SC 170 (Robert Smalls Parkway) at Site Access #3



2 Project Traffic

2.1 Trip Generation

The trip generation rates and equations published in the *Institute of Transportation Engineers' (ITE) Trip Generation Manual, 12th Edition* were used to estimate the trip generation potential for the proposed medical development. The analysis was performed using the information provided for the following land use code (LUC):

- LUC 720 – Medical-Dental Office Building – 96,200 square-feet (SF)

For buildings designated as Free-Standing Emergency Room or Person-Centered Care, LUC 720 was used because it has more reliable and conservative data than LUC 650 – Free-Standing Emergency Room. Internal capture and pass-by trip reductions were not considered in the trip generation analysis.

The estimated trip generation for the Beaufort FSED is summarized in **Table 1**, which indicates that the development is anticipated to generate 230 trips (179 in/51 out) during the AM peak hour and 350 trips (105 in/245 out) during the PM peak hour.

Table 1 – Trip Generation Summary

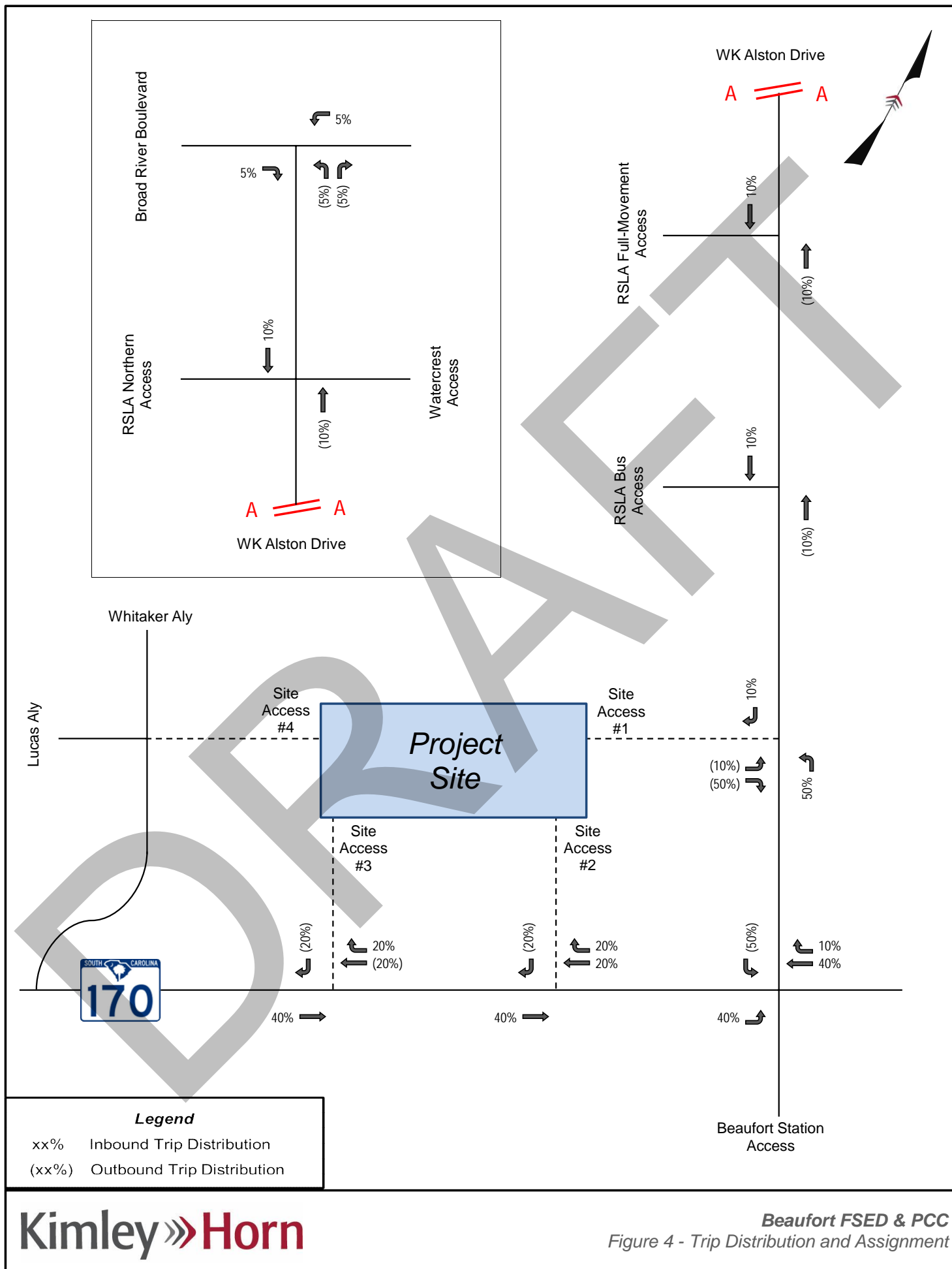
Land Use	Intensity	Units	Daily	AM Peak Hour			PM Peak Hour		
				Total	In	Out	Total	In	Out
720 – Medical-Dental Office Building	96.2	KSF	3,274	230	179	51	350	105	245
Total Net New External Trips			3,274	230	179	51	350	105	245
<u>Daily Traffic Generation</u>									
720 – Medical-Dental Office Building		ITE 720	=	T = 40.60 * (X) + (-75.15); (50% In; 50% Out)					
<u>AM Peak-Hour Traffic Generation</u>									
720 – Medical-Dental Office Building		ITE 720	=	T = 0.9 * LN (X) + (1.33); (78% In; 22% Out)					
<u>PM Peak-Hour Traffic Generation</u>									
720 – Medical-Dental Office Building		ITE 720	=	T = 3.70 * (X) + (-5.75); (30% In; 70% Out)					

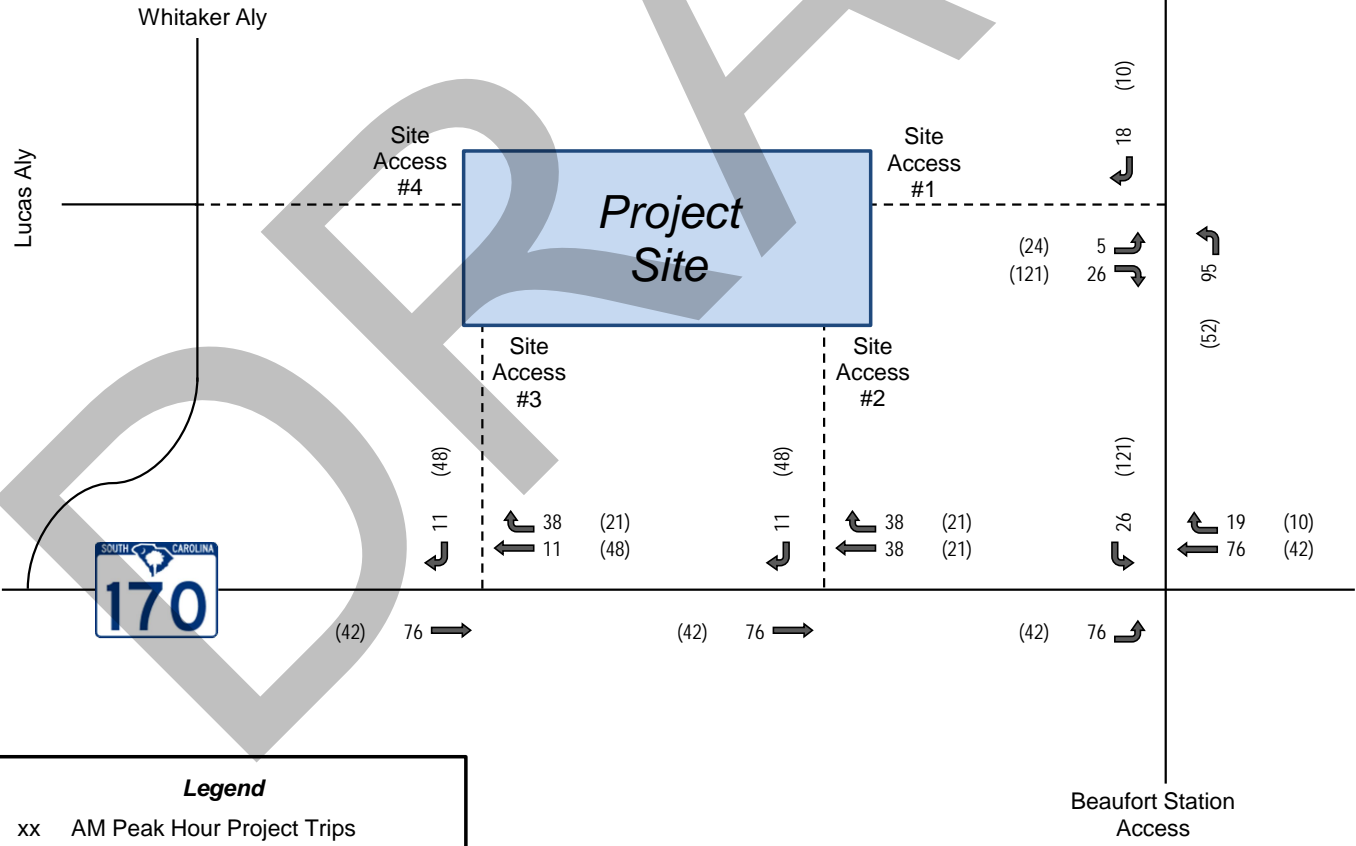
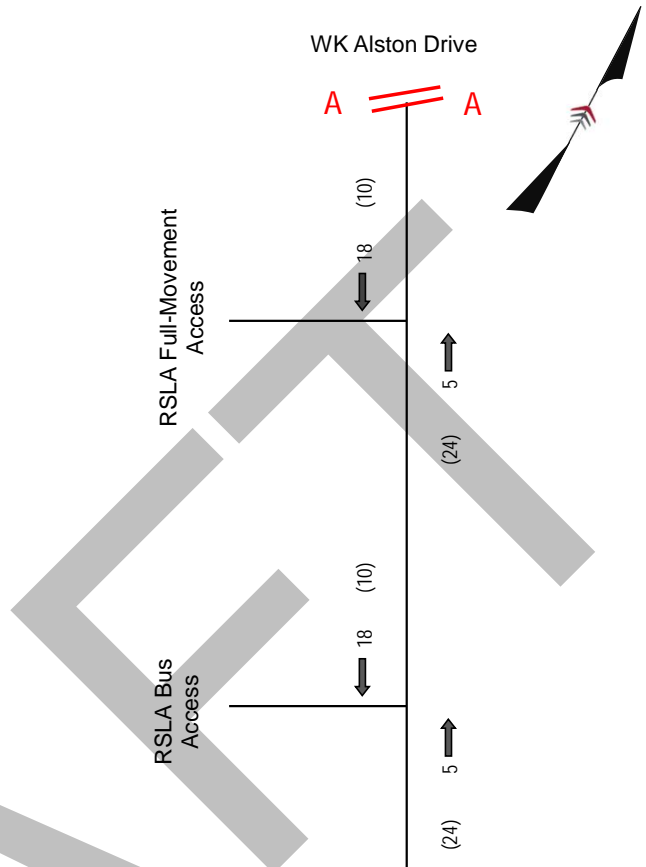
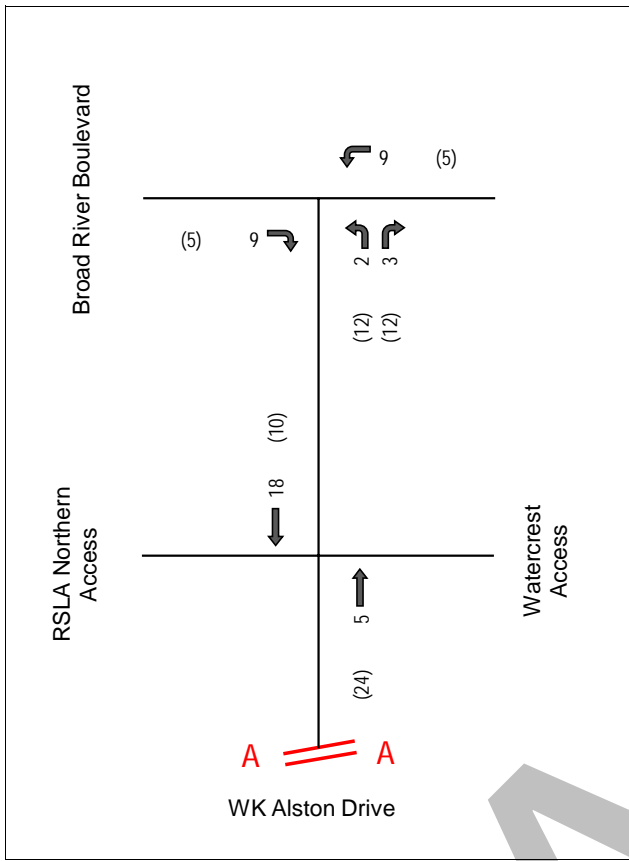
2.2 Trip Distribution & Assignment

New external trips generated by the proposed development were distributed and assigned to the surrounding roadway network based on existing travel patterns, surrounding land uses, and the proposed site layout. The trip distribution percentages used in this analysis are as follows:

- 50% to/from the East via SC 170 (Robert Smalls Parkway)
- 40% to/from the West via SC 170 (Robert Smalls Parkway)
- 5% to/from the West via Broad River Boulevard
- 5% to/from the East via Broad River Boulevard

The site trip distribution and assignment and project trips are illustrated in **Figure 4** and **Figure 5**, respectively.





Legend

xx AM Peak Hour Project Trips

(xx) PM Peak Hour Project Trips

3 Existing and Future Traffic Volume Development

3.1 Existing Traffic Development

Peak-period turning movement counts were collected at the intersections of SC 170 (Robert Smalls Parkway) & WK Alston Drive and Lucas Aly & Whitaker Aly on Tuesday, October 14th, 2025 from 7:00 AM to 9:00 AM and 4:00 PM to 6:00 PM. Previously collected counts were used for the peak-period volumes at the RSLA access driveways. These counts were collected on Thursday, March 27th, 2025 from 6:00 AM to 9:00 AM and 2:00 PM to 6:00 PM. **Figure 6** illustrates the 2025 Existing AM and PM peak hour traffic volumes. The raw turning-movement count data is included in **Appendix A**.

3.2 Future-Year No-Build Traffic Development

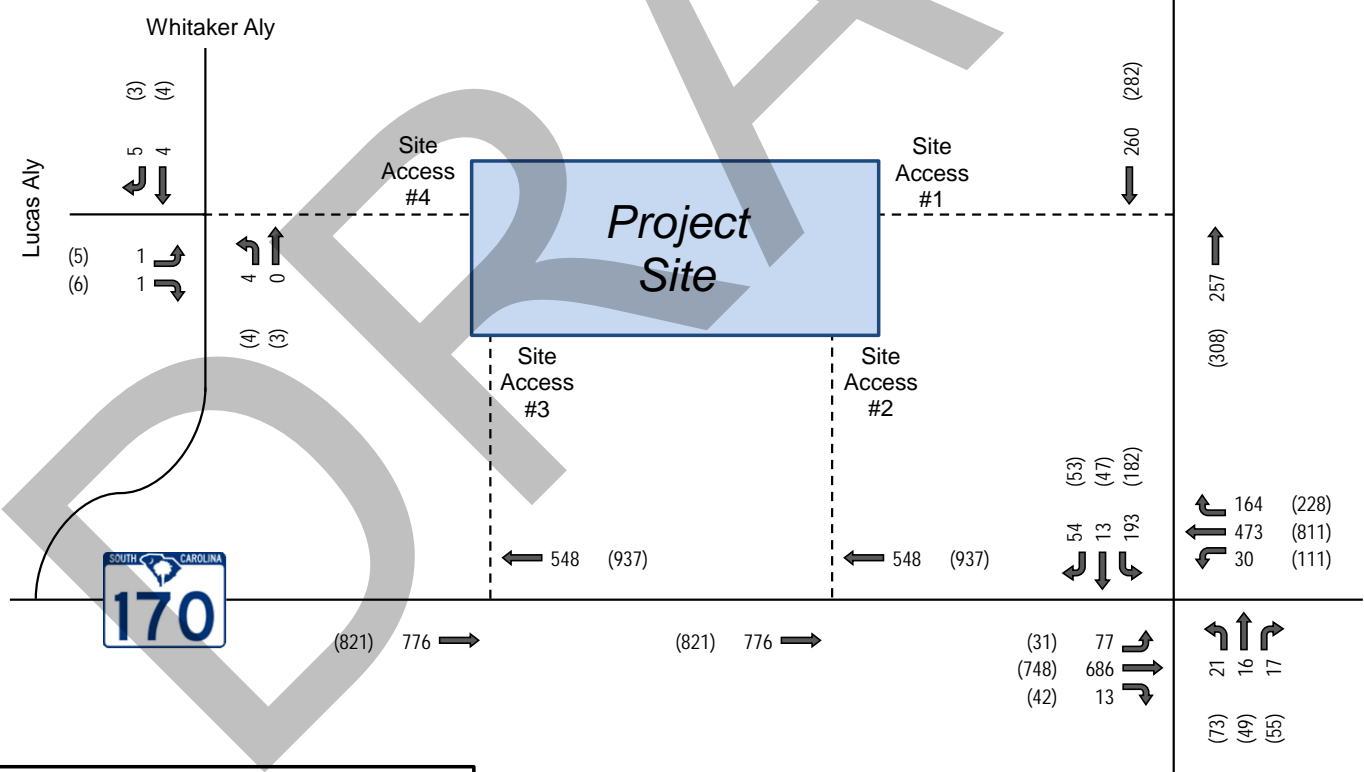
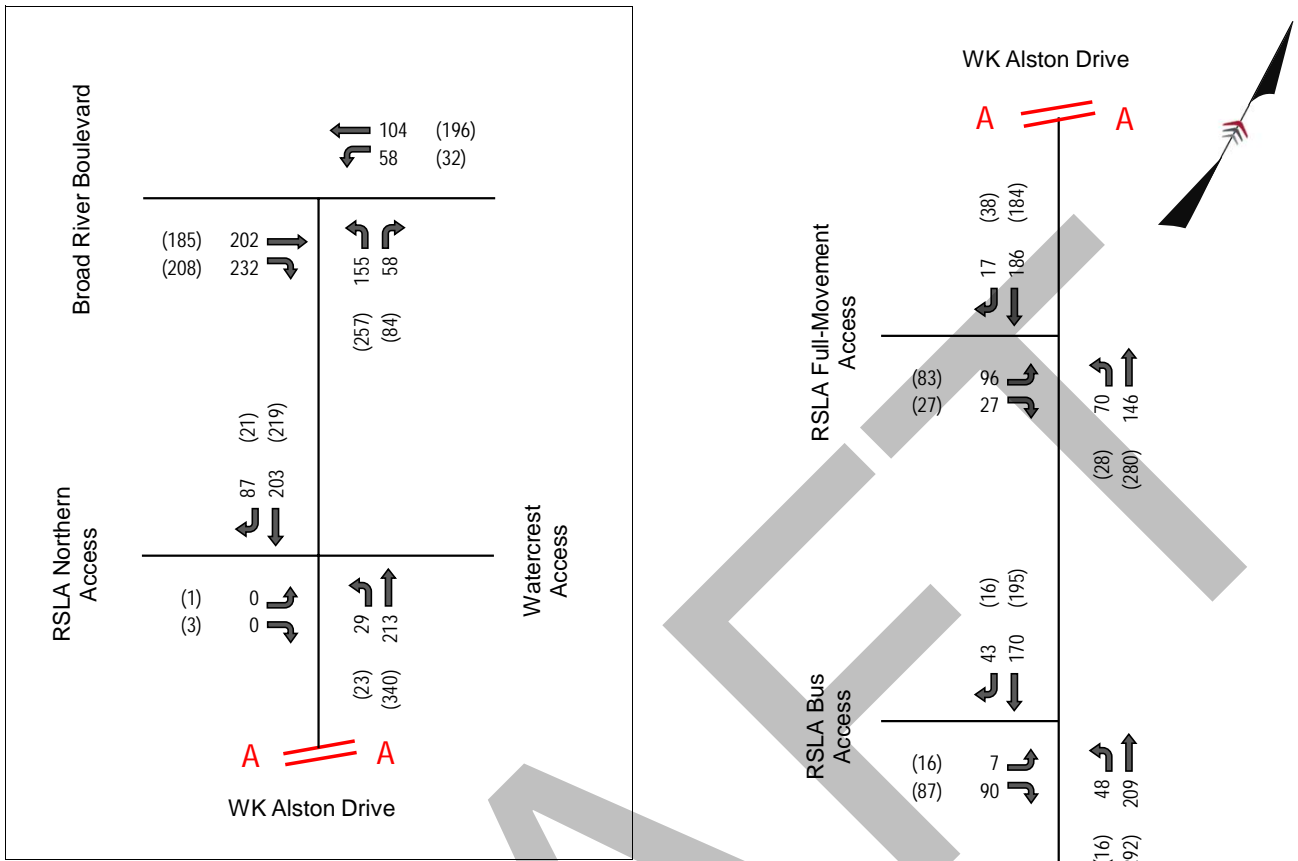
Historic traffic growth represents the increase in existing traffic volumes due to usage increases and non-specific growth throughout the area (i.e., that not associated with the subject development). SCDOT count station ID 07-0175 data on SC 170 (Robert Smalls Parkway) shows an annual growth rate of 3.9% for traffic volume since 2019. Therefore, an annual growth rate of 4.0% was used to capture the expected increase in traffic volume associated with the surrounding developments over the next 5 years.

The 2030 No-Build traffic volumes were estimated by increasing the 2025 Existing traffic volumes at a rate of 4.0% for five years. The 2030 No-Build AM and PM peak hour traffic volumes are shown in **Figure 7**.

3.3 Future-Year Build Traffic Development

The Beaufort FSED project traffic volumes were added to the 2030 No-Build traffic volumes to develop 2030 Build traffic volumes. **Figure 8** illustrates the 2030 Build traffic volumes for the AM and PM peak hours.

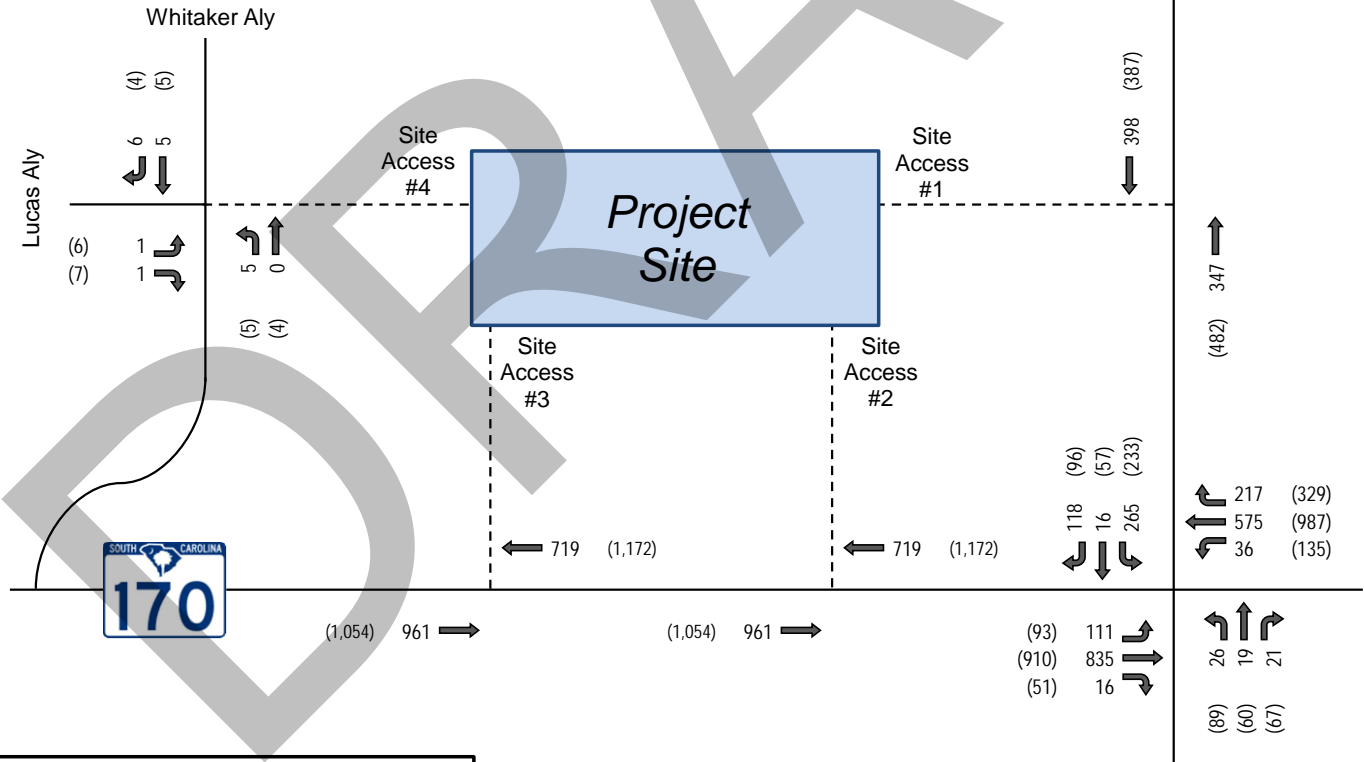
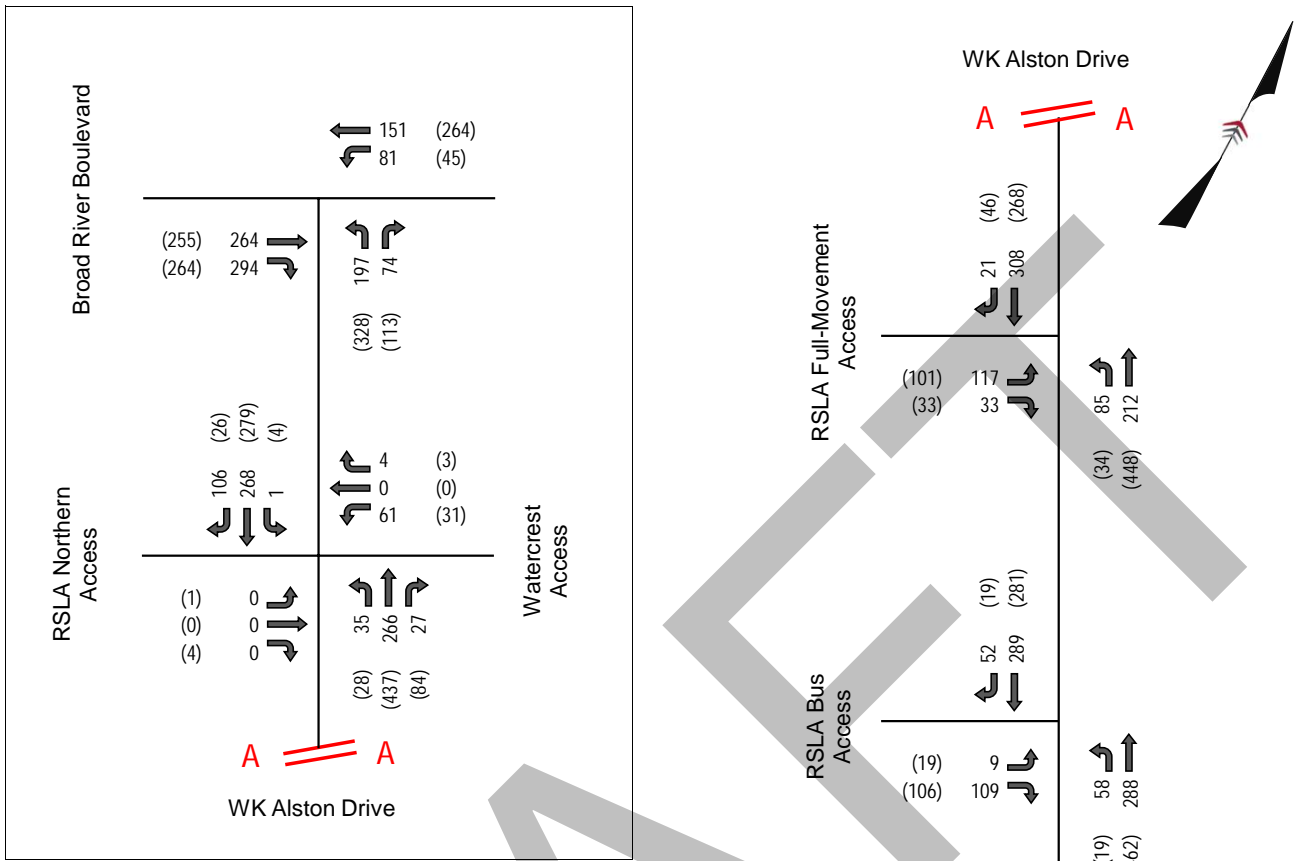
Worksheets documenting the traffic volume development are provided in **Appendix B**.



Legend

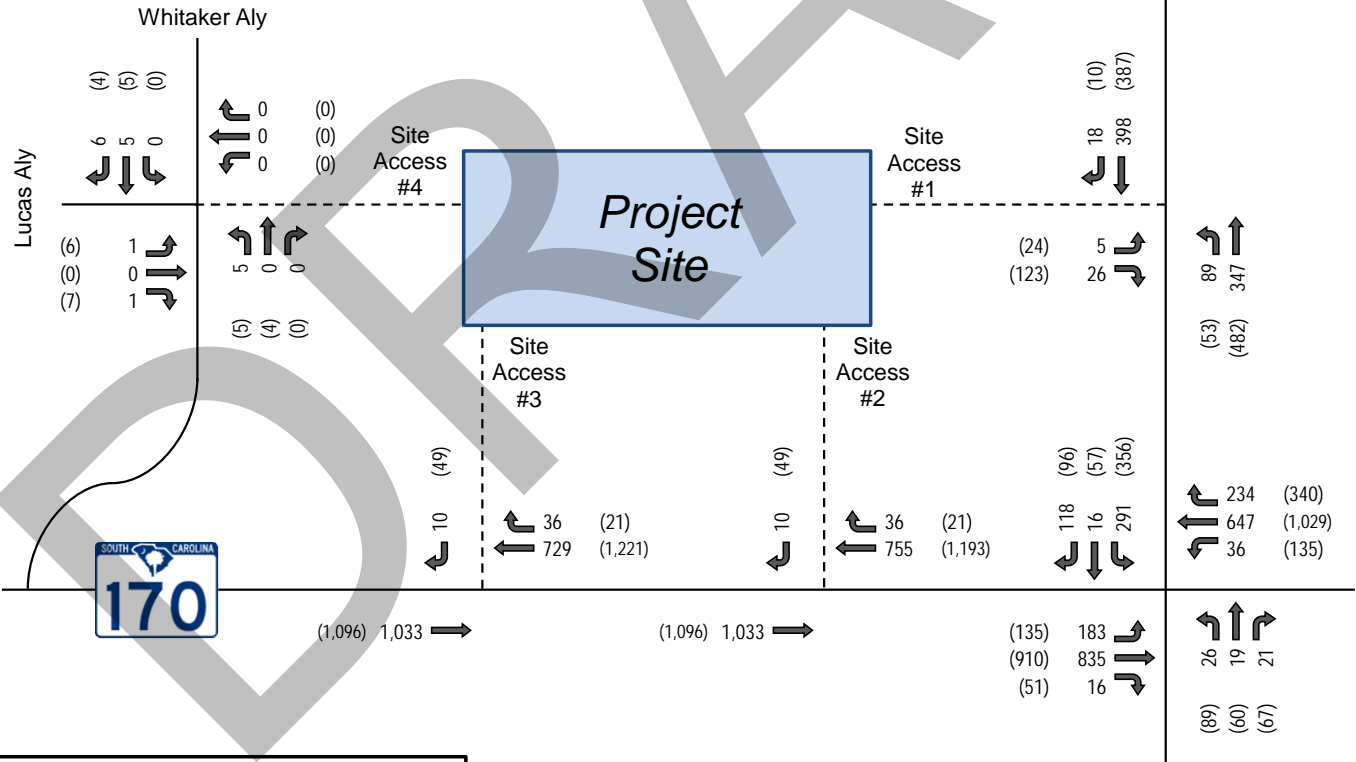
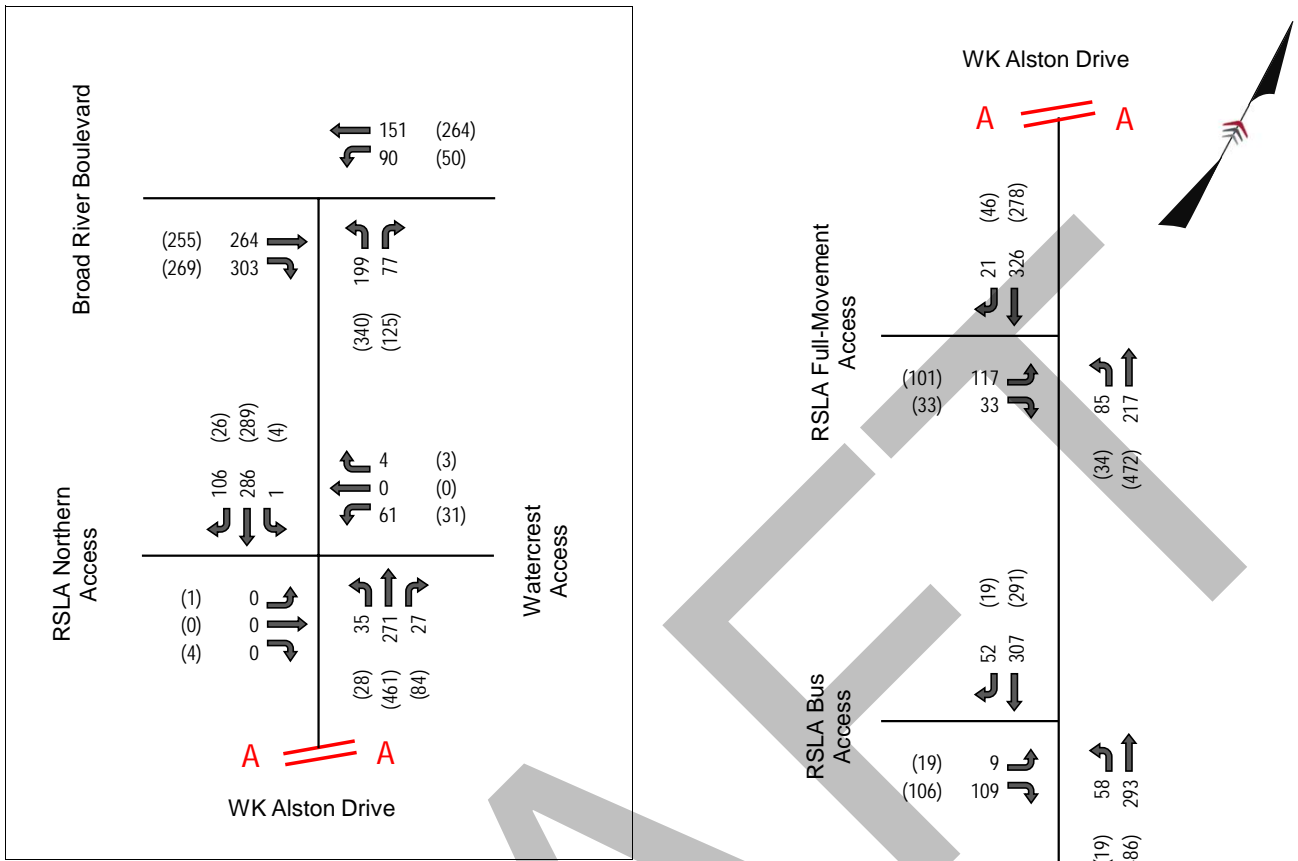
xx AM Peak Hour Traffic Volumes

(xx) PM Peak Hour Traffic Volumes



Legend

xx AM Peak Hour Traffic Volumes
 (xx) PM Peak Hour Traffic Volumes



Legend

xx AM Peak Hour Traffic Volumes

(xx) PM Peak Hour Traffic Volumes

4 Capacity Analysis

Capacity/level-of-Service (LOS) analyses were conducted using the *Highway Capacity Manual (HCM)*, 7th Edition, methodologies of the *Synchro*, Version 12, traffic analysis software. Capacity analyses were conducted for the AM and PM peak hours of the 2025 Existing conditions, 2030 No-Build conditions, and 2030 Build conditions analysis scenarios.

Intersection level of service (LOS) grades range from LOS A to LOS F, which are directly related to the level of control delay at the intersection and characterize the operational conditions of the intersection traffic flow. LOS A operations typically represent ideal, free-flow conditions where vehicles experience little to no delays, and LOS F operations typically represent poor, gridlocked conditions with high vehicular delays, and are generally considered undesirable. **Table 2** lists the LOS control delay thresholds published in the *HCM* for signalized and unsignalized intersections.

Table 2 – HCM Level of Service Criteria

LOS	Control Delay per Vehicle (sec/veh)	
	Signalized Intersections	Unsignalized Intersections
A	≤ 10	≤ 10
B	> 10 – 20	> 10 – 15
C	> 20 – 35	> 15 – 25
D	> 35 – 55	> 25 – 35
E	> 55 – 80	> 35 – 50
F	> 80	> 50

Existing peak-hour factors (PHF) were utilized for the existing and future scenarios. Existing heavy vehicle percentages were utilized for all scenarios, with a minimum of 2% considered.

Unsignalized intersections operating at LOS A-LOS C are considered to operate with short delays, unsignalized intersections operating at LOS D-LOS E are considered to operate with moderate delays, and intersections operating at LOS F are considered to operate with long delays.

The following sections outline the results of the capacity analysis for each of the study intersections. The capacity analysis worksheets are included in **Appendix C**.

4.1 Whitaker Aly at Lucas Aly and Site Access #4

The capacity analysis results for the unsignalized intersection of Whitaker Aly at Lucas Aly and Site Access #4 are summarized in **Table 3**.

Table 3 – Whitaker Aly at Lucas Aly and Site Access #4 Analysis Results

Condition	Measure	EB (Lucas Aly)		NB (Whitaker Aly)		SB (Whitaker Aly)	
		EBL	EBR	NBL	NBT	SBT	SBR
AM Peak Hour							
2025 Existing	LOS (Delay)	A (8.5)		A (7.3)*		A (0.0)	
	Synchro 95th Q	0'		0'		0'	
2030 No-Build	LOS (Delay)	A (8.6)		A (7.3)*		A (0.0)	
	Synchro 95th Q	0'		0'		0'	
2030 Build	LOS (Delay)	A (8.6)		A (7.3)*		A (0.0)	
	Synchro 95th Q	0'		0'		0'	
PM Peak Hour							
2025 Existing	LOS (Delay)	A (8.5)		A (7.2)*		A (0.0)	
	Synchro 95th Q	0'		0'		0'	
2030 No-Build	LOS (Delay)	A (8.6)		A (7.2)*		A (0.0)	
	Synchro 95th Q	0'		0'		0'	
2030 Build	LOS (Delay)	A (8.6)		A (7.2)*		A (0.0)	
	Synchro 95th Q	0'		0'		0'	

* Major street approach left-turn delay reported

Under 2025 Existing, 2030 No-Build, and 2030 Build conditions, all intersection approaches are anticipated to operate at LOS A during the AM and PM peak hours.

The intersection currently has an east leg stub for a future connection to the site access. Upon the addition of this leg, less than 1% of site traffic is anticipated to use the route to access the site. With the addition of the proposed development, all approaches and movements are expected to operate with an acceptable LOS.

Based on the results presented in **Table 3**, no mitigation is recommended at this intersection due to the proposed development.

4.2 SC 170 (Robert Smalls Parkway) at WK Alston Drive

The capacity analysis results for the signalized intersection of SC 170 (Robert Smalls Parkway) at WK Alston Drive are summarized in **Table 4**.

Table 4 – SC 170 (Robert Smalls Parkway) at WK Alston Drive Analysis Results

Condition	Measure	EB (SC 170)			WB (SC 170)			NB (Plaza Access)			SB (WK Alston Drive)			Intersection
		EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	
AM Peak Hour														
2025 Existing	LOS (Delay)	B (18.6)			B (16.7)			C (23.8)			C (21.3)			B (18.4)
	Synchro 95th Q	48'	243'	0'	25'	166'	46'	20'	33'	123'	36'			
2030 No-Build	LOS (Delay)	C (21.7)			B (19.3)			C (30.0)			C (26.5)			C (21.9)
	Synchro 95th Q	74'	344'	0'	32'	233'	53'	27'	44'	207'	53'			
2030 Build	LOS (Delay)	C (22.5)			C (23.0)			C (30.3)			C (26.5)			C (23.6)
	Synchro 95th Q	122'	357'	0'	33'	300'	60'	28'	47'	236'	53'			
PM Peak Hour														
2025 Existing	LOS (Delay)	C (20.8)			B (16.4)			C (28.6)			C (25.8)			B (19.9)
	Synchro 95th Q	23'	284'	0'	66'	278'	46'	59'	91'	132'	73'			
2030 No-Build	LOS (Delay)	C (22.3)			B (19.6)			D (35.6)			C (31.9)			C (23.1)
	Synchro 95th Q	66'	431'	0'	95'	448'	109'	87'	141'	213'	122'			
2030 Build	LOS (Delay)	C (27.1)			C (26.6)			D (39.0)			C (33.2)			C (28.6)
	Synchro 95th Q	122'	454'	0'	106'	543'	160'	93'	161'	366'	123'			

95th percentile volume exceeds capacity, queue may be longer

Under 2025 Existing conditions, the intersection operates at an LOS B overall. All approaches operate at LOS C or better. Under the 2030 No-Build conditions, the eastbound approach is anticipated to go from LOS B to LOS C in the AM peak hour. In the PM peak hour, the northbound approach is anticipated to drop from LOS C to LOS D. In the 2030 Build condition, the intersection is anticipated to operate at LOS C overall in the AM and PM peak hours.

Based on the results presented in **Table 4**, no mitigation is recommended at this intersection due to the proposed development.

4.3 WK Alston Drive at RSLA Bus Access

The capacity analysis results for the unsignalized intersection of WK Alston Drive at RSLA Bus Access are summarized in **Table 5**.

Table 5 – WK Alston Drive at RSLA Bus Access Analysis Results

Condition	Measure	EB (RSLA Bus Access)		NB (WK Alston Drive)		SB (WK Alston Drive)	
		EBL	EBR	NBL	NBT	SBT	SBR
AM Peak Hour							
2025 Existing	LOS (Delay)	B (10.8)		A (8.0)*		A (0.0)	
	Synchro 95th Q	15'		3'		0'	
2030 No-Build	LOS (Delay)	B (13.2)		A (8.5)*		A (0.0)	
	Synchro 95th Q	25'		5'		0'	
2030 Build	LOS (Delay)	B (13.6)		A (8.6)*		A (0.0)	
	Synchro 95th Q	25'		5'		0'	
PM Peak Hour							
2025 Existing	LOS (Delay)	B (11.8)		A (8.1)*		A (0.0)	
	Synchro 95th Q	18'		0'		0'	
2030 No-Build	LOS (Delay)	B (14.9)		A (8.4)*		A (0.0)	
	Synchro 95th Q	30'		3'		0'	
2030 Build	LOS (Delay)	C (15.4)		A (8.4)*		A (0.0)	
	Synchro 95th Q	33'		3'		0'	

* Major street approach left-turn delay reported

Under 2025 Existing and 2030 No-Build conditions, the eastbound left-turn and northbound left-turn are expected to operate at LOS A. With the addition of project trips, all approaches are anticipated to continue to operate at LOS C or better.

Based on the results presented in **Table 5**, no mitigation is recommended at this intersection due to the proposed development.

4.4 WK Alston Drive at RSLA Full-Movement Access

The capacity analysis results for the unsignalized intersection of WK Alston Drive at RSLA Full-Movement Access are summarized in **Table 6**.

Table 6 – WK Alston Drive at RSLA Full-Movement Access Analysis Results

Condition	Measure	EB (RSLA Full-Movement Access)		NB (WK Alston Drive)		SB (WK Alston Drive)	
		EBL	EBR	NBL	NBT	SBT	SBR
AM Peak Hour							
2025 Existing	LOS (Delay)	B (14.9)		A (7.9)*		A (0.0)	
	Synchro 95th Q	28'	3'	5'		0'	
2030 No-Build	LOS (Delay)	C (24.6)		A (8.4)*		A (0.0)	
	Synchro 95th Q	63'	5'	8'		0'	
2030 Build	LOS (Delay)	D (26.0)		A (8.5)*		A (0.0)	
	Synchro 95th Q	68'	5'	8'		0'	
PM Peak Hour							
2025 Existing	LOS (Delay)	C (16.5)		A (8.0)*		A (0.0)	
	Synchro 95th Q	30'	5'	3'		0'	
2030 No-Build	LOS (Delay)	D (34.2)		A (8.3)*		A (0.0)	
	Synchro 95th Q	85'	5'	3'		0'	
2030 Build	LOS (Delay)	E (38.4)		A (8.4)*		A (0.0)	
	Synchro 95th Q	95'	5'	3'		0'	

* Major street approach left-turn delay reported

Under 2030 No-Build conditions, all major street approaches and movements are expected to operate at LOS C or better during the AM and PM peak hours. The eastbound approach is anticipated to operate at LOS D in the PM peak hour. With the addition of project traffic, all approaches and movements are expected to continue to operate at similar LOS during the AM and PM peak hours. Additionally, it is not uncommon for the minor street approaches to experience more delays during the peak hours. As part of previous studies and existing traffic conditions, a northbound left turn lane with 100 feet of storage has been recommended and included in the 2030 No-Build and 2030 Build analysis. This improvement is not the responsibility of the FSED development as previously existing conditions have warranted this improvement.

Based on the results presented in **Table 6**, no mitigation is recommended at this intersection due to the proposed development.

4.5 WK Alston Drive at RSLA Northern Access

The capacity analysis results for the unsignalized intersection of WK Alston Drive at RSLA Northern Access are summarized in **Table 7**.

Table 7 – WK Alston Drive at RSLA Northern Access Analysis Results

Condition	Measure	EB (RSLA North Access)			WB (Watercrest Access)			NB (WK Alston Drive)			SB (WK Alston Drive)		
		EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
AM Peak Hour													
2025 Existing	LOS (Delay)	A (0.0)			-			A (8.1)*			A (0.0)		
	Synchro 95th Q	0'			-			3'			0'		
2030 No-Build	LOS (Delay)	A (0.0)			C (21.0)			A (8.5)*			A (8.0)*		
	Synchro 95th Q	0'			28'			3'			0'		
2030 Build	LOS (Delay)	A (0.0)			C (22.0)			A (8.5)*			A (8.1)*		
	Synchro 95th Q	0'			28'			3'			0'		
PM Peak Hour													
2025 Existing	LOS (Delay)	B (11.2)			-			A (7.9)*			A (0.0)		
	Synchro 95th Q	0'			-			3'			0'		
2030 No-Build	LOS (Delay)	B (12.7)			C (24.8)			A (8.2)*			A (8.9)*		
	Synchro 95th Q	0'			18'			3'			0'		
2030 Build	LOS (Delay)	B (13.0)			D (26.6)			A (8.2)*			A (9.0)*		
	Synchro 95th Q	0'			18'			3'			0'		

* Major street approach left-turn delay reported

Under 2030 No-Build conditions, all approaches and movements are expected to operate at LOS C or better during the AM and PM peak hours. With the addition of project traffic, all approaches and movements are expected to continue to operate a similar LOS to the 2030 No-Build conditions during the AM and PM peak hours. Additionally, it is not uncommon for the minor street approaches to experience more delays during the peak hours.

As part of the Watercrest Apartments development, a northbound right-turn lane with 100 feet of storage and a southbound right-turn lane with 100 feet of storage have been recommended to maintain acceptable operations at the intersection. The additional turn lane improvements are included in the 2030 No-Build and 2030 Build analysis.

Based on the results presented in **Table 7**, no mitigation is recommended at this intersection due to the proposed development.

4.6 WK Alston Drive at Broad River Boulevard

The capacity analysis results for the unsignalized intersection of WK Alston Drive at Broad River Boulevard are summarized in **Table 8**.

Table 8 – WK Alston Drive at Broad River Boulevard Analysis Results

Condition	Measure	EB (Broad River Boulevard)		WB (Broad River Boulevard)		NB (WK Alston Drive)	
		EBT	EBR	WBL	WBT	NBL	NBR
AM Peak Hour							
2025 Existing	LOS (Delay)	A (0.0)		A (8.7)*		C (19.1)	
	Synchro 95th Q	0'		5'		63'	10'
2030 No-Build	LOS (Delay)	A (0.0)		A (9.4)*		D (27.8)	
	Synchro 95th Q	0'		10'		120'	10'
2030 Build	LOS (Delay)	A (0.0)		A (9.5)*		D (29.9)	
	Synchro 95th Q	0'		10'		130'	10'
PM Peak Hour							
2025 Existing	LOS (Delay)	A (0.0)		A (8.4)*		C (24.6)	
	Synchro 95th Q	0'		3'		123'	13'
2030 No-Build	LOS (Delay)	A (0.0)		A (8.9)*		F (56.5)	
	Synchro 95th Q	0'		5'		278'	15'
2030 Build	LOS (Delay)	A (0.0)		A (9.0)*		F (64.7)	
	Synchro 95th Q	0'		5'		313'	18'

* Major street approach left-turn delay reported

Under 2025 Existing conditions, the northbound approach is anticipated to operate at LOS C. Under 2030 No-Build conditions, it is anticipated to lower to LOS D and LOS F in the AM and PM peak hours, respectively. With the addition of project traffic, the approach is anticipated to continue to operate at LOS D and LOS F in the AM and PM peak hours, respectively. The additional delay is 1.0 seconds or less for the analyzed conditions. Additionally, it is not uncommon for the minor street approaches to experience more delays during the peak hours.

Background conditions have shown an eastbound right-turn lane with 200 feet of storage to be warranted at this intersection to provide acceptable operations. This is warranted based on anticipated conditions without the additional project trips. Therefore, it is not the responsibility of the proposed FSED Development.

Based on the results presented in **Table 8**, an additional westbound left-turn lane with 200 feet of storage is warranted and recommended at this intersection to provide acceptable operations.

4.7 WK Alston Drive at Site Access #1

The capacity analysis results for the unsignalized intersection of WK Alston Drive at Site Access #1 are summarized in **Table 9**.

Table 9 – WK Alston Drive at Site Access #1 Analysis Results

Condition	Measure	EB (FSED Access #1)		NB (WK Alston Drive)		SB (WK Alston Drive)	
		EBL	EBR	NBL	NBT	SBT	SBR
AM Peak Hour							
2030 Build	LOS (Delay)	B (13.0)		A (8.6)*		A (0.0)	
	Synchro 95th Q	5'		8'		0'	
PM Peak Hour							
2030 Build	LOS (Delay)	C (16.2)		A (8.4)*		A (0.0)	
	Synchro 95th Q	38'		5'		0'	

* Major street approach left-turn delay reported

Under 2030 Build conditions, all approaches and movements are expected to operate at LOS C or better during the AM and PM peak hours.

Based on the results presented in **Table 9** and the results of the turn lane warrant analysis, a northbound left-turn lane with 100 feet of storage is recommended to provide acceptable operations at this intersection. It is recommended the access operate as full-movement with one ingress and one egress lane.

4.8 SC 170 (Robert Smalls Parkway) at Site Access #2

The capacity analysis results for the unsignalized intersection of SC 170 (Robert Smalls Parkway) at Site Access #2 are summarized in **Table 10**.

Table 10 – SC 170 (Robert Smalls Parkway) at Site Access #2 Analysis Results

Condition	Measure	EB (SC 170)	WB (SC 170)		SB (FSED Access #2)
		EBT	WBT	WBR	SBR
AM Peak Hour					
2030 Build	LOS (Delay)	A (0.0)	A (0.0)		B (11.5)
	Synchro 95th Q	0'	0'		3'
PM Peak Hour					
2030 Build	LOS (Delay)	A (0.0)	A (0.0)		C (15.5)
	Synchro 95th Q	0'	0'		13'

Under 2030 Build conditions, all approaches and movements are expected to operate at LOS C or better during the AM and PM peak hours.

Based on the results presented in **Table 10**, no mitigation is recommended at this intersection due to the proposed development. It is recommended the access operate as right-in/right-out only with one ingress and one egress lane.

4.9 SC 170 (Robert Smalls Parkway) at Site Access #3

The capacity analysis results for the unsignalized intersection of SC 170 (Robert Smalls Parkway) at Site Access #3 are summarized in **Table 10**.

Table 11 – SC 170 (Robert Smalls Parkway) at Site Access #3 Analysis Results

Condition	Measure	EB (SC 170)	WB (SC 170)		SB (FSED Access #3)
		EBT	WBT	WBR	SBR
AM Peak Hour					
2030 Build	LOS (Delay)	A (0.0)	A (0.0)		B (11.4)
	Synchro 95th Q	0'	0'		3'
PM Peak Hour					
2030 Build	LOS (Delay)	A (0.0)	A (0.0)		C (15.8)
	Synchro 95th Q	0'	0'		13'

Under 2030 Build conditions, all approaches and movements are expected to operate at LOS C or better during the AM and PM peak hours.

Based on the results presented in **Table 10**, no mitigation is recommended at this intersection due to the proposed development. It is recommended the access operate as right-in/right-out only with one ingress and one egress lane.

5 Auxiliary Turn Lane Warrants

Warrants for additional turn-lane improvements for unsignalized intersections beyond those necessary for capacity were determined based on a review of the 2021 SCDOT Highway Design Manual. The results of the warrants for the left-turn and right-turn lanes under 2030 Build conditions are summarized below and included in **Appendix D**.

A turn lane warrant analysis was conducted to determine the impact of the proposed project on the RSLA access driveways and the site access driveways for the proposed Beaufort FSED development. The following turn lane warrants were met and recommended in previous studies:

WK Alston Drive at RSLA Northern Access / Watercrest Apartments Access

- A northbound right-turn lane with 100 feet of full width storage and a 100-foot taper
- A southbound right-turn lane with 100 feet of full width storage and a 100-foot taper

WK Alston Drive at RSLA Full-Movement Access

- A northbound left-turn lane with 100 feet of full width storage and a 100-foot taper

WK Alston Drive at Broad River Boulevard

- An eastbound right-turn lane with 200 feet of full width storage and a 100-foot taper

The findings of this study support the recommendation to install the listed turn lanes to maintain acceptable operating conditions for the RSLA access driveways and proposed Beaufort FSED site access driveways. These turn lanes are not the responsibility of the FSED development. They are warranted prior to the site traffic being added to the study area network.

Additional turn lanes are warranted and recommended as a result of the impacts of the development:

- A westbound left-turn lane with 200 feet of storage at the intersection of WK Alston Drive and Broad River Boulevard
- A northbound left-turn lane with 100 feet of storage at the intersection of WK Alston Drive and Site Access #1

6 Conclusion

The purpose of this Traffic Impact Analysis (TIA) is to review vehicular traffic impacts as a result of the proposed Beaufort FSED development. The development is proposed to be located at the northwest quadrant of the intersection of SC 170 (Robert Smalls Parkway) at WK Alston Drive and is planned to consist of the following:

- 96,200 SF of Medical-Office Building Space

The development is anticipated to be completed in 2030. Based on the site layout, the proposed development will be accessed via the following site accesses:

- One full-movement driveway with one ingress and one egress lane as the west leg of the proposed intersection of WK Alston Drive at Site Access #1
- One right-in/right-out access only driveway with one ingress and one egress lane as the north leg of the proposed intersection of SC 170 (Robert Smalls Parkway) at Site Access #2
- One right-in/right-out access only driveway with one ingress and one egress lane as the north leg of the proposed intersection of SC 170 (Robert Smalls Parkway) at Site Access #3
- One full-movement driveway with one ingress and one egress lane as the proposed east leg of the intersection of Whitaker Aly at Lucas Aly and Site Access #4

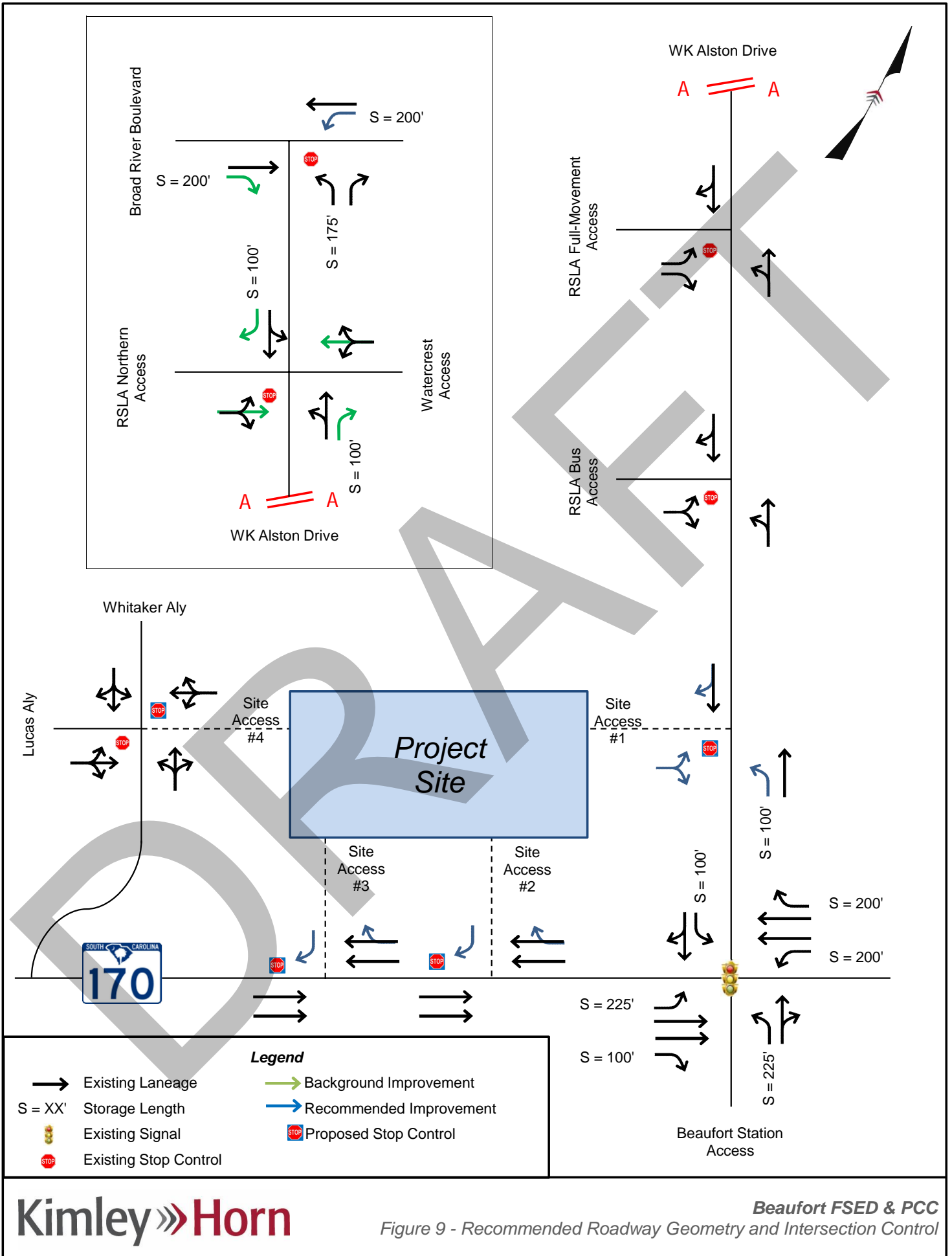
This TIA evaluates the traffic operations under 2025 Existing conditions, 2030 No-Build conditions, and 2030 Build conditions during the AM and PM peak hours at the following intersections:

- Whitaker Aly at Lucas Aly and Site Access #4
- SC 170 (Robert Smalls Parkway) at WK Alston Drive
- WK Alston Drive at RSLA Bus Access
- WK Alston Drive at RSLA Full-Movement Access
- WK Alston Drive at RSLA Northern Access
- WK Alston Drive at Broad River Boulevard
- WK Alston Drive at Site Access #1
- SC 170 (Robert Smalls Parkway) at Site Access #2
- SC 170 (Robert Smalls Parkway) at Site Access #3

Based on the results of the traffic analysis, the proposed Beaufort FSED Development is anticipated to have impact on the surrounding road network warranting additional turn lanes as follows:

- A westbound left-turn lane with 200 feet of storage at the intersection of WK Alston Drive and Broad River Boulevard
- A northbound left-turn lane with 100 feet of storage at the intersection of WK Alston Drive and Site Access #1

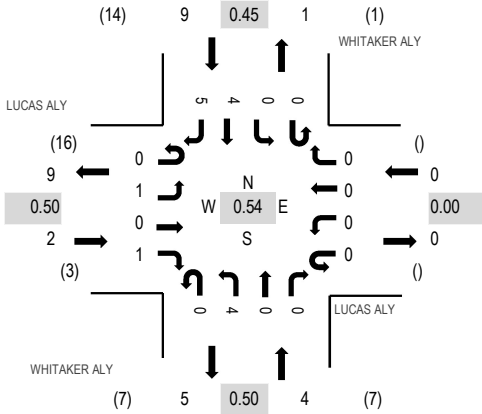
Recommended roadway geometry and traffic control is illustrated in **Figure 9**.



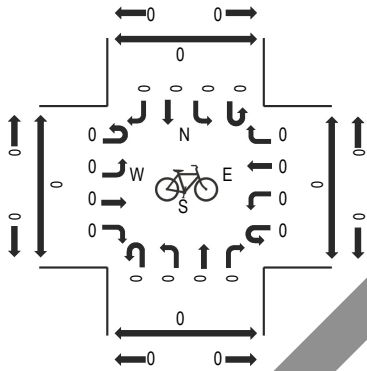
Appendix A – Turning Movement Counts

DRAFT

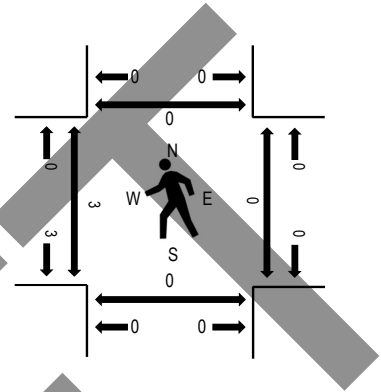
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	LUCAS ALY Eastbound				LUCAS ALY Westbound				WHITAKER ALY Northbound				WHITAKER ALY Southbound				Total	Rolling Hour	Pedestrian Crossings				
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North	
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	2	15	0	0	0	0
7:15 AM	0	1	0	0	0	0	0	0	0	2	0	0	0	0	1	0	4	13	3	0	0	0	0
7:30 AM	0	0	0	0	0	0	0	0	0	2	0	0	0	0	3	2	7	13	0	0	0	0	
7:45 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	2	8	0	0	0	0	
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9	0	0	0	0	
8:15 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	3	4	0	0	0	0	0	
8:30 AM	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	0	2	0	0	0	0	0	
8:45 AM	0	0	0	0	0	0	0	0	0	2	0	0	0	0	0	1	3	0	0	0	0	0	

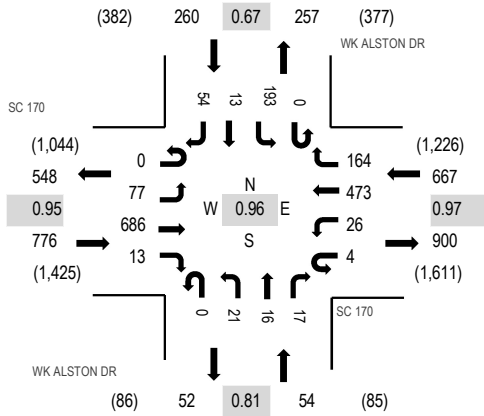
Peak Rolling Hour Flow Rates

Vehicle Type	Eastbound				Westbound				Northbound				Southbound				Total
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Lights	0	1	0	1	0	0	0	0	0	4	0	0	0	0	4	5	15
Mediums	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	1	0	1	0	0	0	0	0	4	0	0	0	0	4	5	15

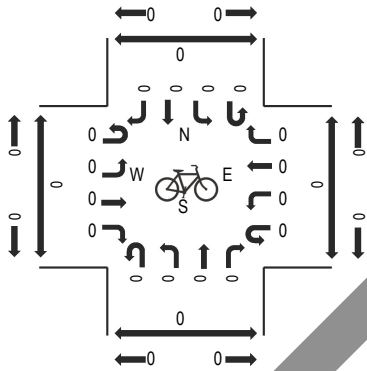
Heavy Vehicle Percentage and Peak Hour Factor

	Eastbound				Westbound				Northbound				Southbound				Total
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	
Heavy Vehicle %	0.0%				0.0%				0.0%				0.0%				0.0%
Heavy Vehicle %	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
Peak Hour Factor	0.50				0.00				0.50				0.45				0.54
Peak Hour Factor	0.00	0.25	0.00	0.50	0.00	0.00	0.00	0.00	0.00	0.50	0.00	0.00	0.00	0.00	0.33	0.50	0.54

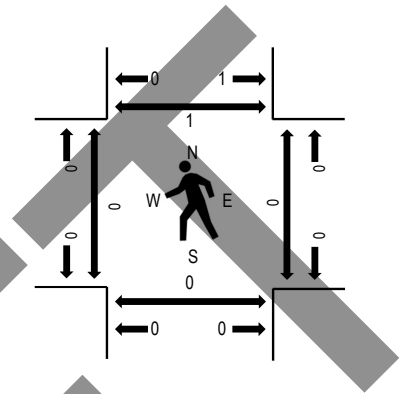
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	SC 170 Eastbound				SC 170 Westbound				WK ALSTON DR Northbound				WK ALSTON DR Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
7:00 AM	0	2	123	1	0	3	119	16	0	5	2	1	0	15	1	7	295	1,365	0	0	0	0
7:15 AM	0	11	144	1	0	2	104	19	0	3	3	0	0	17	2	0	306	1,496	0	1	0	0
7:30 AM	0	11	155	3	0	2	104	18	0	4	2	1	0	25	2	4	331	1,631	0	0	0	0
7:45 AM	0	17	183	4	1	5	127	39	0	5	3	1	0	39	4	5	433	1,757	0	0	0	0
8:00 AM	0	21	174	2	1	7	124	35	0	5	2	6	0	34	4	11	426	1,753	0	0	0	1
8:15 AM	0	22	165	3	2	5	123	41	0	5	5	5	0	54	2	9	441		0	0	0	0
8:30 AM	0	17	164	4	0	9	99	49	0	6	6	5	0	66	3	29	457		0	0	0	0
8:45 AM	0	6	190	2	0	10	136	26	0	3	4	3	0	37	5	7	429		0	0	0	0

Peak Rolling Hour Flow Rates

Vehicle Type	Eastbound				Westbound				Northbound				Southbound				Total
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	
Articulated Trucks	0	0	4	1	0	0	6	0	0	0	0	0	0	0	0	0	11
Lights	0	77	672	11	4	25	448	156	0	21	16	17	0	177	13	52	1,689
Mediums	0	0	10	1	0	1	19	8	0	0	0	0	0	16	0	2	57
Total	0	77	686	13	4	26	473	164	0	21	16	17	0	193	13	54	1,757

Heavy Vehicle Percentage and Peak Hour Factor

	Eastbound				Westbound				Northbound				Southbound				Total
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	
Heavy Vehicle %		0.6%				0.9%				0.0%				0.0%			0.6%
Heavy Vehicle %	0.0%	0.0%	0.6%	7.7%	0.0%	0.0%	1.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.6%
Peak Hour Factor		0.95				0.97				0.81				0.67			0.96
Peak Hour Factor	0.00	0.88	0.91	0.81	0.50	0.78	0.89	0.84	0.00	0.88	0.71	0.79	0.00	0.73	0.70	0.48	0.96

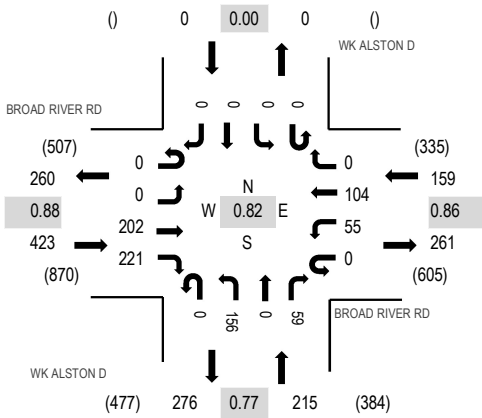
Location: 1 WK ALSTON D & BROAD RIVER RD AM

Date: Thursday, March 27, 2025

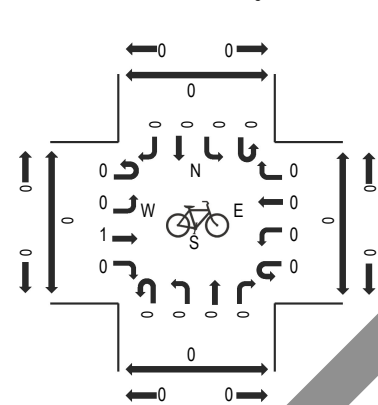
Peak Hour: 07:45 AM - 08:45 AM

Peak 15-Minutes: 08:30 AM - 08:45 AM

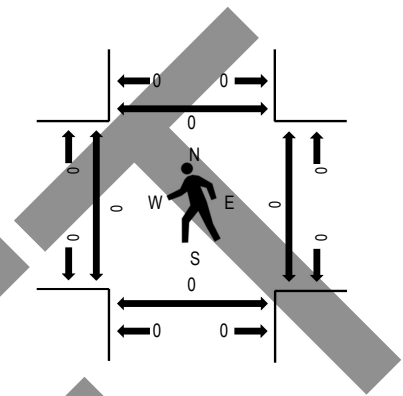
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

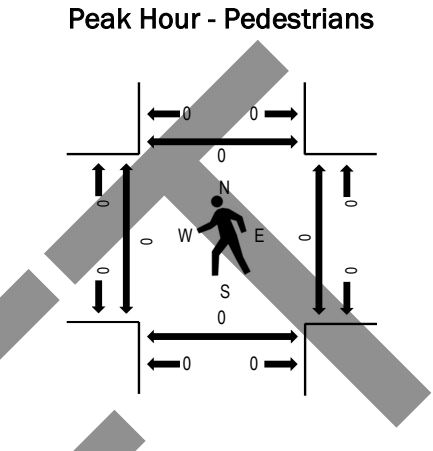
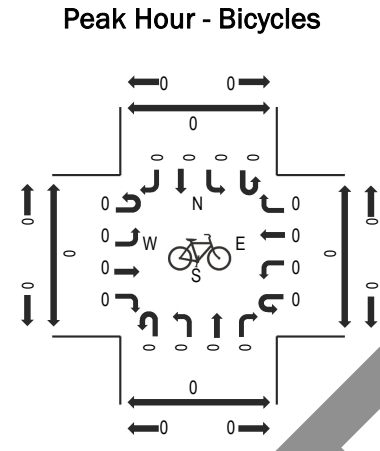
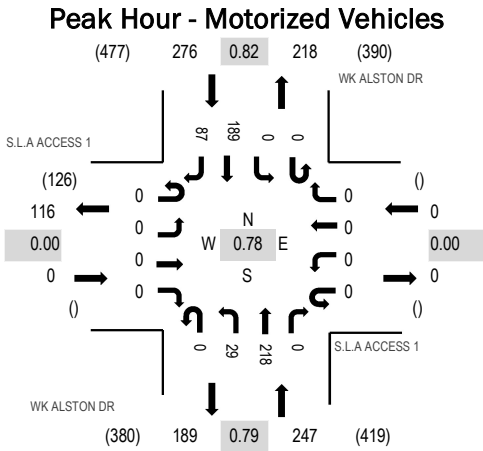
Interval Start Time	BROAD RIVER RD Eastbound				BROAD RIVER RD Westbound				WK ALSTON D Northbound				WK ALSTON D Southbound				Total	Rolling Hour	Pedestrian Crossings				
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North	
6:00 AM	0	0	13	4	0	5	3	0	0	0	1	0	2	0	0	0	0	28	207	0	0	0	0
6:15 AM	0	0	21	6	0	5	7	0	0	6	0	3	0	0	0	0	0	48	297	0	0	0	0
6:30 AM	0	0	20	11	0	5	10	0	0	6	0	0	0	0	0	0	0	52	416	0	0	0	0
6:45 AM	0	0	39	14	0	5	14	0	0	4	0	3	0	0	0	0	0	79	533	0	0	0	0
7:00 AM	0	0	37	19	0	10	25	0	0	20	0	7	0	0	0	0	0	118	624	0	0	0	0
7:15 AM	0	0	67	33	0	3	29	0	0	20	0	15	0	0	0	0	0	167	675	0	0	0	0
7:30 AM	0	0	66	34	0	8	28	0	0	22	0	11	0	0	0	0	0	169	724	0	0	0	0
7:45 AM	0	0	52	53	0	11	24	0	0	24	0	6	0	0	0	0	0	170	797	0	0	0	0
8:00 AM	0	0	53	36	0	13	26	0	0	33	0	8	0	0	0	0	0	169	758	0	0	0	0
8:15 AM	0	0	46	63	0	16	23	0	0	46	0	22	0	0	0	0	0	216		0	0	0	0
8:30 AM	0	0	51	69	0	15	31	0	0	53	0	23	0	0	0	0	0	242		0	0	0	0
8:45 AM	0	0	29	34	0	5	14	0	0	38	0	11	0	0	0	0	0	131		0	0	0	0

Peak Rolling Hour Flow Rates

Vehicle Type	Eastbound				Westbound				Northbound				Southbound				Total
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Lights	0	0	199	205	0	54	102	0	0	152	0	58	0	0	0	0	770
Mediums	0	0	3	16	0	1	2	0	0	4	0	1	0	0	0	0	27
Total	0	0	202	221	0	55	104	0	0	156	0	59	0	0	0	0	797

Heavy Vehicle Percentage and Peak Hour Factor

	Eastbound				Westbound				Northbound				Southbound				Total
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	
Heavy Vehicle %	0.0%				0.0%				0.0%				0.0%				0.0%
Heavy Vehicle %	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
Peak Hour Factor	0.88				0.86				0.77				0.00				0.82
Peak Hour Factor	0.00	0.00	0.89	0.80	0.00	0.86	0.92	0.00	0.00	0.80	0.00	0.70	0.00	0.00	0.00	0.00	0.82



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	S.L.A ACCESS 1 Eastbound				S.L.A ACCESS 1 Westbound				WK ALSTON DR Northbound				WK ALSTON DR Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
6:00 AM	0	0	0	0	0	0	0	0	0	0	3	0	0	0	8	1	12	79	0	0	0	0
6:15 AM	0	0	0	0	0	0	0	0	0	0	9	0	0	0	11	0	20	124	0	0	0	0
6:30 AM	0	0	0	0	0	0	0	0	0	0	6	0	0	0	16	0	22	177	0	0	0	0
6:45 AM	0	0	0	0	0	0	0	0	0	0	7	0	0	0	18	0	25	228	0	0	0	0
7:00 AM	0	0	0	0	0	0	0	0	0	0	27	0	0	0	30	0	57	298	0	0	0	0
7:15 AM	0	0	0	0	0	0	0	0	0	0	38	0	0	0	35	0	73	337	0	0	0	0
7:30 AM	0	0	0	0	0	0	0	0	0	0	30	0	0	0	41	2	73	429	0	0	0	0
7:45 AM	0	0	0	0	0	0	0	0	0	1	30	0	0	0	60	4	95	523	0	0	0	0
8:00 AM	0	0	0	0	0	0	0	0	0	7	41	0	0	0	42	6	96	519	0	0	0	0
8:15 AM	0	0	0	0	0	0	0	0	0	15	70	0	0	0	44	36	165		0	0	0	0
8:30 AM	0	0	0	0	0	0	0	0	0	6	77	0	0	0	43	41	167		0	0	0	0
8:45 AM	0	0	0	0	0	0	0	0	0	0	52	0	0	0	32	7	91		0	0	0	0

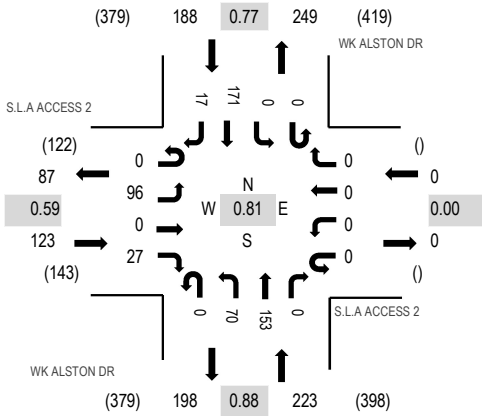
Peak Rolling Hour Flow Rates

Vehicle Type	Eastbound				Westbound				Northbound				Southbound				Total
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Lights	0	0	0	0	0	0	0	0	0	29	214	0	0	0	172	87	502
Mediums	0	0	0	0	0	0	0	0	0	0	4	0	0	0	17	0	21
Total	0	0	0	0	0	0	0	0	0	29	218	0	0	0	189	87	523

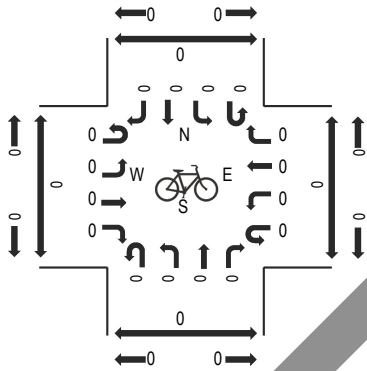
Heavy Vehicle Percentage and Peak Hour Factor

	Eastbound				Westbound				Northbound				Southbound				Total
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	
Heavy Vehicle %	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
Peak Hour Factor	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.48	0.78	0.00	0.00	0.00	0.79	0.55	0.78

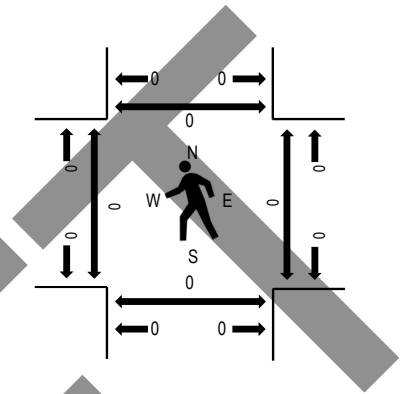
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

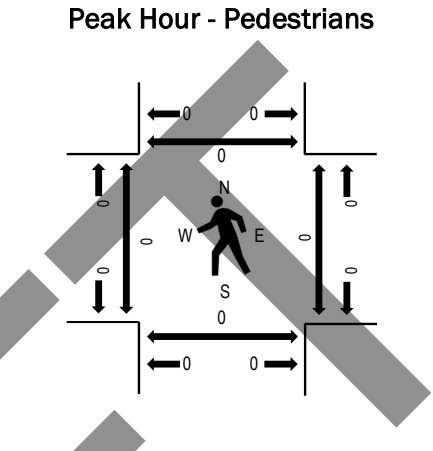
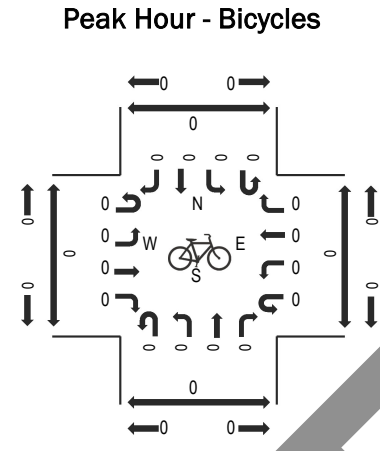
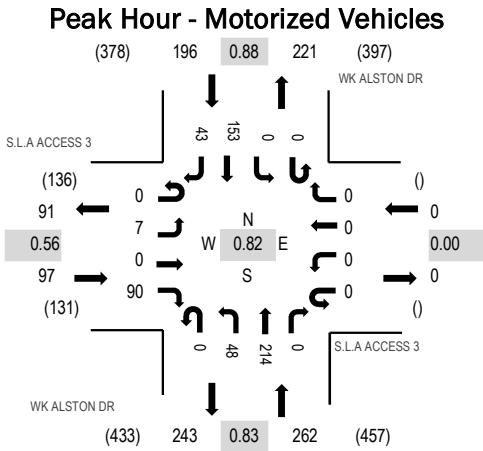
Interval Start Time	S.L.A ACCESS 2 Eastbound				S.L.A ACCESS 2 Westbound				WK ALSTON DR Northbound				WK ALSTON DR Southbound				Total	Rolling Hour	Pedestrian Crossings				
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North	
6:00 AM	0	0	0	0	0	0	0	0	0	0	0	3	0	0	0	9	0	12	82	0	0	0	0
6:15 AM	0	1	0	0	0	0	0	0	0	0	1	8	0	0	0	11	0	21	128	0	0	0	0
6:30 AM	0	0	0	0	0	0	0	0	0	0	0	6	0	0	0	16	0	22	183	0	0	0	0
6:45 AM	0	0	0	0	0	0	0	0	0	0	2	7	0	0	0	18	0	27	234	0	0	0	0
7:00 AM	0	0	0	1	0	0	0	0	0	0	1	27	0	0	0	29	0	58	313	0	0	0	0
7:15 AM	0	0	0	0	0	0	0	0	0	0	2	39	0	0	0	34	1	76	358	0	0	0	0
7:30 AM	0	0	0	0	0	0	0	0	0	0	3	29	0	0	0	36	5	73	447	0	0	0	0
7:45 AM	0	1	0	0	0	0	0	0	0	0	14	30	0	0	0	55	6	106	534	0	0	0	0
8:00 AM	0	5	0	1	0	0	0	0	0	0	12	43	0	0	0	35	7	103	525	0	0	0	0
8:15 AM	0	43	0	14	0	0	0	0	0	0	19	45	0	0	0	42	2	165	0	0	0	0	
8:30 AM	0	47	0	12	0	0	0	0	0	0	25	35	0	0	0	39	2	160	0	0	0	0	
8:45 AM	0	16	0	2	0	0	0	0	0	0	13	34	0	0	0	25	7	97	0	0	0	0	

Peak Rolling Hour Flow Rates

Vehicle Type	Eastbound				Westbound				Northbound				Southbound				Total					
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right						
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Lights	0	95	0	27	0	0	0	0	0	69	149	0	0	0	154	17	511					
Mediums	0	1	0	0	0	0	0	0	0	1	4	0	0	0	17	0	23					
Total	0	96	0	27	0	0	0	0	0	70	153	0	0	0	171	17	534					

Heavy Vehicle Percentage and Peak Hour Factor

	Eastbound				Westbound				Northbound				Southbound				Total					
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right						
Heavy Vehicle %	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%					
Peak Hour Factor	0.59	0.00	0.00	0.52	0.00	0.00	0.00	0.00	0.00	0.70	0.87	0.00	0.00	0.00	0.78	0.71	0.81					



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

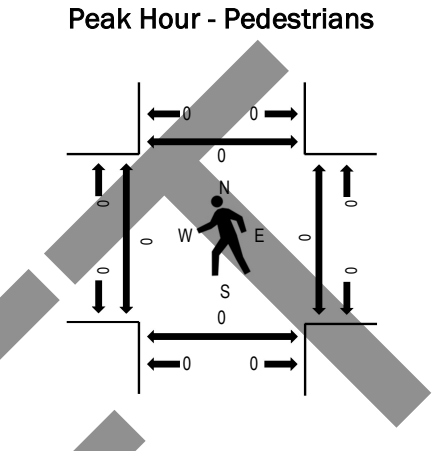
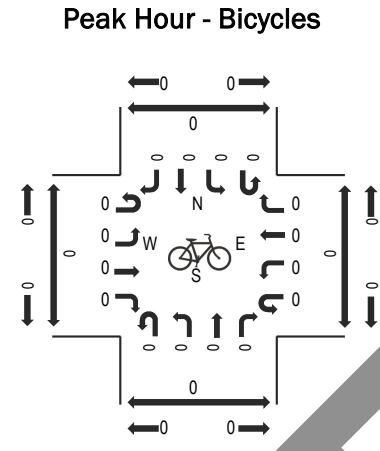
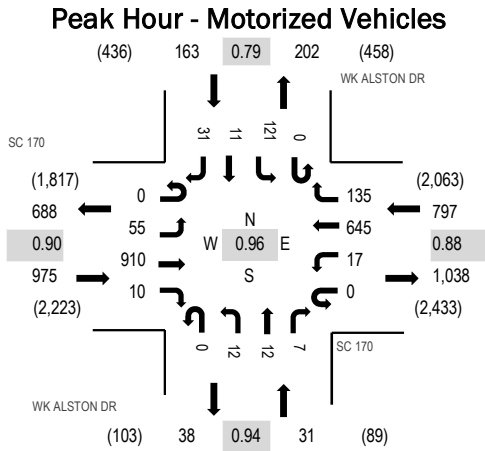
Interval Start Time	S.L.A ACCESS 3 Eastbound				S.L.A ACCESS 3 Westbound				WK ALSTON DR Northbound				WK ALSTON DR Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
6:00 AM	0	0	0	0	0	0	0	0	0	0	3	0	0	0	9	0	12	85	0	0	0	0
6:15 AM	0	0	0	0	0	0	0	0	0	0	10	0	0	0	11	0	21	141	0	0	0	0
6:30 AM	0	0	0	0	0	0	0	0	0	2	6	0	0	0	16	0	24	206	0	0	0	0
6:45 AM	0	0	0	1	0	0	0	0	0	1	8	0	0	0	16	2	28	266	0	0	0	0
7:00 AM	0	4	0	2	0	0	0	0	0	6	26	0	0	0	27	3	68	356	0	0	0	0
7:15 AM	0	4	0	3	0	0	0	0	0	10	35	0	0	0	31	3	86	396	0	0	0	0
7:30 AM	0	3	0	6	0	0	0	0	0	8	31	0	0	0	31	5	84	480	0	0	0	0
7:45 AM	0	4	0	10	0	0	0	0	0	10	40	0	0	0	46	8	118	555	0	0	0	0
8:00 AM	0	1	0	4	0	0	0	0	0	14	53	0	0	0	29	7	108	525	0	0	0	0
8:15 AM	0	0	0	35	0	0	0	0	0	15	64	0	0	0	41	15	170	0	0	0	0	
8:30 AM	0	2	0	41	0	0	0	0	0	9	57	0	0	0	37	13	159	0	0	0	0	
8:45 AM	0	1	0	10	0	0	0	0	0	4	45	0	0	0	27	1	88	0	0	0	0	

Peak Rolling Hour Flow Rates

Vehicle Type	Eastbound				Westbound				Northbound				Southbound				Total					
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right						
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Lights	0	5	0	81	0	0	0	0	0	43	210	0	0	0	143	36	518					
Mediums	0	2	0	9	0	0	0	0	0	5	4	0	0	0	10	7	37					
Total	0	7	0	90	0	0	0	0	0	48	214	0	0	0	153	43	555					

Heavy Vehicle Percentage and Peak Hour Factor

	Eastbound				Westbound				Northbound				Southbound				Total					
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right						
Heavy Vehicle %	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%					
Peak Hour Factor	0.56				0.00				0.83					0.88			0.82					
Peak Hour Factor	0.00	0.94	0.00	0.55	0.00	0.00	0.00	0.00	0.00	0.80	0.86	0.00	0.00	0.00	0.83	0.72	0.82					



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	SC 170 Eastbound				SC 170 Westbound				WK ALSTON DR Northbound				WK ALSTON DR Southbound				Total	Rolling Hour	Pedestrian Crossings				
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North	
6:00 AM	0	1	79	1	0	0	101	2	0	4	0	0	0	0	6	2	2	198	1,062	0	0	0	0
6:15 AM	0	2	119	0	1	3	114	8	0	1	0	1	0	5	0	4	258	1,274	0	0	0	0	
6:30 AM	0	1	109	2	1	2	130	6	0	4	1	0	0	10	4	4	274	1,517	0	0	0	0	
6:45 AM	0	3	152	4	1	4	142	6	0	1	1	2	0	8	2	6	332	1,747	0	0	0	0	
7:00 AM	0	10	176	1	0	4	163	20	0	3	1	5	0	17	1	9	410	1,928	0	0	0	0	
7:15 AM	0	11	213	5	0	2	193	31	0	4	5	1	0	23	4	9	501	1,966	0	0	0	0	
7:30 AM	0	17	250	1	0	4	166	22	0	3	1	2	0	32	0	6	504	1,935	0	0	0	0	
7:45 AM	0	11	258	3	0	4	147	35	0	2	2	0	0	36	3	12	513	1,893	0	0	0	0	
8:00 AM	0	16	189	1	0	7	139	47	0	3	4	4	0	30	4	4	448	1,821	0	0	0	0	
8:15 AM	0	18	170	0	2	4	141	54	0	3	6	2	0	52	2	16	470		0	0	0	0	
8:30 AM	0	12	197	3	0	6	112	48	0	2	6	3	0	54	5	14	462		0	0	0	0	
8:45 AM	0	2	181	5	1	6	141	43	0	4	5	3	0	38	4	8	441		0	0	0	0	

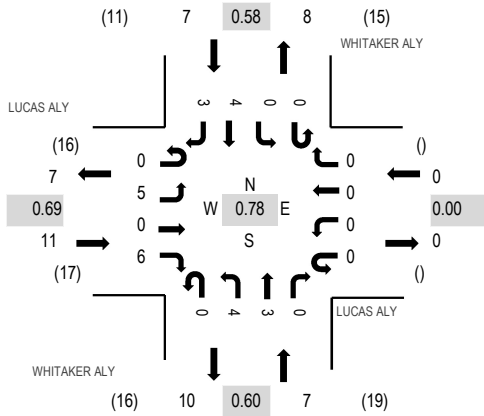
Peak Rolling Hour Flow Rates

Vehicle Type	Eastbound				Westbound				Northbound				Southbound				Total
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	
Articulated Trucks	0	0	5	0	0	0	8	0	0	0	0	0	0	0	1	0	14
Lights	0	54	892	10	0	17	622	132	0	12	11	7	0	120	9	30	1,916
Mediums	0	1	13	0	0	0	15	3	0	0	1	0	0	1	1	1	36
Total	0	55	910	10	0	17	645	135	0	12	12	7	0	121	11	31	1,966

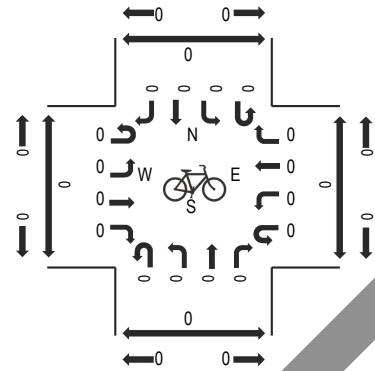
Heavy Vehicle Percentage and Peak Hour Factor

	Eastbound				Westbound				Northbound				Southbound				Total
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	
Heavy Vehicle %		0.5%			1.0%				0.0%					0.6%			0.7%
Heavy Vehicle %	0.0%	0.0%	0.5%	0.0%	0.0%	0.0%	1.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	9.1%	0.0%	0.7%
Peak Hour Factor		0.90			0.88				0.94					0.79			0.96
Peak Hour Factor	0.00	0.86	0.88	0.60	0.38	0.82	0.87	0.89	0.00	0.75	0.88	0.75	0.00	0.81	0.75	0.72	0.96

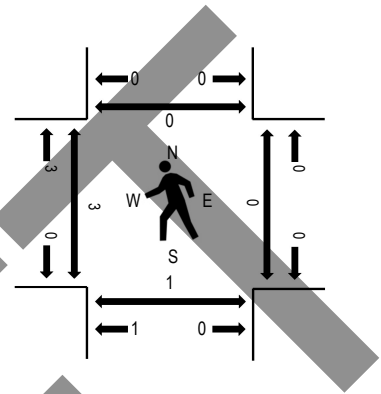
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	LUCAS ALY Eastbound				LUCAS ALY Westbound				WHITAKER ALY Northbound				WHITAKER ALY Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
4:00 PM	0	0	0	2	0	0	0	0	0	0	1	0	0	0	0	0	3	23	0	0	0	0
4:15 PM	0	2	0	2	0	0	0	0	0	1	0	0	0	0	1	0	6	25	0	0	1	0
4:30 PM	0	1	0	1	0	0	0	0	0	1	2	0	0	0	1	2	8	23	0	0	0	0
4:45 PM	0	1	0	2	0	0	0	0	0	1	1	0	0	0	1	0	6	25	2	0	0	0
5:00 PM	0	1	0	1	0	0	0	0	0	1	0	0	0	0	1	1	5	24	1	0	0	0
5:15 PM	0	0	0	1	0	0	0	0	0	2	1	0	0	0	0	0	4		2	0	0	0
5:30 PM	0	2	0	0	0	0	0	0	0	3	2	0	0	0	2	1	10		1	0	0	0
5:45 PM	0	0	0	1	0	0	0	0	0	2	1	0	0	0	0	1	5		0	0	2	0

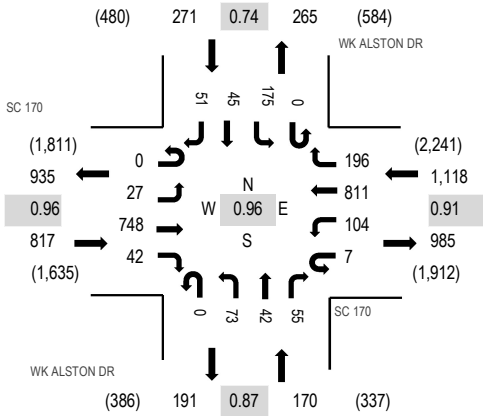
Peak Rolling Hour Flow Rates

Vehicle Type	Eastbound				Westbound				Northbound				Southbound				Total
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Lights	0	5	0	6	0	0	0	0	0	4	3	0	0	0	4	3	25
Mediums	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	5	0	6	0	0	0	0	0	4	3	0	0	0	4	3	25

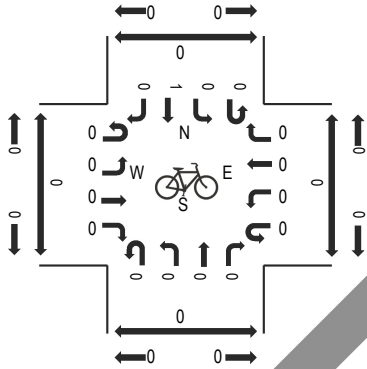
Heavy Vehicle Percentage and Peak Hour Factor

	Eastbound				Westbound				Northbound				Southbound				Total
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	
Heavy Vehicle %																	0.0%
Heavy Vehicle %	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
Peak Hour Factor		0.69				0.00				0.60				0.58			0.78
Peak Hour Factor	0.00	0.63	0.00	0.88	0.00	0.00	0.00	0.00	0.00	0.67	0.50	0.00	0.00	0.00	0.50	0.38	0.78

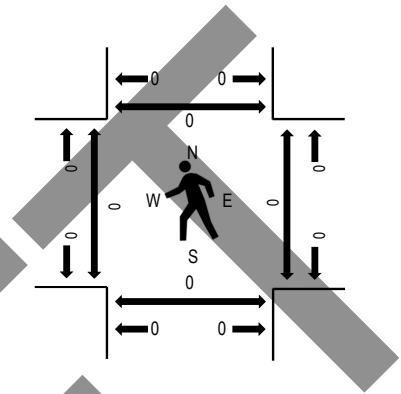
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	SC 170 Eastbound				SC 170 Westbound				WK ALSTON DR Northbound				WK ALSTON DR Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
4:00 PM	0	10	189	10	1	26	195	46	0	26	8	15	0	63	11	17	617	2,376	0	0	0	0
4:15 PM	0	5	189	12	2	26	203	53	0	11	11	13	0	47	10	19	601	2,328	0	0	0	0
4:30 PM	0	6	170	9	1	25	215	47	0	19	14	13	0	36	14	8	577	2,363	0	0	0	0
4:45 PM	0	6	200	11	3	27	198	50	0	17	9	14	0	29	10	7	581	2,374	0	0	0	0
5:00 PM	0	7	168	15	0	39	204	64	0	14	6	13	0	27	8	4	569	2,317	0	0	0	0
5:15 PM	0	9	188	11	1	28	228	75	0	24	8	8	0	42	5	9	636		0	0	0	0
5:30 PM	0	5	190	19	2	14	191	50	0	19	19	13	0	37	12	17	588		0	0	0	0
5:45 PM	0	13	185	8	2	29	143	53	0	20	10	13	0	38	7	3	524		0	0	0	0

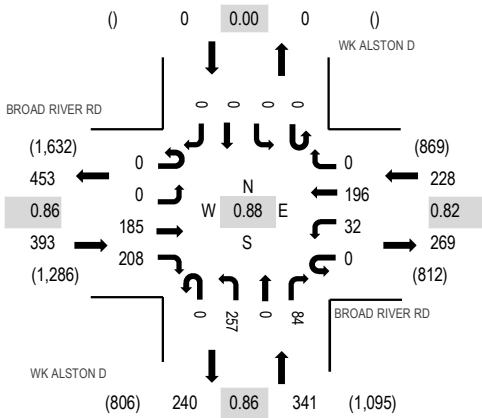
Peak Rolling Hour Flow Rates

Vehicle Type	Eastbound				Westbound				Northbound				Southbound				Total
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	
Articulated Trucks	0	0	7	0	0	0	2	0	0	0	0	1	0	0	0	0	10
Lights	0	27	732	42	7	104	796	193	0	73	42	54	0	168	45	51	2,334
Mediums	0	0	9	0	0	0	13	3	0	0	0	0	0	7	0	0	32
Total	0	27	748	42	7	104	811	196	0	73	42	55	0	175	45	51	2,376

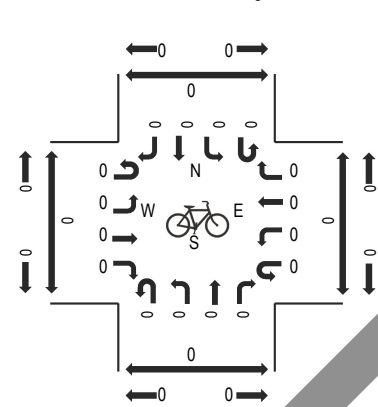
Heavy Vehicle Percentage and Peak Hour Factor

	Eastbound				Westbound				Northbound				Southbound				Total
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	
Heavy Vehicle %	0.9%				0.2%				0.6%				0.0%				0.4%
Heavy Vehicle %	0.0%	0.0%	0.9%	0.0%	0.0%	0.0%	0.2%	0.0%	0.0%	0.0%	0.0%	1.8%	0.0%	0.0%	0.0%	0.0%	0.4%
Peak Hour Factor	0.96				0.91				0.87				0.74				0.96
Peak Hour Factor	0.00	0.65	0.94	0.74	0.58	0.76	0.93	0.81	0.00	0.80	0.57	0.92	0.00	0.69	0.80	0.67	0.96

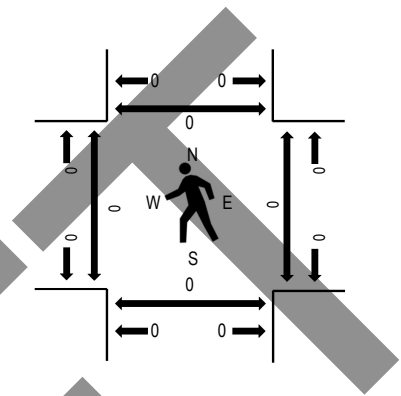
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

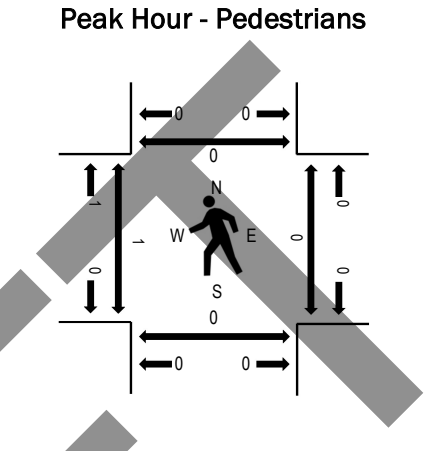
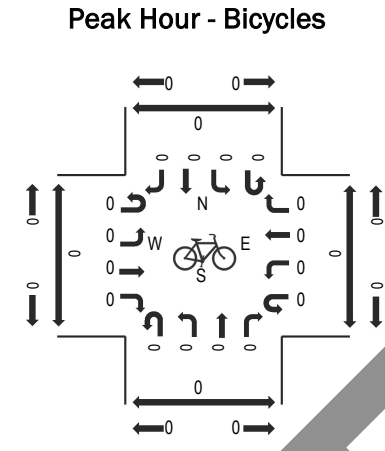
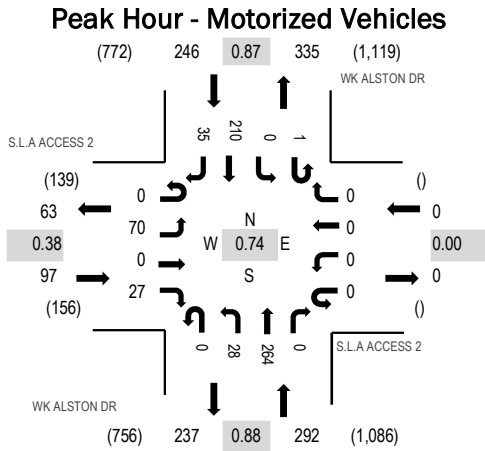
Interval Start Time	BROAD RIVER RD Eastbound				BROAD RIVER RD Westbound				WK ALSTON D Northbound				WK ALSTON D Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
2:00 PM	0	0	27	33	0	2	43	0	0	39	0	5	0	0	0	0	149	633	0	0	0	0
2:15 PM	0	0	34	34	0	6	42	0	0	27	0	5	0	0	0	0	148	675	0	0	0	0
2:30 PM	0	0	35	26	0	5	35	0	0	51	0	12	0	0	0	0	164	719	0	0	0	0
2:45 PM	0	0	43	23	0	4	40	0	0	47	0	15	0	0	0	0	172	759	0	0	0	0
3:00 PM	0	0	39	42	0	10	48	0	0	37	0	15	0	0	0	0	191	859	0	0	0	0
3:15 PM	0	0	32	48	0	9	43	0	0	47	0	13	0	0	0	0	192	921	0	0	0	0
3:30 PM	0	0	36	45	0	15	43	0	0	56	0	9	0	0	0	0	204	940	0	0	0	0
3:45 PM	0	0	51	63	0	10	49	0	0	62	0	37	0	0	0	0	272	962	0	0	0	0
4:00 PM	0	0	44	66	0	6	48	0	0	70	0	19	0	0	0	0	253	907	0	0	0	0
4:15 PM	0	0	47	38	0	7	48	0	0	55	0	16	0	0	0	0	211	898	0	0	0	0
4:30 PM	0	0	43	41	0	9	51	0	0	70	0	12	0	0	0	0	226	887	0	0	0	0
4:45 PM	0	0	37	39	0	8	72	0	0	53	0	8	0	0	0	0	217	870	0	0	0	0
5:00 PM	0	0	38	47	0	13	54	0	0	76	0	16	0	0	0	0	244	851	0	0	0	0
5:15 PM	0	0	31	50	0	7	38	0	0	65	0	9	0	0	0	0	200		0	0	0	0
5:30 PM	0	0	31	51	0	3	54	0	0	51	0	19	0	0	0	0	209		0	0	0	0
5:45 PM	0	0	26	46	0	0	47	0	0	71	0	8	0	0	0	0	198		0	0	0	0

Peak Rolling Hour Flow Rates

Vehicle Type	Eastbound				Westbound				Northbound				Southbound				Total
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	
Articulated Trucks	0	0	2	0	0	0	3	0	0	0	0	0	0	0	0	0	5
Lights	0	0	181	207	0	30	189	0	0	254	0	78	0	0	0	0	939
Mediums	0	0	2	1	0	2	4	0	0	3	0	6	0	0	0	0	18
Total	0	0	185	208	0	32	196	0	0	257	0	84	0	0	0	0	962

Heavy Vehicle Percentage and Peak Hour Factor

	Eastbound				Westbound				Northbound				Southbound				Total
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	
Heavy Vehicle %			0.5%				1.3%					0.0%				0.0%	0.5%
Heavy Vehicle %	0.0%	0.0%	1.1%	0.0%	0.0%	0.0%	1.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.5%
Peak Hour Factor			0.86				0.82					0.86				0.00	0.88
Peak Hour Factor	0.00	0.00	0.91	0.84	0.00	0.73	0.78	0.00	0.00	0.87	0.00	0.57	0.00	0.00	0.00	0.00	0.88



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

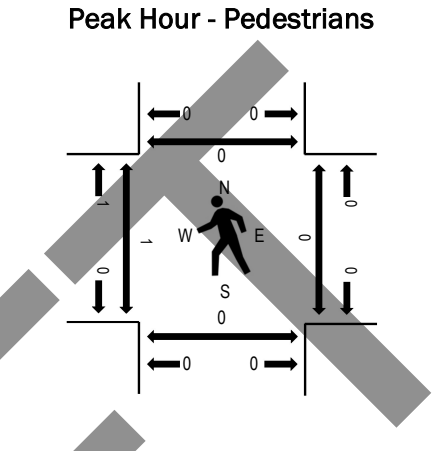
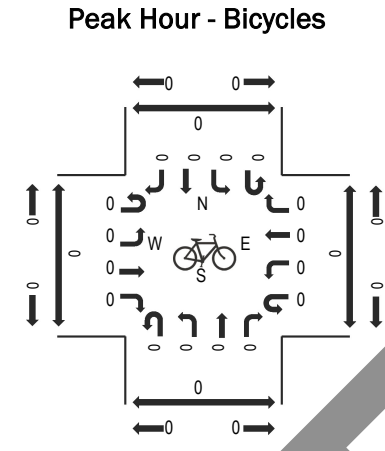
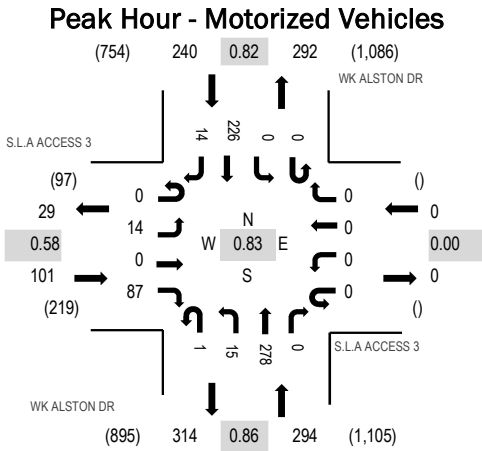
Interval Start Time	S.L.A ACCESS 2 Eastbound				S.L.A ACCESS 2 Westbound				WK ALSTON DR Northbound				WK ALSTON DR Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
2:00 PM	0	1	0	1	0	0	0	0	0	3	42	0	0	0	34	1	82	353	0	0	0	0
2:15 PM	0	4	0	4	0	0	0	0	0	5	30	0	0	0	36	2	81	369	0	0	0	0
2:30 PM	0	2	0	3	0	0	0	0	0	2	63	0	0	0	24	4	98	412	0	0	0	0
2:45 PM	0	3	0	0	0	0	0	0	0	2	60	0	0	0	25	2	92	460	0	0	0	0
3:00 PM	0	1	0	1	0	0	0	0	0	5	48	0	0	0	34	9	98	582	0	0	0	0
3:15 PM	0	0	0	0	0	0	0	0	0	7	66	0	0	0	46	5	124	635	1	0	0	0
3:30 PM	0	4	0	2	0	0	0	0	0	12	73	0	1	0	41	13	146	630	0	0	0	0
3:45 PM	0	49	0	21	0	0	0	0	0	7	68	0	0	0	57	12	214	620	0	0	0	0
4:00 PM	0	17	0	4	0	0	0	0	0	2	57	0	0	0	66	5	151	522	0	0	0	0
4:15 PM	0	6	0	4	0	0	0	0	0	0	64	0	0	0	43	2	119	536	0	0	0	0
4:30 PM	0	1	0	3	0	0	0	0	0	2	80	0	0	0	49	1	136	550	0	0	0	0
4:45 PM	0	1	0	0	0	0	0	0	0	5	63	0	0	0	45	2	116	538	0	0	0	0
5:00 PM	0	10	0	5	0	0	0	0	0	6	85	0	0	0	56	3	165	557	0	0	0	0
5:15 PM	0	3	0	1	0	0	0	0	0	2	69	0	0	0	55	3	133		0	0	0	0
5:30 PM	0	3	0	2	0	0	0	0	0	3	67	0	0	0	47	2	124		0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	10	78	0	0	0	47	0	135		0	0	0	0

Peak Rolling Hour Flow Rates

Vehicle Type	Eastbound				Westbound				Northbound				Southbound				Total
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Lights	0	70	0	27	0	0	0	0	0	27	250	0	1	0	199	35	609
Mediums	0	0	0	0	0	0	0	0	0	1	14	0	0	0	11	0	26
Total	0	70	0	27	0	0	0	0	0	28	264	0	1	0	210	35	635

Heavy Vehicle Percentage and Peak Hour Factor

	Eastbound				Westbound				Northbound				Southbound				Total
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	
Heavy Vehicle %		0.0%			0.0%				0.0%				0.0%				0.0%
Peak Hour Factor	0.00	0.39	0.00	0.38	0.00	0.00	0.00	0.00	0.00	0.65	0.88	0.00	0.25	0.00	0.81	0.75	0.74



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	S.L.A ACCESS 3 Eastbound				S.L.A ACCESS 3 Westbound				WK ALSTON DR Northbound				WK ALSTON DR Southbound				Total	Rolling Hour	Pedestrian Crossings				
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North	
2:00 PM	0	1	0	0	0	0	0	0	0	0	3	45	0	0	0	34	1	84	357	0	0	0	0
2:15 PM	0	0	0	9	0	0	0	0	0	0	0	34	0	0	0	38	2	83	363	0	0	0	0
2:30 PM	0	2	0	7	0	0	0	0	0	0	0	64	0	0	0	27	0	100	410	0	0	0	0
2:45 PM	0	0	0	2	0	0	0	0	0	0	2	61	0	0	0	24	1	90	463	0	0	0	0
3:00 PM	0	1	0	1	0	0	0	0	0	0	4	53	0	0	0	30	1	90	565	0	0	0	0
3:15 PM	0	4	0	6	0	0	0	0	0	0	2	70	0	0	0	42	6	130	635	1	0	0	0
3:30 PM	0	1	0	18	0	0	0	0	0	1	4	84	0	0	0	41	4	153	634	0	0	0	0
3:45 PM	0	8	0	39	0	0	0	0	0	0	4	66	0	0	0	73	2	192	626	0	0	0	0
4:00 PM	0	1	0	24	0	0	0	0	0	0	5	58	0	0	0	70	2	160	562	0	0	0	0
4:15 PM	0	1	0	17	0	0	0	0	0	0	2	63	0	0	0	44	2	129	582	1	0	0	0
4:30 PM	0	2	0	7	0	0	0	0	0	0	4	79	0	0	0	49	4	145	600	0	0	0	0
4:45 PM	0	0	0	8	0	0	0	0	0	0	6	69	0	0	0	42	3	128	583	0	0	0	0
5:00 PM	0	2	0	20	0	0	0	0	0	0	8	88	0	0	0	59	3	180	594	0	0	0	0
5:15 PM	0	5	0	13	0	0	0	0	0	0	7	68	0	0	0	46	8	147		0	0	0	0
5:30 PM	0	6	0	6	0	0	0	0	0	0	2	63	0	0	0	48	3	128		0	0	0	0
5:45 PM	0	1	0	7	0	0	0	0	0	0	0	86	0	0	0	43	2	139		0	0	0	0

Peak Rolling Hour Flow Rates

Vehicle Type	Eastbound				Westbound				Northbound				Southbound				Total	
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right		
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Lights	0	4	0	83	0	0	0	0	1	11	273	0	0	0	223	6	601	
Mediums	0	10	0	4	0	0	0	0	0	4	5	0	0	0	3	8	34	
Total	0	14	0	87	0	0	0	0	1	15	278	0	0	0	226	14	635	

Heavy Vehicle Percentage and Peak Hour Factor

	Eastbound				Westbound				Northbound				Southbound				Total	
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right		
Heavy Vehicle %																		
Heavy Vehicle %	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
Peak Hour Factor		0.58				0.00				0.86				0.82			0.83	
Peak Hour Factor	0.00	0.44	0.00	0.63	0.00	0.00	0.00	0.00	0.25	0.78	0.87	0.00	0.00	0.00	0.81	0.56	0.83	

Appendix B – Traffic Volume Development Worksheets

DRAFT

Growth Rate Calculations

Station ID	Location							Growth
		2019	2020	2021	2022	2023	2024	Short-Term Growth (%)
07-0175	SC170 : S- 20 (BROAD RIVER BLVD) TO US 21 (PARRIS ISLAND GTWY)	17200	16000	18000	18200	20800	20800	3.9%

DRAFT

INTERSECTION TRAFFIC VOLUME DEVELOPMENT

INTERSECTION: Whitaker Aly at Lucas Aly and Site Access #4
COUNT DATE: October 14, 2025
AM PEAK HOUR FACTOR: 0.54 **AM FUTURE PEAK HOUR FACTOR:** 0.54
PM PEAK HOUR FACTOR: 0.78 **PM FUTURE PEAK HOUR FACTOR:** 0.78

AM Peak Hour

AM 2025 EXISTING TRAFFIC	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
AM Adjusted Turning Movement Counts ¹	0	1	0	1	0	0	0	0	0	4	0	0	0	0	4	5
AM Volume Balancing	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
AM 2025 EXISTING TRAFFIC	0	1	0	1	0	0	0	0	0	4	0	0	0	0	4	5
AM Heavy Vehicle Percentage	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%
AM 2030 NO-BUILD TRAFFIC	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Years To Buildout	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5
Annual Growth Rate	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%
AM 2030 NO-BUILD TRAFFIC GROWTH	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	1
AM 2030 NO-BUILD TRAFFIC (No AD)	0	1	0	1	0	0	0	0	0	5	0	0	0	0	5	6
Approved Development 1: The Grove at Broad River																
Approved Development 2: Watercrest Apartments																
Approved Development 3: Ramsey Farms																
TOTAL AM APPROVED DEVELOPMENT TRAFFIC	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
AM 2030 NO-BUILD TRAFFIC	0	1	0	1	0	0	0	0	0	5	0	0	0	0	5	6
Project Trip Distribution	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Net New Distribution	Entering															
	Exiting															
Project Trips	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Project Trip	Net New	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
AM TOTAL PROJECT TRIPS	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
AM 2030 BUILD-OUT TRAFFIC	0	1	0	1	0	0	0	0	0	5	0	0	0	0	5	6

PM Peak Hour

PM 2025 EXISTING TRAFFIC	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
PM Adjusted Turning Movement Counts ¹	0	5	0	6	0	0	0	0	0	4	3	0	0	0	4	3
PM Volume Balancing	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
PM 2025 EXISTING TRAFFIC	0	5	0	6	0	0	0	0	0	4	3	0	0	0	4	3
PM Heavy Vehicle Percentage	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%
PM 2030 NO-BUILD TRAFFIC	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Years To Buildout	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5
Annual Growth Rate	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%
PM 2030 NO-BUILD TRAFFIC GROWTH	0	1	0	1	0	0	0	0	0	1	1	0	0	0	1	1
PM 2030 NO-BUILD TRAFFIC (No AD)	0	6	0	7	0	0	0	0	0	5	4	0	0	0	5	4
Approved Development 1: The Grove at Broad River																
Approved Development 2: Watercrest Apartments																
Approved Development 3: Ramsey Farms																
TOTAL PM APPROVED DEVELOPMENT TRAFFIC	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
PM 2030 NO-BUILD TRAFFIC	0	6	0	7	0	0	0	0	0	5	4	0	0	0	5	4
Project Trip Distribution	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Net New Distribution	Entering															
	Exiting															
Project Trips	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Project Trip	Net New	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
PM TOTAL PROJECT TRIPS	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
PM 2030 BUILD-OUT TRAFFIC	0	6	0	7	0	0	0	0	0	5	4	0	0	0	5	4

INTERSECTION TRAFFIC VOLUME DEVELOPMENT

INTERSECTION: SC 170 (Robert Smalls Parkway) at WK Alston Drive
COUNT DATE: October 14, 2025
AM PEAK HOUR FACTOR: 0.96 **AM FUTURE PEAK HOUR FACTOR:** 0.95
PM PEAK HOUR FACTOR: 0.96 **PM FUTURE PEAK HOUR FACTOR:** 0.95

AM Peak Hour

AM 2025 EXISTING TRAFFIC	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR	
AM Adjusted Turning Movement Counts ¹	0	77	686	13	4	26	473	164	0	21	16	17	0	193	13	54	
AM Volume Balancing	0	0	0	0	-4	4	0	0	0	0	0	0	0	0	0	0	
AM 2025 EXISTING TRAFFIC	0	77	686	13	0	30	473	164	0	21	16	17	0	193	13	54	
AM Heavy Vehicle Percentage	2%	2%	2%	15%	2%	4%	5%	5%	2%	2%	2%	2%	2%	8%	2%	4%	
AM 2030 NO-BUILD TRAFFIC	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR	
Years To Buildout	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	
Annual Growth Rate	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	
AM 2030 NO-BUILD TRAFFIC GROWTH	0	17	149	3	0	6	102	36	0	5	3	4	0	42	3	12	
AM 2030 NO-BUILD TRAFFIC (No AD)	0	94	835	16	0	36	575	200	0	26	19	21	0	235	16	66	
Approved Development 1: The Grove at Broad River		1												2		2	
Approved Development 2: Watercrest Apartments		13					14							20		41	
Approved Development 3: Ramsey Farms		3					3							8		9	
TOTAL AM APPROVED DEVELOPMENT TRAFFIC	0	17	0	0	0	0	0	17	0	0	0	0	0	30	0	52	
AM 2030 NO-BUILD TRAFFIC	0	111	835	16	0	36	575	217	0	26	19	21	0	265	16	118	
Project Trip Distribution	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR	
Net New Distribution	Entering		40%				40%	10%									
	Exiting													50%			
Project Trips	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR	
Project Trip	Net New	0	72	0	0	0	0	72	17	0	0	0	0	0	26	0	0
AM TOTAL PROJECT TRIPS	0	72	0	0	0	0	0	72	17	0	0	0	0	26	0	0	
AM 2030 BUILD-OUT TRAFFIC	0	183	835	16	0	36	647	234	0	26	19	21	0	291	16	118	

PM Peak Hour

PM 2025 EXISTING TRAFFIC	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR	
PM Adjusted Turning Movement Counts ¹	0	27	748	42	7	104	811	196	0	73	42	55	0	175	45	51	
PM Volume Balancing	0	4	0	0	-7	7	0	32	0	0	7	0	0	7	2	2	
PM 2025 EXISTING TRAFFIC	0	31	748	42	0	111	811	228	0	73	49	55	0	182	47	53	
PM Heavy Vehicle Percentage	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	4%	2%	2%	
PM 2030 NO-BUILD TRAFFIC	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR	
Years To Buildout	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	
Annual Growth Rate	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	
PM 2030 NO-BUILD TRAFFIC GROWTH	0	7	162	9	0	24	176	49	0	16	11	12	0	39	10	11	
PM 2030 NO-BUILD TRAFFIC (No AD)	0	38	910	51	0	135	987	277	0	89	60	67	0	221	57	64	
Approved Development 1: The Grove at Broad River		2					2							1		1	
Approved Development 2: Watercrest Apartments		43					41							6		25	
Approved Development 3: Ramsey Farms		10					9							5		6	
TOTAL PM APPROVED DEVELOPMENT TRAFFIC	0	55	0	0	0	0	0	52	0	0	0	0	0	12	0	32	
PM 2030 NO-BUILD TRAFFIC	0	93	910	51	0	135	987	329	0	89	60	67	0	233	57	96	
Project Trip Distribution	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR	
Net New Distribution	Entering		40%				40%	10%									
	Exiting													50%			
Project Trips	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR	
Project Trip	Net New	0	42	0	0	0	0	42	11	0	0	0	0	0	123	0	0
PM TOTAL PROJECT TRIPS	0	42	0	0	0	0	0	42	11	0	0	0	0	123	0	0	
PM 2030 BUILD-OUT TRAFFIC	0	135	910	51	0	135	1,029	340	0	89	60	67	0	356	57	96	

INTERSECTION TRAFFIC VOLUME DEVELOPMENT

INTERSECTION: WK Alston Drive at RSLA Bus Access
COUNT DATE: March 27, 2025
AM PEAK HOUR FACTOR: 0.82 **AM FUTURE PEAK HOUR FACTOR:** 0.82
PM PEAK HOUR FACTOR: 0.83 **PM FUTURE PEAK HOUR FACTOR:** 0.83

AM Peak Hour

AM 2025 EXISTING TRAFFIC	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
AM Adjusted Turning Movement Counts ¹	0	7	0	90	0	0	0	0	0	48	209	0	0	0	170	43
AM Volume Balancing	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
AM 2025 EXISTING TRAFFIC	0	7	0	90	0	0	0	0	0	48	209	0	0	0	170	43
AM Heavy Vehicle Percentage	2%	29%	2%	10%	2%	2%	2%	2%	2%	10%	2%	2%	2%	2%	6%	16%
AM 2030 NO-BUILD TRAFFIC	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Years To Buildout	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5
Annual Growth Rate	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%
AM 2030 NO-BUILD TRAFFIC GROWTH	0	2	0	19	0	0	0	0	0	10	45	0	0	0	37	9
AM 2030 NO-BUILD TRAFFIC (No AD)	0	9	0	109	0	0	0	0	0	58	254	0	0	0	207	52
Approved Development 1: The Grove at Broad River											1				4	
Approved Development 2: Watercrest Apartments											27				61	
Approved Development 3: Ramsey Farms											6				17	
TOTAL AM APPROVED DEVELOPMENT TRAFFIC	0	0	0	0	0	0	0	0	0	0	34	0	0	0	82	0
AM 2030 NO-BUILD TRAFFIC	0	9	0	109	0	0	0	0	0	58	288	0	0	0	289	52
Project Trip Distribution	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Net New Distribution	Entering															10%
	Exiting															10%
Project Trips	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Project Trip	Net New	0	0	0	0	0	0	0	0	0	5	0	0	0	18	0
AM TOTAL PROJECT TRIPS	0	0	0	0	0	0	0	0	0	0	5	0	0	0	18	0
AM 2030 BUILD-OUT TRAFFIC	0	9	0	109	0	0	0	0	0	58	293	0	0	0	307	52

PM Peak Hour

PM 2025 EXISTING TRAFFIC	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
PM Adjusted Turning Movement Counts ¹	0	14	0	87	0	0	0	0	0	16	249	0	0	0	184	14
PM Volume Balancing	0	2	0	0	0	0	0	0	0	0	43	0	0	0	11	2
PM 2025 EXISTING TRAFFIC	0	16	0	87	0	0	0	0	0	16	292	0	0	0	195	16
PM Heavy Vehicle Percentage	2%	71%	2%	5%	2%	2%	2%	2%	2%	25%	2%	2%	2%	2%	2%	57%
PM 2030 NO-BUILD TRAFFIC	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Years To Buildout	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5
Annual Growth Rate	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%
PM 2030 NO-BUILD TRAFFIC GROWTH	0	3	0	19	0	0	0	0	0	3	63	0	0	0	42	3
PM 2030 NO-BUILD TRAFFIC (No AD)	0	19	0	106	0	0	0	0	0	19	355	0	0	0	237	19
Approved Development 1: The Grove at Broad River											4				2	
Approved Development 2: Watercrest Apartments											84				31	
Approved Development 3: Ramsey Farms											19				11	
TOTAL PM APPROVED DEVELOPMENT TRAFFIC	0	0	0	0	0	0	0	0	0	0	107	0	0	0	44	0
PM 2030 NO-BUILD TRAFFIC	0	19	0	106	0	0	0	0	0	19	462	0	0	0	281	19
Project Trip Distribution	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Net New Distribution	Entering															10%
	Exiting															10%
Project Trips	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Project Trip	Net New	0	0	0	0	0	0	0	0	0	24	0	0	0	10	0
PM TOTAL PROJECT TRIPS	0	0	0	0	0	0	0	0	0	0	24	0	0	0	10	0
PM 2030 BUILD-OUT TRAFFIC	0	19	0	106	0	0	0	0	0	19	486	0	0	0	291	19

INTERSECTION TRAFFIC VOLUME DEVELOPMENT

INTERSECTION: WK Alston Drive at RSLA Full-Movement Access
COUNT DATE: March 27, 2025
AM PEAK HOUR FACTOR: 0.81 **AM FUTURE PEAK HOUR FACTOR:** 0.81
PM PEAK HOUR FACTOR: 0.74 **PM FUTURE PEAK HOUR FACTOR:** 0.74

AM Peak Hour

AM 2025 EXISTING TRAFFIC	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
AM Adjusted Turning Movement Counts ¹	0	96	0	27	0	0	0	0	0	70	146	0	0	0	186	17
AM Volume Balancing	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
AM 2025 EXISTING TRAFFIC	0	96	0	27	0	0	0	0	0	70	146	0	0	0	186	17
AM Heavy Vehicle Percentage	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	3%	2%	2%	2%	9%	2%
AM 2030 NO-BUILD TRAFFIC	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Years To Buildout	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5
Annual Growth Rate	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%
AM 2030 NO-BUILD TRAFFIC GROWTH	0	21	0	6	0	0	0	0	0	15	32	0	0	0	40	4
AM 2030 NO-BUILD TRAFFIC (No AD)	0	117	0	33	0	0	0	0	0	85	178	0	0	0	226	21
Approved Development 1: The Grove at Broad River											1				4	
Approved Development 2: Watercrest Apartments											27				61	
Approved Development 3: Ramsey Farms											6				17	
TOTAL AM APPROVED DEVELOPMENT TRAFFIC	0	0	0	0	0	0	0	0	0	0	34	0	0	0	82	0
AM 2030 NO-BUILD TRAFFIC	0	117	0	33	0	0	0	0	0	85	212	0	0	0	308	21
Project Trip Distribution	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Net New Distribution	Entering															10%
	Exiting															10%
Project Trips	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Project Trip	Net New	0	0	0	0	0	0	0	0	0	5	0	0	0	18	0
AM TOTAL PROJECT TRIPS	0	0	0	0	0	0	0	0	0	0	5	0	0	0	18	0
AM 2030 BUILD-OUT TRAFFIC	0	117	0	33	0	0	0	0	0	85	217	0	0	0	326	21

PM Peak Hour

PM 2025 EXISTING TRAFFIC	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
PM Adjusted Turning Movement Counts ¹	0	70	0	27	0	0	0	0	0	28	235	0	0	0	171	35
PM Volume Balancing	0	13	0	0	0	0	0	0	0	0	45	0	0	0	13	3
PM 2025 EXISTING TRAFFIC	0	83	0	27	0	0	0	0	0	28	280	0	0	0	184	38
PM Heavy Vehicle Percentage	2%	2%	2%	2%	2%	2%	2%	2%	2%	4%	6%	2%	2%	2%	6%	2%
PM 2030 NO-BUILD TRAFFIC	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Years To Buildout	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5
Annual Growth Rate	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%
PM 2030 NO-BUILD TRAFFIC GROWTH	0	18	0	6	0	0	0	0	0	6	61	0	0	0	40	8
PM 2030 NO-BUILD TRAFFIC (No AD)	0	101	0	33	0	0	0	0	0	34	341	0	0	0	224	46
Approved Development 1: The Grove at Broad River											4				2	
Approved Development 2: Watercrest Apartments											84				31	
Approved Development 3: Ramsey Farms											19				11	
TOTAL PM APPROVED DEVELOPMENT TRAFFIC	0	0	0	0	0	0	0	0	0	0	107	0	0	0	44	0
PM 2030 NO-BUILD TRAFFIC	0	101	0	33	0	0	0	0	0	34	448	0	0	0	268	46
Project Trip Distribution	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Net New Distribution	Entering															10%
	Exiting															10%
Project Trips	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Project Trip	Net New	0	0	0	0	0	0	0	0	0	24	0	0	0	10	0
PM TOTAL PROJECT TRIPS	0	0	0	0	0	0	0	0	0	0	24	0	0	0	10	0
PM 2030 BUILD-OUT TRAFFIC	0	101	0	33	0	0	0	0	0	34	472	0	0	0	278	46

INTERSECTION TRAFFIC VOLUME DEVELOPMENT

INTERSECTION: WK Alston Drive at RSLA Northern Access
COUNT DATE: March 27, 2025
AM PEAK HOUR FACTOR: 0.78 **AM FUTURE PEAK HOUR FACTOR:** 0.78
PM PEAK HOUR FACTOR: 0.79 **PM FUTURE PEAK HOUR FACTOR:** 0.79

AM Peak Hour

AM 2025 EXISTING TRAFFIC	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
AM Adjusted Turning Movement Counts ¹	0	0	0	0	0	0	0	0	0	29	213	0	0	0	203	87
AM Volume Balancing	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
AM 2025 EXISTING TRAFFIC	0	0	0	0	0	0	0	0	0	29	213	0	0	0	203	87
AM Heavy Vehicle Percentage	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	8%	2%
AM 2030 NO-BUILD TRAFFIC	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Years To Buildout	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5
Annual Growth Rate	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%
AM 2030 NO-BUILD TRAFFIC GROWTH	0	0	0	0	0	0	0	0	0	6	46	0	0	0	44	19
AM 2030 NO-BUILD TRAFFIC (No AD)	0	0	0	0	0	0	0	0	0	35	259	0	0	0	247	106
Approved Development 1: The Grove at Broad River											1				4	
Approved Development 2: Watercrest Apartments						61		4				27		1		
Approved Development 3: Ramsey Farms											6				17	
TOTAL AM APPROVED DEVELOPMENT TRAFFIC	0	0	0	0	0	61	0	4	0	0	7	27	0	1	21	0
AM 2030 NO-BUILD TRAFFIC	0	0	0	0	0	61	0	4	0	35	266	27	0	1	268	106
Project Trip Distribution	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Net New Distribution	Entering															10%
	Exiting											10%				
Project Trips	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Project Trip	Net New	0	0	0	0	0	0	0	0	0	5	0	0	0	18	0
AM TOTAL PROJECT TRIPS	0	0	0	0	0	0	0	0	0	0	5	0	0	0	18	0
AM 2030 BUILD-OUT TRAFFIC	0	0	0	0	0	61	0	4	0	35	271	27	0	1	286	106

PM Peak Hour

PM 2025 EXISTING TRAFFIC	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
PM Adjusted Turning Movement Counts ¹	0	1	0	3	0	0	0	0	0	23	282	0	0	0	203	19
PM Volume Balancing	0	0	0	0	0	0	0	0	0	0	58	0	0	0	16	2
PM 2025 EXISTING TRAFFIC	0	1	0	3	0	0	0	0	0	23	340	0	0	0	219	21
PM Heavy Vehicle Percentage	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	5%	2%	2%	2%	5%	2%
PM 2030 NO-BUILD TRAFFIC	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Years To Buildout	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5
Annual Growth Rate	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%
PM 2030 NO-BUILD TRAFFIC GROWTH	0	0	0	1	0	0	0	0	0	5	74	0	0	0	47	5
PM 2030 NO-BUILD TRAFFIC (No AD)	0	1	0	4	0	0	0	0	0	28	414	0	0	0	266	26
Approved Development 1: The Grove at Broad River											4				2	
Approved Development 2: Watercrest Apartments						31		3				84		4		
Approved Development 3: Ramsey Farms											19				11	
TOTAL PM APPROVED DEVELOPMENT TRAFFIC	0	0	0	0	0	31	0	3	0	0	23	84	0	4	13	0
PM 2030 NO-BUILD TRAFFIC	0	1	0	4	0	31	0	3	0	28	437	84	0	4	279	26
Project Trip Distribution	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Net New Distribution	Entering															10%
	Exiting											10%				
Project Trips	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Project Trip	Net New	0	0	0	0	0	0	0	0	0	24	0	0	0	10	0
PM TOTAL PROJECT TRIPS	0	0	0	0	0	0	0	0	0	0	24	0	0	0	10	0
PM 2030 BUILD-OUT TRAFFIC	0	1	0	4	0	31	0	3	0	28	461	84	0	4	289	26

INTERSECTION TRAFFIC VOLUME DEVELOPMENT

INTERSECTION: WK Alston Drive at Broad River Boulevard
COUNT DATE: March 27, 2025
AM PEAK HOUR FACTOR: 0.82 **AM FUTURE PEAK HOUR FACTOR:** 0.82
PM PEAK HOUR FACTOR: 0.88 **PM FUTURE PEAK HOUR FACTOR:** 0.88

AM Peak Hour

AM 2025 EXISTING TRAFFIC	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
AM Adjusted Turning Movement Counts ¹	0	0	202	221	0	55	104	0	0	155	0	58	0	0	0	0
AM Volume Balancing	0	0	0	11	0	3	0	0	0	0	0	0	0	0	0	0
AM 2025 EXISTING TRAFFIC	0	0	202	232	0	58	104	0	0	155	0	58	0	0	0	0
AM Heavy Vehicle Percentage	2%	2%	2%	7%	2%	2%	2%	2%	2%	3%	2%	2%	2%	2%	2%	2%
AM 2030 NO-BUILD TRAFFIC	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Years To Buildout	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5
Annual Growth Rate	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%
AM 2030 NO-BUILD TRAFFIC GROWTH	0	0	44	50	0	13	23	0	0	34	0	13	0	0	0	0
AM 2030 NO-BUILD TRAFFIC (No AD)	0	0	246	282	0	71	127	0	0	189	0	71	0	0	0	0
Approved Development 1: The Grove at Broad River			2			4	4					1				
Approved Development 2: Watercrest Apartments			1	1			5			4						
Approved Development 3: Ramsey Farms		0	15	11		6	15			4		2				
TOTAL AM APPROVED DEVELOPMENT TRAFFIC	0	0	18	12	0	10	24	0	0	8	0	3	0	0	0	0
AM 2030 NO-BUILD TRAFFIC	0	0	264	294	0	81	151	0	0	197	0	74	0	0	0	0
Project Trip Distribution	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Net New Distribution	Entering			5%		5%										
	Exiting									5%		5%				
Project Trips	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Project Trip	Net New	0	0	0	9	0	9	0	0	0	2	0	3	0	0	0
AM TOTAL PROJECT TRIPS	0	0	0	9	0	9	0	0	0	2	0	3	0	0	0	0
AM 2030 BUILD-OUT TRAFFIC	0	0	264	303	0	90	151	0	0	199	0	77	0	0	0	0

PM Peak Hour

PM 2025 EXISTING TRAFFIC	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
PM Adjusted Turning Movement Counts ¹	0	0	185	208	0	32	196	0	0	257	0	84	0	0	0	0
PM Volume Balancing	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
PM 2025 EXISTING TRAFFIC	0	0	185	208	0	32	196	0	0	257	0	84	0	0	0	0
PM Heavy Vehicle Percentage	2%	2%	2%	2%	2%	6%	4%	2%	2%	2%	2%	7%	2%	2%	2%	2%
PM 2030 NO-BUILD TRAFFIC	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Years To Buildout	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5
Annual Growth Rate	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%
PM 2030 NO-BUILD TRAFFIC GROWTH	0	0	40	45	0	7	42	0	0	56	0	18	0	0	0	0
PM 2030 NO-BUILD TRAFFIC (No AD)	0	0	225	253	0	39	238	0	0	313	0	102	0	0	0	0
Approved Development 1: The Grove at Broad River			5			2	3					4				
Approved Development 2: Watercrest Apartments			5	4			3			3						
Approved Development 3: Ramsey Farms			20	7		4	20			12		7				
TOTAL PM APPROVED DEVELOPMENT TRAFFIC	0	0	30	11	0	6	26	0	0	15	0	11	0	0	0	0
PM 2030 NO-BUILD TRAFFIC	0	0	255	264	0	45	264	0	0	328	0	113	0	0	0	0
Project Trip Distribution	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Net New Distribution	Entering			5%		5%										
	Exiting									5%		5%				
Project Trips	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Project Trip	Net New	0	0	0	5	0	5	0	0	0	12	0	12	0	0	0
PM TOTAL PROJECT TRIPS	0	0	0	5	0	5	0	0	0	12	0	12	0	0	0	0
PM 2030 BUILD-OUT TRAFFIC	0	0	255	269	0	50	264	0	0	340	0	125	0	0	0	0

INTERSECTION TRAFFIC VOLUME DEVELOPMENT

INTERSECTION: WK Alston Drive at Site Access #1
COUNT DATE: October 14, 2025
AM PEAK HOUR FACTOR: 0.90 **AM FUTURE PEAK HOUR FACTOR:** 0.90
PM PEAK HOUR FACTOR: 0.90 **PM FUTURE PEAK HOUR FACTOR:** 0.90

AM Peak Hour

AM 2025 EXISTING TRAFFIC	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
AM Adjusted Turning Movement Counts ¹	0	0	0	0	0	0	0	0	0	0	257	0	0	0	260	0
AM Volume Balancing	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
AM 2025 EXISTING TRAFFIC	0	0	0	0	0	0	0	0	0	0	257	0	0	0	260	0
AM Heavy Vehicle Percentage	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	3%	2%	2%	2%	7%	2%
AM 2030 NO-BUILD TRAFFIC	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Years To Buildout	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5
Annual Growth Rate	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%
AM 2030 NO-BUILD TRAFFIC GROWTH	0	0	0	0	0	0	0	0	0	0	56	0	0	0	56	0
AM 2030 NO-BUILD TRAFFIC (No AD)	0	0	0	0	0	0	0	0	0	0	313	0	0	0	316	0
Approved Development 1: The Grove at Broad River											1				4	
Approved Development 2: Watercrest Apartments											27				61	
Approved Development 3: Ramsey Farms											6				17	
TOTAL AM APPROVED DEVELOPMENT TRAFFIC	0	0	0	0	0	0	0	0	0	0	34	0	0	0	82	0
AM 2030 NO-BUILD TRAFFIC	0	0	0	0	0	0	0	0	0	0	347	0	0	0	398	0
Project Trip Distribution	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Net New Distribution	Entering									50%						10%
	Exiting		10%		50%											
Project Trips	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Project Trip	Net New	0	5	0	26	0	0	0	0	89	0	0	0	0	0	18
AM TOTAL PROJECT TRIPS	0	5	0	26	0	0	0	0	0	89	0	0	0	0	0	18
AM 2030 BUILD-OUT TRAFFIC	0	5	0	26	0	0	0	0	0	89	347	0	0	0	398	18

PM Peak Hour

PM 2025 EXISTING TRAFFIC	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
PM Adjusted Turning Movement Counts ¹	0	0	0	0	0	0	0	0	0	0	265	0	0	0	271	0
PM Volume Balancing	0	0	0	0	0	0	0	0	0	0	43	0	0	0	11	0
PM 2025 EXISTING TRAFFIC	0	0	0	0	0	0	0	0	0	0	308	0	0	0	282	0
PM Heavy Vehicle Percentage	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	3%	2%
PM 2030 NO-BUILD TRAFFIC	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Years To Buildout	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5
Annual Growth Rate	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%	4.0%
PM 2030 NO-BUILD TRAFFIC GROWTH	0	0	0	0	0	0	0	0	0	0	67	0	0	0	61	0
PM 2030 NO-BUILD TRAFFIC (No AD)	0	0	0	0	0	0	0	0	0	0	375	0	0	0	343	0
Approved Development 1: The Grove at Broad River											4				2	
Approved Development 2: Watercrest Apartments											84				31	
Approved Development 3: Ramsey Farms											19				11	
TOTAL PM APPROVED DEVELOPMENT TRAFFIC	0	0	0	0	0	0	0	0	0	0	107	0	0	0	44	0
PM 2030 NO-BUILD TRAFFIC	0	0	0	0	0	0	0	0	0	0	482	0	0	0	387	0
Project Trip Distribution	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Net New Distribution	Entering									50%						10%
	Exiting		10%		50%											
Project Trips	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Project Trip	Net New	0	24	0	123	0	0	0	0	53	0	0	0	0	0	10
PM TOTAL PROJECT TRIPS	0	24	0	123	0	0	0	0	0	53	0	0	0	0	0	10
PM 2030 BUILD-OUT TRAFFIC	0	24	0	123	0	0	0	0	0	53	482	0	0	0	387	10

Appendix C – Capacity Analysis Worksheets

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2025 EXISTING CONDITIONS

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Intersection						
Int Delay, s/veh	3.1					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	W			W	W	
Traffic Vol, veh/h	1	1	4	0	4	5
Future Vol, veh/h	1	1	4	0	4	5
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	54	54	54	54	54	54
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	2	2	7	0	7	9

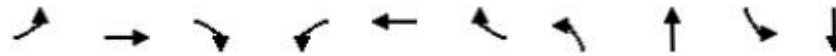
Major/Minor	Minor2	Major1	Major2		
Conflicting Flow All	27	12	17	0	-
Stage 1	12	-	-	-	-
Stage 2	15	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-
Pot Cap-1 Maneuver	988	1069	1601	-	-
Stage 1	1011	-	-	-	-
Stage 2	1008	-	-	-	-
Platoon blocked, %				-	-
Mov Cap-1 Maneuver	984	1069	1601	-	-
Mov Cap-2 Maneuver	984	-	-	-	-
Stage 1	1006	-	-	-	-
Stage 2	1008	-	-	-	-

Approach	EB	NB	SB
HCM Ctrl Dly, s/v	8.53	7.26	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1601	-	1024	-	-
HCM Lane V/C Ratio	0.005	-	0.004	-	-
HCM Ctrl Dly (s/v)	7.3	0	8.5	-	-
HCM Lane LOS	A	A	A	-	-
HCM 95th %tile Q(veh)	0	-	0	-	-

Queues
2: WK Alston Drive & SC 170 (Robert Smalls Parkway)

Beaufort FSED
2025 Existing AM


























Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	SBL	SBT
Lane Group Flow (vph)	80	715	14	31	493	171	22	35	201	70
v/c Ratio	0.17	0.55	0.02	0.08	0.51	0.31	0.04	0.07	0.32	0.12
Control Delay (s/veh)	11.4	20.1	0.1	11.9	22.3	5.8	13.4	17.6	15.4	9.3
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay (s/veh)	11.4	20.1	0.1	11.9	22.3	5.8	13.4	17.6	15.4	9.3
Queue Length 50th (ft)	11	66	0	5	70	0	4	4	47	3
Queue Length 95th (ft)	48	243	0	25	166	46	20	33	123	36
Internal Link Dist (ft)		514			1442			443		414
Turn Bay Length (ft)	225		100	200		200	225		100	
Base Capacity (vph)	525	3182	1275	452	3124	1413	592	604	937	1181
Starvation Cap Reductn	0	0	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.15	0.22	0.01	0.07	0.16	0.12	0.04	0.06	0.21	0.06
Intersection Summary										

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HCM 7th Signalized Intersection Summary
 2: WK Alston Drive & SC 170 (Robert Smalls Parkway)

Beaufort FSED
 2025 Existing AM

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	77	686	13	30	473	164	21	16	17	193	13	54
Future Volume (veh/h)	77	686	13	30	473	164	21	16	17	193	13	54
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1678	1841	1826	1826	1870	1870	1870	1781	1870	1841
Adj Flow Rate, veh/h	80	715	14	31	493	171	22	17	18	201	14	56
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Percent Heavy Veh, %	2	2	15	4	5	5	2	2	2	8	2	4
Cap, veh/h	437	1361	544	362	1386	618	494	157	167	542	94	374
Arrive On Green	0.06	0.38	0.38	0.06	0.40	0.40	0.04	0.19	0.18	0.13	0.29	0.27
Sat Flow, veh/h	1781	3554	1422	1753	3469	1547	1781	831	880	1697	327	1308
Grp Volume(v), veh/h	80	715	14	31	493	171	22	0	35	201	0	70
Grp Sat Flow(s),veh/h/ln	1781	1777	1422	1753	1735	1547	1781	0	1712	1697	0	1635
Q Serve(g_s), s	2.0	12.4	0.5	0.8	7.9	6.0	0.7	0.0	1.4	7.3	0.0	2.6
Cycle Q Clear(g_c), s	2.0	12.4	0.5	0.8	7.9	6.0	0.7	0.0	1.4	7.3	0.0	2.6
Prop In Lane	1.00		1.00	1.00		1.00	1.00		0.51	1.00		0.80
Lane Grp Cap(c), veh/h	437	1361	544	362	1386	618	494	0	324	542	0	468
V/C Ratio(X)	0.18	0.53	0.03	0.09	0.36	0.28	0.04	0.00	0.11	0.37	0.00	0.15
Avail Cap(c_a), veh/h	556	2760	1104	494	2781	1241	618	0	407	942	0	819
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	12.4	19.0	15.4	13.5	16.8	16.2	18.6	0.0	27.0	20.8	0.0	21.5
Incr Delay (d2), s/veh	0.2	0.3	0.0	0.1	0.2	0.2	0.0	0.0	0.1	0.4	0.0	0.1
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.7	4.6	0.2	0.3	2.8	2.1	0.3	0.0	0.6	2.8	0.0	1.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	12.6	19.3	15.4	13.6	16.9	16.4	18.6	0.0	27.1	21.2	0.0	21.7
LnGrp LOS	B	B	B	B	B	B	B		C	C		C
Approach Vol, veh/h		809			695			57				271
Approach Delay, s/veh		18.6			16.7			23.8				21.3
Approach LOS		B			B			C				C
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	10.0	35.6	6.4	27.9	8.7	36.9	14.2	20.1				
Change Period (Y+Rc), s	6.0	6.0	4.5	6.0	4.5	6.0	4.5	6.0				
Max Green Setting (Gmax), s	10.0	61.0	7.5	39.0	9.5	63.0	28.5	18.0				
Max Q Clear Time (g_c+I1), s	2.8	14.4	2.7	4.6	4.0	9.9	9.3	3.4				
Green Ext Time (p_c), s	0.0	15.2	0.0	0.2	0.1	10.1	0.5	0.0				
Intersection Summary												
HCM 7th Control Delay, s/veh			18.4									
HCM 7th LOS			B									

Intersection						
Int Delay, s/veh	2.5					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	W			4	P	
Traffic Vol, veh/h	7	90	48	209	170	43
Future Vol, veh/h	7	90	48	209	170	43
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	82	82	82	82	82	82
Heavy Vehicles, %	29	10	10	2	6	16
Mvmt Flow	9	110	59	255	207	52

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	605	234	260	0	-	0
Stage 1	234	-	-	-	-	-
Stage 2	372	-	-	-	-	-
Critical Hdwy	6.69	6.3	4.2	-	-	-
Critical Hdwy Stg 1	5.69	-	-	-	-	-
Critical Hdwy Stg 2	5.69	-	-	-	-	-
Follow-up Hdwy	3.761	3.39	2.29	-	-	-
Pot Cap-1 Maneuver	419	786	1260	-	-	-
Stage 1	746	-	-	-	-	-
Stage 2	642	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	397	786	1260	-	-	-
Mov Cap-2 Maneuver	397	-	-	-	-	-
Stage 1	705	-	-	-	-	-
Stage 2	642	-	-	-	-	-

Approach	EB	NB	SB
HCM Ctrl Dly, s/v	10.84	1.49	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	336	-	734	-	-
HCM Lane V/C Ratio	0.046	-	0.161	-	-
HCM Ctrl Dly (s/v)	8	0	10.8	-	-
HCM Lane LOS	A	A	B	-	-
HCM 95th %tile Q(veh)	0.1	-	0.6	-	-

Intersection						
Int Delay, s/veh	4.4					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	↖	↗		↑	↘	
Traffic Vol, veh/h	96	27	70	146	186	17
Future Vol, veh/h	96	27	70	146	186	17
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	0	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	81	81	81	81	81	81
Heavy Vehicles, %	2	2	2	3	9	2
Mvmt Flow	119	33	86	180	230	21

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	593	240	251	0	-	0
Stage 1	240	-	-	-	-	-
Stage 2	353	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	468	799	1315	-	-	-
Stage 1	800	-	-	-	-	-
Stage 2	711	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	434	799	1315	-	-	-
Mov Cap-2 Maneuver	434	-	-	-	-	-
Stage 1	741	-	-	-	-	-
Stage 2	711	-	-	-	-	-

Approach	EB	NB	SB
HCM Ctrl Dly, s/v	14.92	2.57	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	EBLn2	SBT	SBR
Capacity (veh/h)	1315	-	434	799	-	-
HCM Lane V/C Ratio	0.066	-	0.273	0.042	-	-
HCM Ctrl Dly (s/v)	7.9	-	16.4	9.7	-	-
HCM Lane LOS	A	-	C	A	-	-
HCM 95th %tile Q(veh)	0.2	-	1.1	0.1	-	-

Intersection						
Int Delay, s/veh	0.4					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	W			T	T	
Traffic Vol, veh/h	0	0	29	213	203	87
Future Vol, veh/h	0	0	29	213	203	87
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	78	78	78	78	78	78
Heavy Vehicles, %	2	2	2	2	8	2
Mvmt Flow	0	0	37	273	260	112

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	663	316	372	0	-	0
Stage 1	316	-	-	-	-	-
Stage 2	347	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	426	724	1187	-	-	-
Stage 1	739	-	-	-	-	-
Stage 2	715	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	410	724	1187	-	-	-
Mov Cap-2 Maneuver	410	-	-	-	-	-
Stage 1	712	-	-	-	-	-
Stage 2	715	-	-	-	-	-

Approach	EB	NB	SB
HCM Ctrl Dly, s/v	0	0.97	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	216	-	-	-	-
HCM Lane V/C Ratio	0.031	-	-	-	-
HCM Ctrl Dly (s/v)	8.1	0	0	-	-
HCM Lane LOS	A	A	A	-	-
HCM 95th %tile Q(veh)	0.1	-	-	-	-

Intersection						
Int Delay, s/veh	5.6					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	1			1	1	1
Traffic Vol, veh/h	202	232	58	104	155	58
Future Vol, veh/h	202	232	58	104	155	58
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	175	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	82	82	82	82	82	82
Heavy Vehicles, %	2	7	2	2	3	2
Mvmt Flow	246	283	71	127	189	71
Major/Minor	Major1	Major2	Minor1			
Conflicting Flow All	0	0	529	0	656	388
Stage 1	-	-	-	-	388	-
Stage 2	-	-	-	-	268	-
Critical Hdwy	-	-	4.12	-	6.43	6.22
Critical Hdwy Stg 1	-	-	-	-	5.43	-
Critical Hdwy Stg 2	-	-	-	-	5.43	-
Follow-up Hdwy	-	-	2.218	-	3.527	3.318
Pot Cap-1 Maneuver	-	-	1038	-	429	660
Stage 1	-	-	-	-	684	-
Stage 2	-	-	-	-	774	-
Platoon blocked, %	-	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1038	-	397	660
Mov Cap-2 Maneuver	-	-	-	-	397	-
Stage 1	-	-	-	-	684	-
Stage 2	-	-	-	-	718	-
Approach	EB	WB	NB			
HCM Ctrl Dly, s/v	0	3.12	19.05			
HCM LOS			C			
Minor Lane/Major Mvmt	NBLn1	NBLn2	EBT	EBR	WBL	WBT
Capacity (veh/h)	397	660	-	-	644	-
HCM Lane V/C Ratio	0.476	0.107	-	-	0.068	-
HCM Ctrl Dly (s/v)	22	11.1	-	-	8.7	0
HCM Lane LOS	C	B	-	-	A	A
HCM 95th %tile Q(veh)	2.5	0.4	-	-	0.2	-

Intersection						
Int Delay, s/veh	4.9					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	W			4	4	
Traffic Vol, veh/h	5	6	4	3	4	3
Future Vol, veh/h	5	6	4	3	4	3
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	78	78	78	78	78	78
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	6	8	5	4	5	4

Major/Minor	Minor2	Major1	Major2		
Conflicting Flow All	21	7	9	0	-
Stage 1	7	-	-	-	-
Stage 2	14	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-
Pot Cap-1 Maneuver	996	1075	1611	-	-
Stage 1	1016	-	-	-	-
Stage 2	1009	-	-	-	-
Platoon blocked, %				-	-
Mov Cap-1 Maneuver	992	1075	1611	-	-
Mov Cap-2 Maneuver	992	-	-	-	-
Stage 1	1013	-	-	-	-
Stage 2	1009	-	-	-	-

Approach	EB	NB	SB
HCM Ctrl Dly, s/v	8.52	4.14	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1029	-	1036	-	-
HCM Lane V/C Ratio	0.003	-	0.014	-	-
HCM Ctrl Dly (s/v)	7.2	0	8.5	-	-
HCM Lane LOS	A	A	A	-	-
HCM 95th %tile Q(veh)	0	-	0	-	-

Queues
2: WK Alston Drive & SC 170 (Robert Smalls Parkway)

Beaufort FSED
2025 Existing PM



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	SBL	SBT
Lane Group Flow (vph)	32	779	44	116	845	238	76	108	190	104
v/c Ratio	0.10	0.77	0.08	0.35	0.58	0.30	0.15	0.27	0.34	0.22
Control Delay (s/veh)	11.6	31.5	0.3	15.4	21.9	3.9	16.5	24.6	18.6	18.4
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay (s/veh)	11.6	31.5	0.3	15.4	21.9	3.9	16.5	24.6	18.6	18.4
Queue Length 50th (ft)	8	185	0	31	191	0	22	31	59	25
Queue Length 95th (ft)	23	284	0	66	278	46	59	91	132	73
Internal Link Dist (ft)		514			1442			443		414
Turn Bay Length (ft)	225		100	200		200	225		100	
Base Capacity (vph)	333	2849	1308	409	3091	1413	526	530	690	894
Starvation Cap Reductn	0	0	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.10	0.27	0.03	0.28	0.27	0.17	0.14	0.20	0.28	0.12
Intersection Summary										

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HCM 7th Signalized Intersection Summary
 2: WK Alston Drive & SC 170 (Robert Smalls Parkway)

Beaufort FSED
 2025 Existing PM

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	31	748	42	111	811	228	73	49	55	182	47	53
Future Volume (veh/h)	31	748	42	111	811	228	73	49	55	182	47	53
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1841	1870	1870
Adj Flow Rate, veh/h	32	779	44	116	845	238	76	51	57	190	49	55
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	4	2	2
Cap, veh/h	310	1393	621	400	1646	734	437	146	163	454	196	220
Arrive On Green	0.04	0.39	0.39	0.10	0.46	0.46	0.06	0.18	0.17	0.12	0.24	0.23
Sat Flow, veh/h	1781	3554	1585	1781	3554	1585	1781	807	901	1753	805	903
Grp Volume(v), veh/h	32	779	44	116	845	238	76	0	108	190	0	104
Grp Sat Flow(s),veh/h/ln	1781	1777	1585	1781	1777	1585	1781	0	1708	1753	0	1708
Q Serve(g_s), s	0.8	15.1	1.5	3.2	14.8	8.4	2.8	0.0	4.9	7.5	0.0	4.4
Cycle Q Clear(g_c), s	0.8	15.1	1.5	3.2	14.8	8.4	2.8	0.0	4.9	7.5	0.0	4.4
Prop In Lane	1.00		1.00	1.00		1.00	1.00		0.53	1.00		0.53
Lane Grp Cap(c), veh/h	310	1393	621	400	1646	734	437	0	308	454	0	416
V/C Ratio(X)	0.10	0.56	0.07	0.29	0.51	0.32	0.17	0.00	0.35	0.42	0.00	0.25
Avail Cap(c_a), veh/h	386	2487	1109	490	2767	1234	503	0	424	724	0	732
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	12.4	21.0	16.8	14.0	16.7	15.0	22.6	0.0	32.0	24.3	0.0	27.2
Incr Delay (d2), s/veh	0.1	0.4	0.0	0.4	0.2	0.3	0.2	0.0	0.7	0.6	0.0	0.3
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.3	5.7	0.6	1.2	5.4	3.0	1.2	0.0	2.1	3.1	0.0	1.8
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	12.5	21.3	16.9	14.4	17.0	15.3	22.8	0.0	32.7	24.9	0.0	27.5
LnGrp LOS	B	C	B	B	B	B	C		C	C		C
Approach Vol, veh/h		855			1199			184				294
Approach Delay, s/veh		20.8			16.4			28.6				25.8
Approach LOS		C			B			C				C
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	13.5	39.7	8.7	26.6	7.2	46.0	14.4	21.0				
Change Period (Y+Rc), s	6.0	6.0	4.5	6.0	4.5	6.0	4.5	6.0				
Max Green Setting (Gmax), s	12.0	61.0	7.5	37.0	6.5	68.0	23.5	21.0				
Max Q Clear Time (g_c+I1), s	5.2	17.1	4.8	6.4	2.8	16.8	9.5	6.9				
Green Ext Time (p_c), s	0.1	16.6	0.0	0.3	0.0	20.4	0.4	0.2				
Intersection Summary												
HCM 7th Control Delay, s/veh					19.9							
HCM 7th LOS					B							

Intersection						
Int Delay, s/veh	2.2					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	W			T	T	
Traffic Vol, veh/h	16	87	16	292	195	16
Future Vol, veh/h	16	87	16	292	195	16
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	83	83	83	83	83	83
Heavy Vehicles, %	71	5	25	2	2	57
Mvmt Flow	19	105	19	352	235	19

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	635	245	254	0	-	0
Stage 1	245	-	-	-	-	-
Stage 2	390	-	-	-	-	-
Critical Hdwy	7.11	6.25	4.35	-	-	-
Critical Hdwy Stg 1	6.11	-	-	-	-	-
Critical Hdwy Stg 2	6.11	-	-	-	-	-
Follow-up Hdwy	4.139	3.345	2.425	-	-	-
Pot Cap-1 Maneuver	350	787	1188	-	-	-
Stage 1	659	-	-	-	-	-
Stage 2	557	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	343	787	1188	-	-	-
Mov Cap-2 Maneuver	343	-	-	-	-	-
Stage 1	646	-	-	-	-	-
Stage 2	557	-	-	-	-	-

Approach	EB	NB	SB
HCM Ctrl Dly, s/v	11.78	0.42	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	94	-	655	-	-
HCM Lane V/C Ratio	0.016	-	0.189	-	-
HCM Ctrl Dly (s/v)	8.1	0	11.8	-	-
HCM Lane LOS	A	A	B	-	-
HCM 95th %tile Q(veh)	0	-	0.7	-	-

Intersection						
Int Delay, s/veh	3.2					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Vol, veh/h	83	27	28	280	184	38
Future Vol, veh/h	83	27	28	280	184	38
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	0	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	74	74	74	74	74	74
Heavy Vehicles, %	2	2	4	6	6	2
Mvmt Flow	112	36	38	378	249	51

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	728	274	300	0	-	0
Stage 1	274	-	-	-	-	-
Stage 2	454	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.14	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.236	-	-	-
Pot Cap-1 Maneuver	390	764	1250	-	-	-
Stage 1	772	-	-	-	-	-
Stage 2	640	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	375	764	1250	-	-	-
Mov Cap-2 Maneuver	375	-	-	-	-	-
Stage 1	742	-	-	-	-	-
Stage 2	640	-	-	-	-	-

Approach	EB	NB	SB
HCM Ctrl Dly, s/v	16.5	0.72	0
HCM LOS	C		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	EBLn2	SBT	SBR
Capacity (veh/h)	164	-	375	764	-	-
HCM Lane V/C Ratio	0.03	-	0.299	0.048	-	-
HCM Ctrl Dly (s/v)	8	0	18.6	9.9	-	-
HCM Lane LOS	A	A	C	A	-	-
HCM 95th %tile Q(veh)	0.1	-	1.2	0.2	-	-

Intersection						
Int Delay, s/veh	0.4					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	W			4	4	
Traffic Vol, veh/h	1	3	23	340	219	21
Future Vol, veh/h	1	3	23	340	219	21
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	79	79	79	79	79	79
Heavy Vehicles, %	2	2	2	5	5	2
Mvmt Flow	1	4	29	430	277	27

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	779	291	304	0	-	0
Stage 1	291	-	-	-	-	-
Stage 2	489	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	364	749	1257	-	-	-
Stage 1	759	-	-	-	-	-
Stage 2	617	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	353	749	1257	-	-	-
Mov Cap-2 Maneuver	353	-	-	-	-	-
Stage 1	736	-	-	-	-	-
Stage 2	617	-	-	-	-	-

Approach	EB	NB	SB
HCM Ctrl Dly, s/v	11.21	0.5	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	114	-	585	-	-
HCM Lane V/C Ratio	0.023	-	0.009	-	-
HCM Ctrl Dly (s/v)	7.9	0	11.2	-	-
HCM Lane LOS	A	A	B	-	-
HCM 95th %tile Q(veh)	0.1	-	0	-	-

Intersection							
Int Delay, s/veh	9						
Movement	EBT	EBR	WBL	WBT	NBL	NBR	
Lane Configurations	1			1	1	1	
Traffic Vol, veh/h	185	208	32	196	257	84	
Future Vol, veh/h	185	208	32	196	257	84	
Conflicting Peds, #/hr	0	0	0	0	0	0	
Sign Control	Free	Free	Free	Free	Stop	Stop	
RT Channelized	-	None	-	None	-	None	
Storage Length	-	-	-	-	175	0	
Veh in Median Storage, #	0	-	-	0	0	-	
Grade, %	0	-	-	0	0	-	
Peak Hour Factor	88	88	88	88	88	88	
Heavy Vehicles, %	2	2	6	4	2	7	
Mvmt Flow	210	236	36	223	292	95	
Major/Minor	Major1	Major2	Minor1				
Conflicting Flow All	0	0	447	0	624	328	
Stage 1	-	-	-	-	328	-	
Stage 2	-	-	-	-	295	-	
Critical Hdwy	-	-	4.16	-	6.42	6.27	
Critical Hdwy Stg 1	-	-	-	-	5.42	-	
Critical Hdwy Stg 2	-	-	-	-	5.42	-	
Follow-up Hdwy	-	-	2.254	-	3.518	3.363	
Pot Cap-1 Maneuver	-	-	1093	-	449	702	
Stage 1	-	-	-	-	730	-	
Stage 2	-	-	-	-	755	-	
Platoon blocked, %	-	-	-	-	-	-	
Mov Cap-1 Maneuver	-	-	1093	-	432	702	
Mov Cap-2 Maneuver	-	-	-	-	432	-	
Stage 1	-	-	-	-	730	-	
Stage 2	-	-	-	-	726	-	
Approach	EB	WB	NB				
HCM Ctrl Dly, s/v	0	1.18	24.55				
HCM LOS				C			
Minor Lane/Major Mvmt	NBLn1	NBLn2	EBT	EBR	WBL	WBT	
Capacity (veh/h)	432	702	-	-	253	-	
HCM Lane V/C Ratio	0.676	0.136	-	-	0.033	-	
HCM Ctrl Dly (s/v)	29	10.9	-	-	8.4	0	
HCM Lane LOS	D	B	-	-	A	A	
HCM 95th %tile Q(veh)	4.9	0.5	-	-	0.1	-	

2030 NO-BUILD CONDITIONS

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Intersection						
Int Delay, s/veh	3					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	W			T	T	
Traffic Vol, veh/h	1	1	5	0	5	6
Future Vol, veh/h	1	1	5	0	5	6
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	54	54	54	54	54	54
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	2	2	9	0	9	11

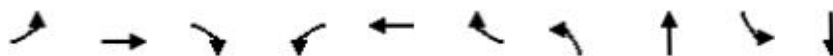
Major/Minor	Minor2	Major1	Major2		
Conflicting Flow All	33	15	20	0	0
Stage 1	15	-	-	-	-
Stage 2	19	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-
Pot Cap-1 Maneuver	980	1065	1596	-	-
Stage 1	1008	-	-	-	-
Stage 2	1004	-	-	-	-
Platoon blocked, %				-	-
Mov Cap-1 Maneuver	974	1065	1596	-	-
Mov Cap-2 Maneuver	974	-	-	-	-
Stage 1	1002	-	-	-	-
Stage 2	1004	-	-	-	-

Approach	EB	NB	SB
HCM Ctrl Dly, s/v	8.55	7.27	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1596	-	1018	-	-
HCM Lane V/C Ratio	0.006	-	0.004	-	-
HCM Ctrl Dly (s/v)	7.3	0	8.6	-	-
HCM Lane LOS	A	A	A	-	-
HCM 95th %tile Q(veh)	0	-	0	-	-

Queues
2: WK Alston Drive & SC 170 (Robert Smalls Parkway)

Beaufort FSED
2030 No-Build AM



























Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	SBL	SBT
Lane Group Flow (vph)	117	879	17	38	605	228	27	42	279	141
v/c Ratio	0.28	0.70	0.03	0.12	0.57	0.36	0.05	0.09	0.45	0.22
Control Delay (s/veh)	14.4	27.0	0.1	14.2	25.7	5.3	16.2	22.4	20.0	7.5
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay (s/veh)	14.4	27.0	0.1	14.2	25.7	5.3	16.2	22.4	20.0	7.5
Queue Length 50th (ft)	34	233	0	11	147	0	8	9	104	5
Queue Length 95th (ft)	74	344	0	32	233	53	27	44	207	53
Internal Link Dist (ft)		514			1442			443		414
Turn Bay Length (ft)	225		100	200		200	225		100	
Base Capacity (vph)	472	2776	1129	373	2719	1264	560	499	917	1072
Starvation Cap Reductn	0	0	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.25	0.32	0.02	0.10	0.22	0.18	0.05	0.08	0.30	0.13
Intersection Summary										

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HCM 7th Signalized Intersection Summary
 2: WK Alston Drive & SC 170 (Robert Smalls Parkway)

Beaufort FSED
 2030 No-Build AM

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	111	835	16	36	575	217	26	19	21	265	16	118
Future Volume (veh/h)	111	835	16	36	575	217	26	19	21	265	16	118
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1678	1841	1826	1826	1870	1870	1870	1781	1870	1841
Adj Flow Rate, veh/h	117	879	17	38	605	228	27	20	22	279	17	124
Peak Hour Factor	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95
Percent Heavy Veh, %	2	2	15	4	5	5	2	2	2	8	2	4
Cap, veh/h	390	1460	584	318	1472	656	427	134	148	549	58	423
Arrive On Green	0.07	0.41	0.41	0.06	0.42	0.42	0.04	0.16	0.15	0.17	0.30	0.29
Sat Flow, veh/h	1781	3554	1422	1753	3469	1547	1781	814	895	1697	195	1420
Grp Volume(v), veh/h	117	879	17	38	605	228	27	0	42	279	0	141
Grp Sat Flow(s),veh/h/ln	1781	1777	1422	1753	1735	1547	1781	0	1709	1697	0	1615
Q Serve(g_s), s	3.5	18.7	0.7	1.1	11.8	9.6	1.0	0.0	2.0	12.7	0.0	6.6
Cycle Q Clear(g_c), s	3.5	18.7	0.7	1.1	11.8	9.6	1.0	0.0	2.0	12.7	0.0	6.6
Prop In Lane	1.00		1.00	1.00		1.00	1.00		0.52	1.00		0.88
Lane Grp Cap(c), veh/h	390	1460	584	318	1472	656	427	0	282	549	0	481
V/C Ratio(X)	0.30	0.60	0.03	0.12	0.41	0.35	0.06	0.00	0.15	0.51	0.00	0.29
Avail Cap(c_a), veh/h	486	2204	882	407	2187	976	481	0	318	831	0	734
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	14.4	22.3	17.0	15.6	19.4	18.8	22.0	0.0	34.8	25.7	0.0	26.5
Incr Delay (d2), s/veh	0.4	0.4	0.0	0.2	0.2	0.3	0.1	0.0	0.2	0.7	0.0	0.3
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	1.3	7.2	0.2	0.4	4.4	3.5	0.4	0.0	0.9	5.0	0.0	2.5
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	14.8	22.7	17.0	15.8	19.6	19.1	22.0	0.0	35.1	26.4	0.0	26.8
LnGrp LOS	B	C	B	B	B	B	C		D	C		C
Approach Vol, veh/h		1013			871			69				420
Approach Delay, s/veh		21.7			19.3			30.0				26.5
Approach LOS		C			B			C				C
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	11.1	44.8	7.1	33.8	9.8	46.0	19.9	20.9				
Change Period (Y+Rc), s	6.0	6.0	4.5	6.0	4.5	6.0	4.5	6.0				
Max Green Setting (Gmax), s	10.0	59.0	5.5	43.0	10.5	60.0	31.5	17.0				
Max Q Clear Time (g_c+I1), s	3.1	20.7	3.0	8.6	5.5	13.8	14.7	4.0				
Green Ext Time (p_c), s	0.0	18.0	0.0	0.5	0.1	12.9	0.8	0.1				
Intersection Summary												
HCM 7th Control Delay, s/veh			21.9									
HCM 7th LOS			C									

Intersection						
Int Delay, s/veh	2.5					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	W			4	4	
Traffic Vol, veh/h	9	109	58	288	289	52
Future Vol, veh/h	9	109	58	288	289	52
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	82	82	82	82	82	82
Heavy Vehicles, %	29	10	10	2	6	16
Mvmt Flow	11	133	71	351	352	63

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	877	384	416	0	-	0
Stage 1	384	-	-	-	-	-
Stage 2	493	-	-	-	-	-
Critical Hdwy	6.69	6.3	4.2	-	-	-
Critical Hdwy Stg 1	5.69	-	-	-	-	-
Critical Hdwy Stg 2	5.69	-	-	-	-	-
Follow-up Hdwy	3.761	3.39	2.29	-	-	-
Pot Cap-1 Maneuver	287	646	1101	-	-	-
Stage 1	633	-	-	-	-	-
Stage 2	562	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	264	646	1101	-	-	-
Mov Cap-2 Maneuver	264	-	-	-	-	-
Stage 1	583	-	-	-	-	-
Stage 2	562	-	-	-	-	-

Approach	EB	NB	SB
HCM Ctrl Dly, s/v	13.21	1.42	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	302	-	582	-	-
HCM Lane V/C Ratio	0.064	-	0.247	-	-
HCM Ctrl Dly (s/v)	8.5	0	13.2	-	-
HCM Lane LOS	A	A	B	-	-
HCM 95th %tile Q(veh)	0.2	-	1	-	-

Intersection						
Int Delay, s/veh	5.7					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Vol, veh/h	117	33	85	212	308	21
Future Vol, veh/h	117	33	85	212	308	21
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	0	100	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	81	81	81	81	81	81
Heavy Vehicles, %	2	2	2	3	9	2
Mvmt Flow	144	41	105	262	380	26

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	865	393	406	0	-	0
Stage 1	393	-	-	-	-	-
Stage 2	472	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	324	656	1153	-	-	-
Stage 1	682	-	-	-	-	-
Stage 2	628	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	295	656	1153	-	-	-
Mov Cap-2 Maneuver	295	-	-	-	-	-
Stage 1	620	-	-	-	-	-
Stage 2	628	-	-	-	-	-

Approach	EB	NB	SB
HCM Ctrl Dly, s/v	24.55	2.41	0
HCM LOS	C		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	EBLn2	SBT	SBR
Capacity (veh/h)	1153	-	295	656	-	-
HCM Lane V/C Ratio	0.091	-	0.49	0.062	-	-
HCM Ctrl Dly (s/v)	8.4	-	28.4	10.9	-	-
HCM Lane LOS	A	-	D	B	-	-
HCM 95th %tile Q(veh)	0.3	-	2.5	0.2	-	-

Intersection												
Int Delay, s/veh	2.2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕	↕		↕	↕
Traffic Vol, veh/h	0	0	0	61	0	4	35	266	27	1	268	106
Future Vol, veh/h	0	0	0	61	0	4	35	266	27	1	268	106
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	100	-	-	100
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	78	78	78	78	78	78	78	78	78	78	78	78
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	8	2
Mvmt Flow	0	0	0	78	0	5	45	341	35	1	344	136

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	777	812	344	777	913	341	479	0	0	376	0	0
Stage 1	346	346	-	431	431	-	-	-	-	-	-	-
Stage 2	431	465	-	346	482	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	314	313	699	314	273	701	1083	-	-	1183	-	-
Stage 1	670	635	-	603	583	-	-	-	-	-	-	-
Stage 2	603	563	-	670	553	-	-	-	-	-	-	-
Platoon blocked, %												
Mov Cap-1 Maneuver	295	296	699	297	259	701	1083	-	-	1183	-	-
Mov Cap-2 Maneuver	295	296	-	297	259	-	-	-	-	-	-	-
Stage 1	669	634	-	571	552	-	-	-	-	-	-	-
Stage 2	567	533	-	669	552	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Ctrl Dly, s/v	0	20.96	0.9	0.02
HCM LOS	A	C		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	209	-	-	-	308	1183	-
HCM Lane V/C Ratio	0.041	-	-	-	0.27	0.001	-
HCM Ctrl Dly (s/v)	8.5	0	-	0	21	8	-
HCM Lane LOS	A	A	-	A	C	A	-
HCM 95th %tile Q(veh)	0.1	-	-	-	1.1	0	-

Intersection							
Int Delay, s/veh	7.8						
Movement	EBT	EBR	WBL	WBT	NBL	NBR	
Lane Configurations	↑	↑		↓	↓	↓	
Traffic Vol, veh/h	264	294	81	151	197	74	
Future Vol, veh/h	264	294	81	151	197	74	
Conflicting Peds, #/hr	0	0	0	0	0	0	
Sign Control	Free	Free	Free	Free	Stop	Stop	
RT Channelized	-	None	-	None	-	None	
Storage Length	-	200	-	-	175	0	
Veh in Median Storage, #	0	-	-	0	0	-	
Grade, %	0	-	-	0	0	-	
Peak Hour Factor	82	82	82	82	82	82	
Heavy Vehicles, %	2	7	2	2	3	2	
Mvmt Flow	322	359	99	184	240	90	
Major/Minor	Major1	Major2	Minor1				
Conflicting Flow All	0	0	680	0	704	322	
Stage 1	-	-	-	-	322	-	
Stage 2	-	-	-	-	382	-	
Critical Hdwy	-	-	4.12	-	6.43	6.22	
Critical Hdwy Stg 1	-	-	-	-	5.43	-	
Critical Hdwy Stg 2	-	-	-	-	5.43	-	
Follow-up Hdwy	-	-	2.218	-	3.527	3.318	
Pot Cap-1 Maneuver	-	-	912	-	402	719	
Stage 1	-	-	-	-	732	-	
Stage 2	-	-	-	-	688	-	
Platoon blocked, %	-	-	-	-	-	-	
Mov Cap-1 Maneuver	-	-	912	-	353	719	
Mov Cap-2 Maneuver	-	-	-	-	353	-	
Stage 1	-	-	-	-	732	-	
Stage 2	-	-	-	-	605	-	
Approach	EB	WB	NB				
HCM Ctrl Dly, s/v	0	3.29	27.84				
HCM LOS				D			
Minor Lane/Major Mvmt	NBLn1	NBLn2	EBT	EBR	WBL	WBT	
Capacity (veh/h)	353	719	-	-	628	-	
HCM Lane V/C Ratio	0.68	0.126	-	-	0.108	-	
HCM Ctrl Dly (s/v)	34.3	10.7	-	-	9.4	0	
HCM Lane LOS	D	B	-	-	A	A	
HCM 95th %tile Q(veh)	4.8	0.4	-	-	0.4	-	

Intersection						
Int Delay, s/veh	4.8					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	W			W	W	
Traffic Vol, veh/h	6	7	5	4	5	4
Future Vol, veh/h	6	7	5	4	5	4
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	78	78	78	78	78	78
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	8	9	6	5	6	5

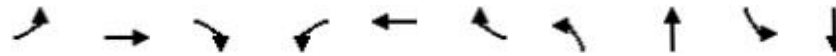
Major/Minor	Minor2	Major1	Major2		
Conflicting Flow All	27	9	12	0	0
Stage 1	9	-	-	-	-
Stage 2	18	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-
Pot Cap-1 Maneuver	988	1073	1607	-	-
Stage 1	1014	-	-	-	-
Stage 2	1005	-	-	-	-
Platoon blocked, %				-	-
Mov Cap-1 Maneuver	984	1073	1607	-	-
Mov Cap-2 Maneuver	984	-	-	-	-
Stage 1	1010	-	-	-	-
Stage 2	1005	-	-	-	-

Approach	EB	NB	SB
HCM Ctrl Dly, s/v	8.55	4.03	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1000	-	1030	-	-
HCM Lane V/C Ratio	0.004	-	0.016	-	-
HCM Ctrl Dly (s/v)	7.2	0	8.6	-	-
HCM Lane LOS	A	A	A	-	-
HCM 95th %tile Q(veh)	0	-	0	-	-

Queues
2: WK Alston Drive & SC 170 (Robert Smalls Parkway)

Beaufort FSED
2030 No-Build PM



























Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	SBL	SBT
Lane Group Flow (vph)	98	958	54	142	1039	346	94	134	245	161
v/c Ratio	0.38	0.83	0.09	0.49	0.81	0.46	0.19	0.40	0.47	0.29
Control Delay (s/veh)	16.6	36.5	0.3	20.2	33.0	7.6	21.2	33.3	24.6	20.2
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay (s/veh)	16.6	36.5	0.3	20.2	33.0	7.6	21.2	33.3	24.6	20.2
Queue Length 50th (ft)	27	265	0	42	282	28	34	51	98	46
Queue Length 95th (ft)	66	431	0	95	448	109	87	141	213	122
Internal Link Dist (ft)		514			1442			443		414
Turn Bay Length (ft)	225		100	200		200	225		100	
Base Capacity (vph)	305	2325	1084	368	2519	1205	486	402	633	755
Starvation Cap Reductn	0	0	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.32	0.41	0.05	0.39	0.41	0.29	0.19	0.33	0.39	0.21
Intersection Summary										

DRAFT

HCM 7th Signalized Intersection Summary
 2: WK Alston Drive & SC 170 (Robert Smalls Parkway)

Beaufort FSED
 2030 No-Build PM

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	93	910	51	135	987	329	89	60	67	233	57	96
Future Volume (veh/h)	93	910	51	135	987	329	89	60	67	233	57	96
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1841	1870	1870
Adj Flow Rate, veh/h	98	958	54	142	1039	346	94	63	71	245	60	101
Peak Hour Factor	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	4	2	2
Cap, veh/h	270	1533	684	347	1694	755	378	125	141	432	150	252
Arrive On Green	0.06	0.43	0.43	0.09	0.48	0.48	0.06	0.16	0.15	0.15	0.24	0.23
Sat Flow, veh/h	1781	3554	1585	1781	3554	1585	1781	803	905	1753	626	1054
Grp Volume(v), veh/h	98	958	54	142	1039	346	94	0	134	245	0	161
Grp Sat Flow(s),veh/h/ln	1781	1777	1585	1781	1777	1585	1781	0	1708	1753	0	1681
Q Serve(g_s), s	2.8	21.6	2.1	4.3	22.3	15.1	4.0	0.0	7.4	11.7	0.0	8.3
Cycle Q Clear(g_c), s	2.8	21.6	2.1	4.3	22.3	15.1	4.0	0.0	7.4	11.7	0.0	8.3
Prop In Lane	1.00		1.00	1.00		1.00	1.00		0.53	1.00		0.63
Lane Grp Cap(c), veh/h	270	1533	684	347	1694	755	378	0	265	432	0	402
V/C Ratio(X)	0.36	0.63	0.08	0.41	0.61	0.46	0.25	0.00	0.51	0.57	0.00	0.40
Avail Cap(c_a), veh/h	371	2071	924	453	2243	1001	393	0	332	624	0	637
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	15.2	22.8	17.2	16.1	19.9	18.0	26.8	0.0	40.1	29.4	0.0	33.3
Incr Delay (d2), s/veh	0.8	0.4	0.0	0.8	0.4	0.4	0.3	0.0	1.5	1.2	0.0	0.6
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	1.1	8.4	0.8	1.6	8.5	5.5	1.7	0.0	3.3	4.9	0.0	3.4
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	16.0	23.2	17.3	16.9	20.3	18.5	27.1	0.0	41.6	30.6	0.0	33.9
LnGrp LOS	B	C	B	B	C	B	C		D	C		C
Approach Vol, veh/h		1110			1527			228				406
Approach Delay, s/veh		22.3			19.6			35.6				31.9
Approach LOS		C			B			D				C
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	13.9	49.4	10.1	29.6	9.2	54.1	18.7	21.0				
Change Period (Y+Rc), s	6.0	6.0	4.5	6.0	4.5	6.0	4.5	6.0				
Max Green Setting (Gmax), s	14.0	59.0	6.5	38.0	10.5	64.0	25.5	19.0				
Max Q Clear Time (g_c+I1), s	6.3	23.6	6.0	10.3	4.8	24.3	13.7	9.4				
Green Ext Time (p_c), s	0.2	19.2	0.0	0.5	0.1	23.8	0.6	0.2				
Intersection Summary												
HCM 7th Control Delay, s/veh			23.1									
HCM 7th LOS			C									

Intersection						
Int Delay, s/veh	2.2					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	W			T	T	
Traffic Vol, veh/h	19	106	19	462	281	19
Future Vol, veh/h	19	106	19	462	281	19
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	83	83	83	83	83	83
Heavy Vehicles, %	71	5	25	2	2	57
Mvmt Flow	23	128	23	557	339	23

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	952	350	361	0	-	0
Stage 1	350	-	-	-	-	-
Stage 2	602	-	-	-	-	-
Critical Hdwy	7.11	6.25	4.35	-	-	-
Critical Hdwy Stg 1	6.11	-	-	-	-	-
Critical Hdwy Stg 2	6.11	-	-	-	-	-
Follow-up Hdwy	4.139	3.345	2.425	-	-	-
Pot Cap-1 Maneuver	218	687	1081	-	-	-
Stage 1	583	-	-	-	-	-
Stage 2	434	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	211	687	1081	-	-	-
Mov Cap-2 Maneuver	211	-	-	-	-	-
Stage 1	565	-	-	-	-	-
Stage 2	434	-	-	-	-	-

Approach	EB	NB	SB
HCM Ctrl Dly, s/v	14.94	0.33	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	71	-	512	-	-
HCM Lane V/C Ratio	0.021	-	0.294	-	-
HCM Ctrl Dly (s/v)	8.4	0	14.9	-	-
HCM Lane LOS	A	A	B	-	-
HCM 95th %tile Q(veh)	0.1	-	1.2	-	-

Intersection						
Int Delay, s/veh	5.2					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	↖	↗	↖	↖	↗	↗
Traffic Vol, veh/h	101	33	34	448	268	46
Future Vol, veh/h	101	33	34	448	268	46
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	0	100	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	74	74	74	74	74	74
Heavy Vehicles, %	2	2	4	6	6	2
Mvmt Flow	136	45	46	605	362	62

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1091	393	424	0	-	0
Stage 1	393	-	-	-	-	-
Stage 2	697	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.14	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.236	-	-	-
Pot Cap-1 Maneuver	238	656	1124	-	-	-
Stage 1	682	-	-	-	-	-
Stage 2	494	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	228	656	1124	-	-	-
Mov Cap-2 Maneuver	228	-	-	-	-	-
Stage 1	654	-	-	-	-	-
Stage 2	494	-	-	-	-	-

Approach	EB	NB	SB
HCM Ctrl Dly, s/v	34.19	0.59	0
HCM LOS	D		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	EBLn2	SBT	SBR
Capacity (veh/h)	1124	-	228	656	-	-
HCM Lane V/C Ratio	0.041	-	0.598	0.068	-	-
HCM Ctrl Dly (s/v)	8.3	-	41.8	10.9	-	-
HCM Lane LOS	A	-	E	B	-	-
HCM 95th %tile Q(veh)	0.1	-	3.4	0.2	-	-

Intersection												
Int Delay, s/veh	1.3											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		+			+			+	+		+	+
Traffic Vol, veh/h	1	0	4	31	0	3	28	437	84	4	279	26
Future Vol, veh/h	1	0	4	31	0	3	28	437	84	4	279	26
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	100	-	-	100
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	79	79	79	79	79	79	79	79	79	79	79	79
Heavy Vehicles, %	2	2	2	2	2	2	2	5	2	2	5	2
Mvmt Flow	1	0	5	39	0	4	35	553	106	5	353	33

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	987	1094	353	987	1020	553	386	0	0	659	0	0
Stage 1	363	363	-	624	624	-	-	-	-	-	-	-
Stage 2	624	730	-	363	396	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	226	214	691	226	237	533	1172	-	-	929	-	-
Stage 1	656	624	-	473	478	-	-	-	-	-	-	-
Stage 2	473	428	-	656	604	-	-	-	-	-	-	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	212	202	691	212	223	533	1172	-	-	929	-	-
Mov Cap-2 Maneuver	212	202	-	212	223	-	-	-	-	-	-	-
Stage 1	651	620	-	450	455	-	-	-	-	-	-	-
Stage 2	447	407	-	646	600	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Ctrl Dly, s/v	12.66	24.84	0.42	0.12
HCM LOS	B	C		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	108	-	-	476	224	929	-
HCM Lane V/C Ratio	0.03	-	-	0.013	0.192	0.005	-
HCM Ctrl Dly (s/v)	8.2	0	-	12.7	24.8	8.9	-
HCM Lane LOS	A	A	-	B	C	A	-
HCM 95th %tile Q(veh)	0.1	-	-	0	0.7	0	-

Intersection						
Int Delay, s/veh	20					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑	↑		↓	↓	↑
Traffic Vol, veh/h	255	264	45	264	328	113
Future Vol, veh/h	255	264	45	264	328	113
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	200	-	-	175	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	88	88	88	88	88	88
Heavy Vehicles, %	2	2	6	4	2	7
Mvmt Flow	290	300	51	300	373	128

Major/Minor	Major1	Major2	Minor1	Minor2
Conflicting Flow All	0	0	590	290
Stage 1	-	-	-	290
Stage 2	-	-	-	402
Critical Hdwy	-	4.16	-	6.27
Critical Hdwy Stg 1	-	-	-	5.42
Critical Hdwy Stg 2	-	-	-	5.42
Follow-up Hdwy	-	2.254	-	3.518
Pot Cap-1 Maneuver	-	966	-	738
Stage 1	-	-	-	760
Stage 2	-	-	-	675
Platoon blocked, %	-	-	-	-
Mov Cap-1 Maneuver	-	966	-	738
Mov Cap-2 Maneuver	-	-	-	384
Stage 1	-	-	-	760
Stage 2	-	-	-	632

Approach	EB	WB	NB
HCM Ctrl Dly, s/v	0	1.3	56.51
HCM LOS			F

Minor Lane/Major Mvmt	NBLn1	NBLn2	EBT	EBR	WBL	WBT
Capacity (veh/h)	384	738	-	-	262	-
HCM Lane V/C Ratio	0.971	0.174	-	-	0.053	-
HCM Ctrl Dly (s/v)	72.2	10.9	-	-	8.9	0
HCM Lane LOS	F	B	-	-	A	A
HCM 95th %tile Q(veh)	11.1	0.6	-	-	0.2	-

2030 BUILD CONDITIONS

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Intersection												
Int Delay, s/veh	1.9											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	1	0	1	0	1	0	5	0	0	0	5	6
Future Vol, veh/h	1	0	1	0	1	0	5	0	0	0	5	6
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	54	54	54	54	54	54	54	54	54	54	54	54
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	2	0	2	0	2	0	9	0	0	0	9	11

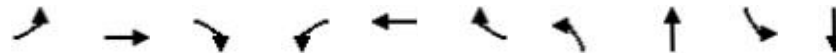
Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	34	33	15	28	39	0	20	0	0	0	0	0
Stage 1	15	15	-	19	19	-	-	-	-	-	-	-
Stage 2	19	19	-	9	20	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	972	859	1065	982	853	-	1596	-	-	-	-	-
Stage 1	1005	883	-	1001	880	-	-	-	-	-	-	-
Stage 2	999	880	-	1012	878	-	-	-	-	-	-	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	-	854	1065	974	848	-	1596	-	-	-	-	-
Mov Cap-2 Maneuver	-	854	-	974	848	-	-	-	-	-	-	-
Stage 1	1005	883	-	995	875	-	-	-	-	-	-	-
Stage 2	992	875	-	1010	878	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Ctrl Dly, s/v			7.27	0
HCM LOS	-	-		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	1596	-	-	-	-	-	-
HCM Lane V/C Ratio	0.006	-	-	-	-	-	-
HCM Ctrl Dly (s/v)	7.3	0	-	-	0	-	-
HCM Lane LOS	A	A	-	-	A	-	-
HCM 95th %tile Q(veh)	0	-	-	-	-	-	-

Queues
2: WK Alston Drive & SC 170 (Robert Smalls Parkway)

Beaufort FSED
2030 Build AM



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	SBL	SBT
Lane Group Flow (vph)	193	879	17	38	681	246	27	42	306	141
v/c Ratio	0.46	0.62	0.03	0.12	0.71	0.41	0.05	0.11	0.53	0.23
Control Delay (s/veh)	17.0	25.7	0.1	14.6	32.1	6.1	16.5	23.5	22.3	7.4
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay (s/veh)	17.0	25.7	0.1	14.6	32.1	6.1	16.5	23.5	22.3	7.4
Queue Length 50th (ft)	60	238	0	11	182	0	8	9	118	5
Queue Length 95th (ft)	122	357	0	33	300	60	28	47	236	53
Internal Link Dist (ft)		514			1442			443		414
Turn Bay Length (ft)	225		100	200		200	225		100	
Base Capacity (vph)	600	2683	1098	370	2342	1126	521	441	855	1050
Starvation Cap Reductn	0	0	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.32	0.33	0.02	0.10	0.29	0.22	0.05	0.10	0.36	0.13
Intersection Summary										

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HCM 7th Signalized Intersection Summary
 2: WK Alston Drive & SC 170 (Robert Smalls Parkway)

Beaufort FSED
 2030 Build AM

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	183	835	16	36	647	234	26	19	21	291	16	118
Future Volume (veh/h)	183	835	16	36	647	234	26	19	21	291	16	118
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1678	1841	1826	1826	1870	1870	1870	1781	1870	1841
Adj Flow Rate, veh/h	193	879	17	38	681	246	27	20	22	306	17	124
Peak Hour Factor	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95
Percent Heavy Veh, %	2	2	15	4	5	5	2	2	2	8	2	4
Cap, veh/h	387	1434	574	310	1334	595	441	132	145	568	60	439
Arrive On Green	0.10	0.40	0.40	0.06	0.38	0.38	0.04	0.16	0.15	0.18	0.31	0.30
Sat Flow, veh/h	1781	3554	1422	1753	3469	1547	1781	814	895	1697	195	1420
Grp Volume(v), veh/h	193	879	17	38	681	246	27	0	42	306	0	141
Grp Sat Flow(s),veh/h/ln	1781	1777	1422	1753	1735	1547	1781	0	1709	1697	0	1615
Q Serve(g_s), s	6.2	19.3	0.7	1.2	14.8	11.4	1.0	0.0	2.1	14.2	0.0	6.6
Cycle Q Clear(g_c), s	6.2	19.3	0.7	1.2	14.8	11.4	1.0	0.0	2.1	14.2	0.0	6.6
Prop In Lane	1.00		1.00	1.00		1.00	1.00		0.52	1.00		0.88
Lane Grp Cap(c), veh/h	387	1434	574	310	1334	595	441	0	277	568	0	499
V/C Ratio(X)	0.50	0.61	0.03	0.12	0.51	0.41	0.06	0.00	0.15	0.54	0.00	0.28
Avail Cap(c_a), veh/h	586	2097	839	396	1765	787	493	0	313	852	0	756
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	16.5	23.2	17.7	16.3	23.2	22.1	21.6	0.0	35.6	25.7	0.0	26.1
Incr Delay (d2), s/veh	1.0	0.4	0.0	0.2	0.3	0.5	0.1	0.0	0.3	0.8	0.0	0.3
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	2.4	7.5	0.2	0.4	5.7	4.2	0.4	0.0	0.9	5.6	0.0	2.5
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	17.4	23.7	17.7	16.5	23.5	22.6	21.7	0.0	35.8	26.5	0.0	26.4
LnGrp LOS	B	C	B	B	C	C	C		D	C		C
Approach Vol, veh/h		1089			965			69				447
Approach Delay, s/veh		22.5			23.0			30.3				26.5
Approach LOS		C			C			C				C
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	11.2	44.7	7.1	35.4	13.0	42.8	21.5	21.0				
Change Period (Y+Rc), s	6.0	6.0	4.5	6.0	4.5	6.0	4.5	6.0				
Max Green Setting (Gmax), s	10.0	57.0	5.5	45.0	19.5	49.0	33.5	17.0				
Max Q Clear Time (g_c+I1), s	3.2	21.3	3.0	8.6	8.2	16.8	16.2	4.1				
Green Ext Time (p_c), s	0.0	17.4	0.0	0.5	0.4	13.1	0.9	0.1				
Intersection Summary												
HCM 7th Control Delay, s/veh				23.6								
HCM 7th LOS				C								

Intersection						
Int Delay, s/veh	2.5					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	W			W	W	
Traffic Vol, veh/h	9	109	58	293	307	52
Future Vol, veh/h	9	109	58	293	307	52
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	82	82	82	82	82	82
Heavy Vehicles, %	29	10	10	2	6	16
Mvmt Flow	11	133	71	357	374	63

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	905	406	438	0	-	0
Stage 1	406	-	-	-	-	-
Stage 2	499	-	-	-	-	-
Critical Hdwy	6.69	6.3	4.2	-	-	-
Critical Hdwy Stg 1	5.69	-	-	-	-	-
Critical Hdwy Stg 2	5.69	-	-	-	-	-
Follow-up Hdwy	3.761	3.39	2.29	-	-	-
Pot Cap-1 Maneuver	275	628	1081	-	-	-
Stage 1	618	-	-	-	-	-
Stage 2	558	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	253	628	1081	-	-	-
Mov Cap-2 Maneuver	253	-	-	-	-	-
Stage 1	568	-	-	-	-	-
Stage 2	558	-	-	-	-	-

Approach	EB	NB	SB
HCM Ctrl Dly, s/v	13.55	1.42	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	297	-	564	-	-
HCM Lane V/C Ratio	0.065	-	0.255	-	-
HCM Ctrl Dly (s/v)	8.6	0	13.6	-	-
HCM Lane LOS	A	A	B	-	-
HCM 95th %tile Q(veh)	0.2	-	1	-	-

Intersection						
Int Delay, s/veh	5.8					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	↘	↗	↘	↗	↘	↗
Traffic Vol, veh/h	117	33	85	217	326	21
Future Vol, veh/h	117	33	85	217	326	21
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	0	100	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	81	81	81	81	81	81
Heavy Vehicles, %	2	2	2	3	9	2
Mvmt Flow	144	41	105	268	402	26

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	893	415	428	0	-	0
Stage 1	415	-	-	-	-	-
Stage 2	478	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	312	637	1131	-	-	-
Stage 1	666	-	-	-	-	-
Stage 2	624	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	283	637	1131	-	-	-
Mov Cap-2 Maneuver	283	-	-	-	-	-
Stage 1	604	-	-	-	-	-
Stage 2	624	-	-	-	-	-

Approach	EB	NB	SB
HCM Ctrl Dly, s/v	26.04	2.39	0
HCM LOS	D		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	EBLn2	SBT	SBR
Capacity (veh/h)	1131	-	283	637	-	-
HCM Lane V/C Ratio	0.093	-	0.51	0.064	-	-
HCM Ctrl Dly (s/v)	8.5	-	30.3	11	-	-
HCM Lane LOS	A	-	D	B	-	-
HCM 95th %tile Q(veh)	0.3	-	2.7	0.2	-	-

Intersection												
Int Delay, s/veh	2.2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕	↕		↕	↕
Traffic Vol, veh/h	0	0	0	61	0	4	35	271	27	1	286	106
Future Vol, veh/h	0	0	0	61	0	4	35	271	27	1	286	106
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	100	-	-	100
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	78	78	78	78	78	78	78	78	78	78	78	78
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	8	2
Mvmt Flow	0	0	0	78	0	5	45	347	35	1	367	136

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	806	841	367	806	942	347	503	0	0	382	0	0
Stage 1	369	369	-	437	437	-	-	-	-	-	-	-
Stage 2	437	472	-	369	505	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	300	301	679	300	263	696	1062	-	-	1176	-	-
Stage 1	651	621	-	598	579	-	-	-	-	-	-	-
Stage 2	598	559	-	651	540	-	-	-	-	-	-	-
Platoon blocked, %												
Mov Cap-1 Maneuver	281	284	679	284	248	696	1062	-	-	1176	-	-
Mov Cap-2 Maneuver	281	284	-	284	248	-	-	-	-	-	-	-
Stage 1	650	620	-	566	548	-	-	-	-	-	-	-
Stage 2	562	529	-	650	539	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Ctrl Dly, s/v	0	22	0.9	0.02
HCM LOS	A	C		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	206	-	-	-	294	1176	-
HCM Lane V/C Ratio	0.042	-	-	-	0.283	0.001	-
HCM Ctrl Dly (s/v)	8.5	0	-	0	22	8.1	-
HCM Lane LOS	A	A	-	A	C	A	-
HCM 95th %tile Q(veh)	0.1	-	-	-	1.1	0	-

Intersection						
Int Delay, s/veh	8.7					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑	↑		↓	↓	↓
Traffic Vol, veh/h	264	303	90	151	199	77
Future Vol, veh/h	264	303	90	151	199	77
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	200	-	-	175	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	82	82	82	82	82	82
Heavy Vehicles, %	2	7	2	2	3	2
Mvmt Flow	322	370	110	184	243	94
Major/Minor	Major1	Major2	Minor1			
Conflicting Flow All	0	0	691	0	726	322
Stage 1	-	-	-	-	322	-
Stage 2	-	-	-	-	404	-
Critical Hdwy	-	-	4.12	-	6.43	6.22
Critical Hdwy Stg 1	-	-	-	-	5.43	-
Critical Hdwy Stg 2	-	-	-	-	5.43	-
Follow-up Hdwy	-	-	2.218	-	3.527	3.318
Pot Cap-1 Maneuver	-	-	903	-	390	719
Stage 1	-	-	-	-	732	-
Stage 2	-	-	-	-	672	-
Platoon blocked, %	-	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	903	-	337	719
Mov Cap-2 Maneuver	-	-	-	-	337	-
Stage 1	-	-	-	-	732	-
Stage 2	-	-	-	-	581	-
Approach	EB	WB	NB			
HCM Ctrl Dly, s/v	0	3.56	30.96			
HCM LOS			D			
Minor Lane/Major Mvmt	NBLn1	NBLn2	EBT	EBR	WBL	WBT
Capacity (veh/h)	337	719	-	-	672	-
HCM Lane V/C Ratio	0.719	0.131	-	-	0.121	-
HCM Ctrl Dly (s/v)	38.8	10.8	-	-	9.5	0
HCM Lane LOS	E	B	-	-	A	A
HCM 95th %tile Q(veh)	5.3	0.4	-	-	0.4	-

Intersection						
Int Delay, s/veh	1.3					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	W			W	W	
Traffic Vol, veh/h	5	26	89	347	398	18
Future Vol, veh/h	5	26	89	347	398	18
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	90	90	90	90	90	90
Heavy Vehicles, %	2	2	2	3	7	2
Mvmt Flow	6	29	99	386	442	20

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1036	452	462	0	-	0
Stage 1	452	-	-	-	-	-
Stage 2	583	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	257	607	1099	-	-	-
Stage 1	641	-	-	-	-	-
Stage 2	558	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	227	607	1099	-	-	-
Mov Cap-2 Maneuver	227	-	-	-	-	-
Stage 1	567	-	-	-	-	-
Stage 2	558	-	-	-	-	-

Approach	EB	NB	SB
HCM Ctrl Dly, s/v	13.11	1.76	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	367	-	478	-	-
HCM Lane V/C Ratio	0.09	-	0.072	-	-
HCM Ctrl Dly (s/v)	8.6	0	13.1	-	-
HCM Lane LOS	A	A	B	-	-
HCM 95th %tile Q(veh)	0.3	-	0.2	-	-

Intersection						
Int Delay, s/veh	0.1					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↑↑	↑↑			↑
Traffic Vol, veh/h	0	1033	755	36	0	10
Future Vol, veh/h	0	1033	755	36	0	10
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	90	90	90	90	90	90
Heavy Vehicles, %	2	2	5	2	2	2
Mvmt Flow	0	1148	839	40	0	11

Major/Minor	Major1	Major2	Minor2
Conflicting Flow All	-	0	0
Stage 1	-	-	-
Stage 2	-	-	-
Critical Hdwy	-	-	-
Critical Hdwy Stg 1	-	-	-
Critical Hdwy Stg 2	-	-	-
Follow-up Hdwy	-	-	-
Pot Cap-1 Maneuver	0	-	0
Stage 1	0	-	0
Stage 2	0	-	0
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	-	-	-
Mov Cap-2 Maneuver	-	-	-
Stage 1	-	-	-
Stage 2	-	-	-

Approach	EB	WB	SB
HCM Ctrl Dly, s/v	0	0	11.5
HCM LOS			B

Minor Lane/Major Mvmt	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	-	-	-	565
HCM Lane V/C Ratio	-	-	-	0.02
HCM Ctrl Dly (s/v)	-	-	-	11.5
HCM Lane LOS	-	-	-	B
HCM 95th %tile Q(veh)	-	-	-	0.1

Intersection						
Int Delay, s/veh	0.1					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↑↑	↑↑			↑
Traffic Vol, veh/h	0	1033	729	36	0	10
Future Vol, veh/h	0	1033	729	36	0	10
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	90	90	90	90	90	90
Heavy Vehicles, %	2	2	5	2	2	2
Mvmt Flow	0	1148	810	40	0	11

Major/Minor	Major1	Major2	Minor2
Conflicting Flow All	-	0	0
Stage 1	-	-	-
Stage 2	-	-	-
Critical Hdwy	-	-	-
Critical Hdwy Stg 1	-	-	-
Critical Hdwy Stg 2	-	-	-
Follow-up Hdwy	-	-	-
Pot Cap-1 Maneuver	0	-	0
Stage 1	0	-	0
Stage 2	0	-	0
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	-	-	-
Mov Cap-2 Maneuver	-	-	-
Stage 1	-	-	-
Stage 2	-	-	-

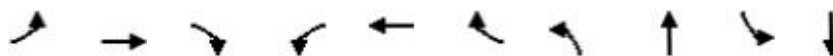
Approach	EB	WB	SB
HCM Ctrl Dly, s/v	0	0	11.35
HCM LOS			B

Minor Lane/Major Mvmt	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	-	-	-	578
HCM Lane V/C Ratio	-	-	-	0.019
HCM Ctrl Dly (s/v)	-	-	-	11.4
HCM Lane LOS	-	-	-	B
HCM 95th %tile Q(veh)	-	-	-	0.1

Intersection													
Int Delay, s/veh	4.8												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	
Lane Configurations		+			+			+			+		
Traffic Vol, veh/h	6	0	7	0	0	0	5	4	0	0	5	4	
Future Vol, veh/h	6	0	7	0	0	0	5	4	0	0	5	4	
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0	
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free	
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None	
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-	
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-	
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-	
Peak Hour Factor	78	78	78	78	78	78	78	78	78	78	78	78	
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2	
Mvmt Flow	8	0	9	0	0	0	6	5	0	0	6	5	
Major/Minor	Minor2		Minor1			Major1		Major2					
Conflicting Flow All	27	27	9	24	29	5	12	0	0	5	0	0	
Stage 1	9	9	-	18	18	-	-	-	-	-	-	-	
Stage 2	18	18	-	6	12	-	-	-	-	-	-	-	
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-	
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-	
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-	
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-	
Pot Cap-1 Maneuver	983	866	1073	987	863	1078	1607	-	-	1616	-	-	
Stage 1	1012	888	-	1001	880	-	-	-	-	-	-	-	
Stage 2	1001	880	-	1015	886	-	-	-	-	-	-	-	
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-	
Mov Cap-1 Maneuver	979	863	1073	975	860	1078	1607	-	-	1616	-	-	
Mov Cap-2 Maneuver	979	863	-	975	860	-	-	-	-	-	-	-	
Stage 1	1012	888	-	997	877	-	-	-	-	-	-	-	
Stage 2	997	877	-	1007	886	-	-	-	-	-	-	-	
Approach	EB		WB			NB		SB					
HCM Ctrl Dly, s/v	8.56		0			4.03		0					
HCM LOS	A		A										
Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR					
Capacity (veh/h)	1000	-	-	1027	-	1616	-	-					
HCM Lane V/C Ratio	0.004	-	-	0.016	-	-	-	-					
HCM Ctrl Dly (s/v)	7.2	0	-	8.6	0	0	-	-					
HCM Lane LOS	A	A	-	A	A	A	-	-					
HCM 95th %tile Q(veh)	0	-	-	0	-	0	-	-					

Queues
2: WK Alston Drive & SC 170 (Robert Smalls Parkway)

Beaufort FSED
2030 Build PM



























Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	SBL	SBT
Lane Group Flow (vph)	142	958	54	142	1083	358	94	134	375	161
v/c Ratio	0.56	0.77	0.08	0.55	0.85	0.49	0.20	0.46	0.67	0.28
Control Delay (s/veh)	26.7	36.4	0.3	24.6	39.7	11.6	23.5	42.0	32.3	19.7
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay (s/veh)	26.7	36.4	0.3	24.6	39.7	11.6	23.5	42.0	32.3	19.7
Queue Length 50th (ft)	50	310	0	51	358	55	39	64	193	49
Queue Length 95th (ft)	122	454	0	106	543	160	93	161	366	123
Internal Link Dist (ft)		514			1442			443		414
Turn Bay Length (ft)	225		100	200		200	225		100	
Base Capacity (vph)	313	1903	916	282	1866	948	478	319	662	778
Starvation Cap Reductn	0	0	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.45	0.50	0.06	0.50	0.58	0.38	0.20	0.42	0.57	0.21
Intersection Summary										

DRAFT

HCM 7th Signalized Intersection Summary
 2: WK Alston Drive & SC 170 (Robert Smalls Parkway)

Beaufort FSED
 2030 Build PM

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	135	910	51	135	1029	340	89	60	67	356	57	96
Future Volume (veh/h)	135	910	51	135	1029	340	89	60	67	356	57	96
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1841	1870	1870
Adj Flow Rate, veh/h	142	958	54	142	1083	358	94	63	71	375	60	101
Peak Hour Factor	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	4	2	2
Cap, veh/h	255	1453	648	308	1527	681	432	112	127	515	185	312
Arrive On Green	0.07	0.41	0.41	0.08	0.43	0.43	0.06	0.14	0.13	0.21	0.30	0.29
Sat Flow, veh/h	1781	3554	1585	1781	3554	1585	1781	803	905	1753	626	1054
Grp Volume(v), veh/h	142	958	54	142	1083	358	94	0	134	375	0	161
Grp Sat Flow(s),veh/h/ln	1781	1777	1585	1781	1777	1585	1781	0	1708	1753	0	1681
Q Serve(g_s), s	4.9	24.9	2.4	5.1	28.6	19.0	4.1	0.0	8.4	20.2	0.0	8.6
Cycle Q Clear(g_c), s	4.9	24.9	2.4	5.1	28.6	19.0	4.1	0.0	8.4	20.2	0.0	8.6
Prop In Lane	1.00		1.00	1.00		1.00	1.00		0.53	1.00		0.63
Lane Grp Cap(c), veh/h	255	1453	648	308	1527	681	432	0	239	515	0	498
V/C Ratio(X)	0.56	0.66	0.08	0.46	0.71	0.53	0.22	0.00	0.56	0.73	0.00	0.32
Avail Cap(c_a), veh/h	357	1767	788	356	1733	773	432	0	269	678	0	683
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	21.3	27.3	20.7	20.0	26.7	24.0	25.7	0.0	46.1	31.0	0.0	31.6
Incr Delay (d2), s/veh	1.9	0.7	0.1	1.1	1.2	0.6	0.3	0.0	2.1	2.7	0.0	0.4
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	2.0	10.1	0.9	2.0	11.6	7.3	1.8	0.0	3.7	8.7	0.0	3.5
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	23.2	28.0	20.7	21.1	27.9	24.6	26.0	0.0	48.1	33.8	0.0	31.9
LnGrp LOS	C	C	C	C	C	C	C		D	C		C
Approach Vol, veh/h		1154			1583			228				536
Approach Delay, s/veh		27.1			26.6			39.0				33.2
Approach LOS		C			C			D				C
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	13.9	51.7	9.8	38.8	11.5	54.1	27.6	21.0				
Change Period (Y+Rc), s	6.0	6.0	4.5	6.0	4.5	6.0	4.5	6.0				
Max Green Setting (Gmax), s	11.0	55.8	5.3	45.4	13.6	54.7	33.7	17.0				
Max Q Clear Time (g_c+I1), s	7.1	26.9	6.1	10.6	6.9	30.6	22.2	10.4				
Green Ext Time (p_c), s	0.1	16.9	0.0	0.5	0.2	17.5	0.9	0.2				
Intersection Summary												
HCM 7th Control Delay, s/veh			28.6									
HCM 7th LOS			C									

Intersection						
Int Delay, s/veh	2.2					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	W			T	T	
Traffic Vol, veh/h	19	106	19	486	291	19
Future Vol, veh/h	19	106	19	486	291	19
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	83	83	83	83	83	83
Heavy Vehicles, %	71	5	25	2	2	57
Mvmt Flow	23	128	23	586	351	23

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	993	362	373	0	-	0
Stage 1	362	-	-	-	-	-
Stage 2	631	-	-	-	-	-
Critical Hdwy	7.11	6.25	4.35	-	-	-
Critical Hdwy Stg 1	6.11	-	-	-	-	-
Critical Hdwy Stg 2	6.11	-	-	-	-	-
Follow-up Hdwy	4.139	3.345	2.425	-	-	-
Pot Cap-1 Maneuver	205	676	1069	-	-	-
Stage 1	575	-	-	-	-	-
Stage 2	419	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	199	676	1069	-	-	-
Mov Cap-2 Maneuver	199	-	-	-	-	-
Stage 1	557	-	-	-	-	-
Stage 2	419	-	-	-	-	-

Approach	EB	NB	SB
HCM Ctrl Dly, s/v	15.42	0.32	0
HCM LOS	C		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	68	-	495	-	-
HCM Lane V/C Ratio	0.021	-	0.304	-	-
HCM Ctrl Dly (s/v)	8.4	0	15.4	-	-
HCM Lane LOS	A	A	C	-	-
HCM 95th %tile Q(veh)	0.1	-	1.3	-	-

Intersection						
Int Delay, s/veh	5.6					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	↖	↗	↖	↖	↖	↖
Traffic Vol, veh/h	101	33	34	472	278	46
Future Vol, veh/h	101	33	34	472	278	46
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	0	100	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	74	74	74	74	74	74
Heavy Vehicles, %	2	2	4	6	6	2
Mvmt Flow	136	45	46	638	376	62
Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1136	407	438	0	-	0
Stage 1	407	-	-	-	-	-
Stage 2	730	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.14	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.236	-	-	-
Pot Cap-1 Maneuver	223	644	1111	-	-	-
Stage 1	672	-	-	-	-	-
Stage 2	477	-	-	-	-	-
Platoon blocked, %	-	-	-	-	-	-
Mov Cap-1 Maneuver	214	644	1111	-	-	-
Mov Cap-2 Maneuver	214	-	-	-	-	-
Stage 1	644	-	-	-	-	-
Stage 2	477	-	-	-	-	-
Approach	EB	NB	SB			
HCM Ctrl Dly, s/v	38.43	0.56	0			
HCM LOS	E					
Minor Lane/Major Mvmt	NBL	NBT	EBLn1	EBLn2	SBT	SBR
Capacity (veh/h)	1111	-	214	644	-	-
HCM Lane V/C Ratio	0.041	-	0.638	0.069	-	-
HCM Ctrl Dly (s/v)	8.4	-	47.4	11	-	-
HCM Lane LOS	A	-	E	B	-	-
HCM 95th %tile Q(veh)	0.1	-	3.8	0.2	-	-

Intersection												
Int Delay, s/veh	1.3											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		+			+			+	+		+	+
Traffic Vol, veh/h	1	0	4	31	0	3	28	461	84	4	289	26
Future Vol, veh/h	1	0	4	31	0	3	28	461	84	4	289	26
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	100	-	-	100
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	79	79	79	79	79	79	79	79	79	79	79	79
Heavy Vehicles, %	2	2	2	2	2	2	2	5	2	2	5	2
Mvmt Flow	1	0	5	39	0	4	35	584	106	5	366	33

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	1030	1137	366	1030	1063	584	399	0	0	690	0	0
Stage 1	376	376	-	654	654	-	-	-	-	-	-	-
Stage 2	654	761	-	376	409	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	212	202	679	212	223	512	1160	-	-	905	-	-
Stage 1	645	616	-	455	463	-	-	-	-	-	-	-
Stage 2	455	414	-	645	596	-	-	-	-	-	-	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	198	190	679	198	210	512	1160	-	-	905	-	-
Mov Cap-2 Maneuver	198	190	-	198	210	-	-	-	-	-	-	-
Stage 1	641	612	-	432	440	-	-	-	-	-	-	-
Stage 2	429	393	-	636	592	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB		
HCM Ctrl Dly, s/v	12.99		26.6		0.4		0.11		
HCM LOS	B		D						

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	103	-	-	457	209	905	-	-
HCM Lane V/C Ratio	0.031	-	-	0.014	0.206	0.006	-	-
HCM Ctrl Dly (s/v)	8.2	0	-	13	26.6	9	-	-
HCM Lane LOS	A	A	-	B	D	A	-	-
HCM 95th %tile Q(veh)	0.1	-	-	0	0.7	0	-	-

Intersection						
Int Delay, s/veh	24.5					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑	↑		↓	↓	↑
Traffic Vol, veh/h	255	269	50	264	340	125
Future Vol, veh/h	255	269	50	264	340	125
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	200	-	-	175	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	88	88	88	88	88	88
Heavy Vehicles, %	2	2	6	4	2	7
Mvmt Flow	290	306	57	300	386	142

Major/Minor	Major1	Major2	Minor1	Minor2	Minor3
Conflicting Flow All	0	0	595	0	703
Stage 1	-	-	-	-	290
Stage 2	-	-	-	-	414
Critical Hdwy	-	-	4.16	-	6.42
Critical Hdwy Stg 1	-	-	-	-	5.42
Critical Hdwy Stg 2	-	-	-	-	5.42
Follow-up Hdwy	-	-	2.254	-	3.518
Pot Cap-1 Maneuver	-	-	962	-	404
Stage 1	-	-	-	-	760
Stage 2	-	-	-	-	667
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	962	-	375
Mov Cap-2 Maneuver	-	-	-	-	375
Stage 1	-	-	-	-	760
Stage 2	-	-	-	-	620

Approach	EB	WB	NB
HCM Ctrl Dly, s/v	0	1.43	67.67
HCM LOS			F

Minor Lane/Major Mvmt	NBLn1	NBLn2	EBT	EBR	WBL	WBT
Capacity (veh/h)	375	738	-	-	287	-
HCM Lane V/C Ratio	1.03	0.193	-	-	0.059	-
HCM Ctrl Dly (s/v)	88.5	11	-	-	9	0
HCM Lane LOS	F	B	-	-	A	A
HCM 95th %tile Q(veh)	12.8	0.7	-	-	0.2	-

Notes	
-: Volume exceeds capacity	\$: Delay exceeds 300s
+: Computation Not Defined	*: All major volume in platoon

Intersection						
Int Delay, s/veh	2.6					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	W			W	W	
Traffic Vol, veh/h	24	123	53	482	387	10
Future Vol, veh/h	24	123	53	482	387	10
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	90	90	90	90	90	90
Heavy Vehicles, %	2	2	2	2	3	2
Mvmt Flow	27	137	59	536	430	11

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1089	436	441	0	-	0
Stage 1	436	-	-	-	-	-
Stage 2	653	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	238	621	1119	-	-	-
Stage 1	652	-	-	-	-	-
Stage 2	518	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	221	621	1119	-	-	-
Mov Cap-2 Maneuver	221	-	-	-	-	-
Stage 1	603	-	-	-	-	-
Stage 2	518	-	-	-	-	-

Approach	EB	NB	SB
HCM Ctrl Dly, s/v	16.36	0.83	0
HCM LOS	C		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	178	-	479	-	-
HCM Lane V/C Ratio	0.053	-	0.341	-	-
HCM Ctrl Dly (s/v)	8.4	0	16.4	-	-
HCM Lane LOS	A	A	C	-	-
HCM 95th %tile Q(veh)	0.2	-	1.5	-	-

Intersection						
Int Delay, s/veh	0.3					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↑↑	↑↑			↑
Traffic Vol, veh/h	0	1096	1193	21	0	49
Future Vol, veh/h	0	1096	1193	21	0	49
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	90	90	90	90	90	90
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	1218	1326	23	0	54

Major/Minor	Major1	Major2	Minor2
Conflicting Flow All	-	0	0
Stage 1	-	-	-
Stage 2	-	-	-
Critical Hdwy	-	-	-
Critical Hdwy Stg 1	-	-	-
Critical Hdwy Stg 2	-	-	-
Follow-up Hdwy	-	-	-
Pot Cap-1 Maneuver	0	-	0
Stage 1	0	-	0
Stage 2	0	-	0
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	-	-	-
Mov Cap-2 Maneuver	-	-	-
Stage 1	-	-	-
Stage 2	-	-	-

Approach	EB	WB	SB
HCM Ctrl Dly, s/v	0	0	15.51
HCM LOS			C

Minor Lane/Major Mvmt	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	-	-	-	397
HCM Lane V/C Ratio	-	-	-	0.137
HCM Ctrl Dly (s/v)	-	-	-	15.5
HCM Lane LOS	-	-	-	C
HCM 95th %tile Q(veh)	-	-	-	0.5

Intersection						
Int Delay, s/veh	0.3					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↑↑	↑↑			↑
Traffic Vol, veh/h	0	1096	1221	21	0	49
Future Vol, veh/h	0	1096	1221	21	0	49
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	90	90	90	90	90	90
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	1218	1357	23	0	54

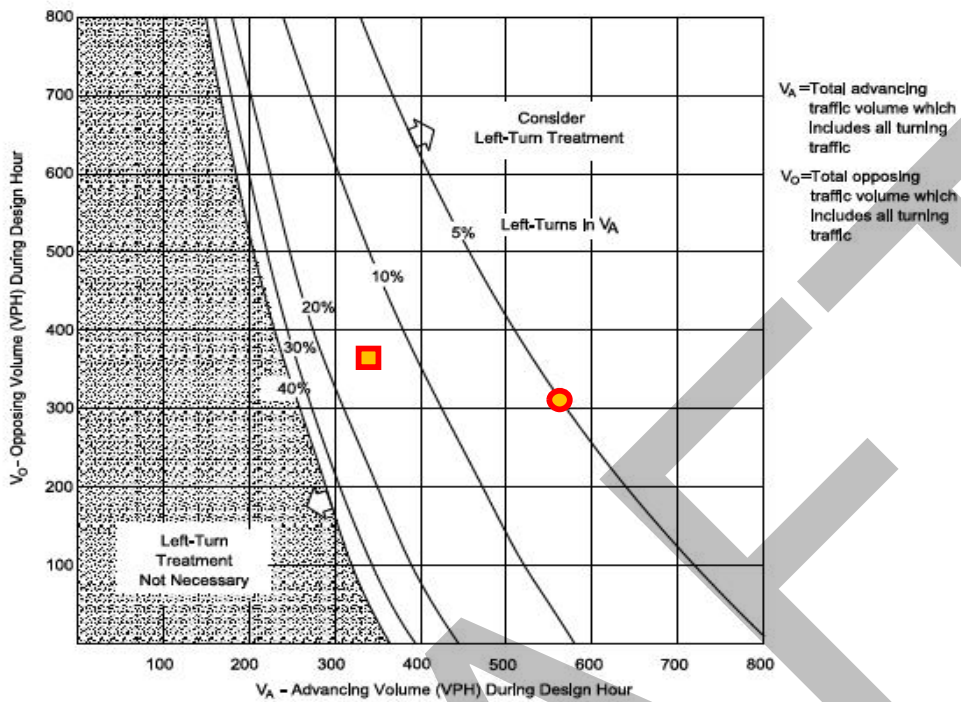
Major/Minor	Major1	Major2	Minor2
Conflicting Flow All	-	0	0
Stage 1	-	-	-
Stage 2	-	-	-
Critical Hdwy	-	-	-
Critical Hdwy Stg 1	-	-	-
Critical Hdwy Stg 2	-	-	-
Follow-up Hdwy	-	-	-
Pot Cap-1 Maneuver	0	-	0
Stage 1	0	-	0
Stage 2	0	-	0
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	-	-	-
Mov Cap-2 Maneuver	-	-	-
Stage 1	-	-	-
Stage 2	-	-	-

Approach	EB	WB	SB
HCM Ctrl Dly, s/v	0	0	15.8
HCM LOS			C

Minor Lane/Major Mvmt	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	-	-	-	388
HCM Lane V/C Ratio	-	-	-	0.14
HCM Ctrl Dly (s/v)	-	-	-	15.8
HCM Lane LOS	-	-	-	C
HCM 95th %tile Q(veh)	-	-	-	0.5

Appendix D – Turn Lane Warrant Analysis Worksheets

DRAFT



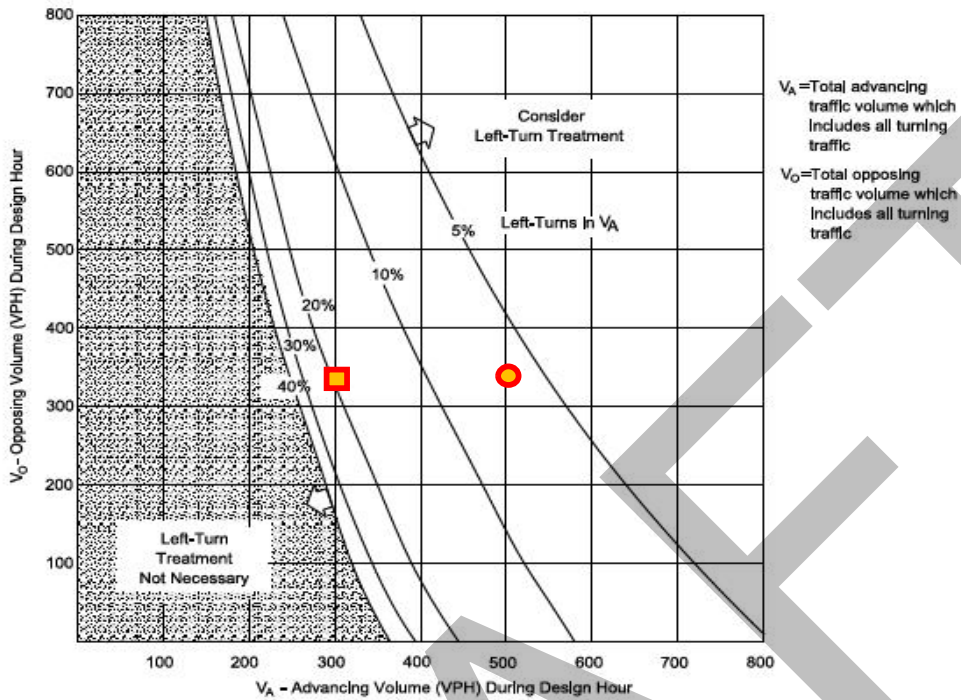
Instructions:

1. The family of curves represents the percent of left turns in the advancing volume (V_A). The designer should locate the curve for the actual percentage of left turns. When this is not an even increment of 5, the designer should estimate where the curve lies.
2. Read V_A and V_O into the chart and locate the intersection of the two volumes.
3. Note the location of the point in #2 relative to the line in #1. If the point is to the right of the line, then a left-turn lane is warranted. If the point is to the left of the line, then a left-turn lane is not warranted based on traffic volumes.

VOLUME GUIDELINES FOR LEFT-TURN LANES AT UNSIGNALIZED INTERSECTIONS ON TWO-LANE HIGHWAYS (40 mph)
Figure 9.5-G

WK Alston Drive at RSLA Northern Access

Northbound	Condition	Va	Vo	LTs	LT %
■	2030 AM Build	333	393	35	10.5%
●	2030 PM Build	573	319	28	4.9%



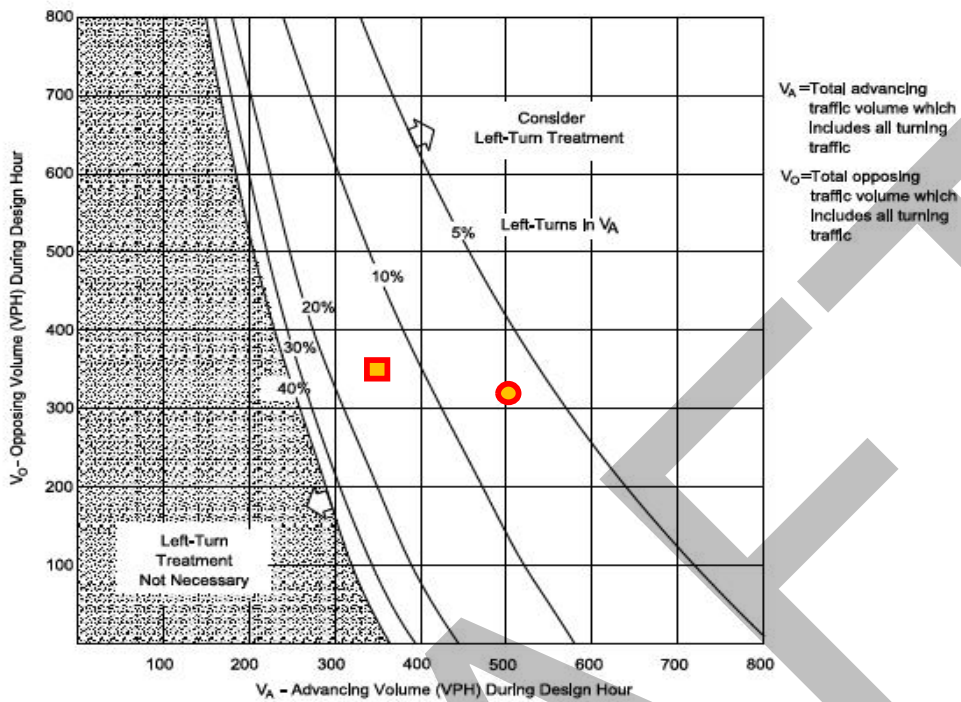
Instructions:

1. The family of curves represents the percent of left turns in the advancing volume (V_A). The designer should locate the curve for the actual percentage of left turns. When this is not an even increment of 5, the designer should estimate where the curve lies.
2. Read V_A and V_O into the chart and locate the intersection of the two volumes.
3. Note the location of the point in #2 relative to the line in #1. If the point is to the right of the line, then a left-turn lane is warranted. If the point is to the left of the line, then a left-turn lane is not warranted based on traffic volumes.

VOLUME GUIDELINES FOR LEFT-TURN LANES AT UNSIGNALIZED INTERSECTIONS ON TWO-LANE HIGHWAYS (40 mph)
 Figure 9.5-G

WK Alston Drive at RSLA Full-Movement Access

Northbound	Condition	Va	Vo	LTs	LT %
■	2030 AM Build	302	347	85	28.1%
●	2030 PM Build	506	324	34	6.7%



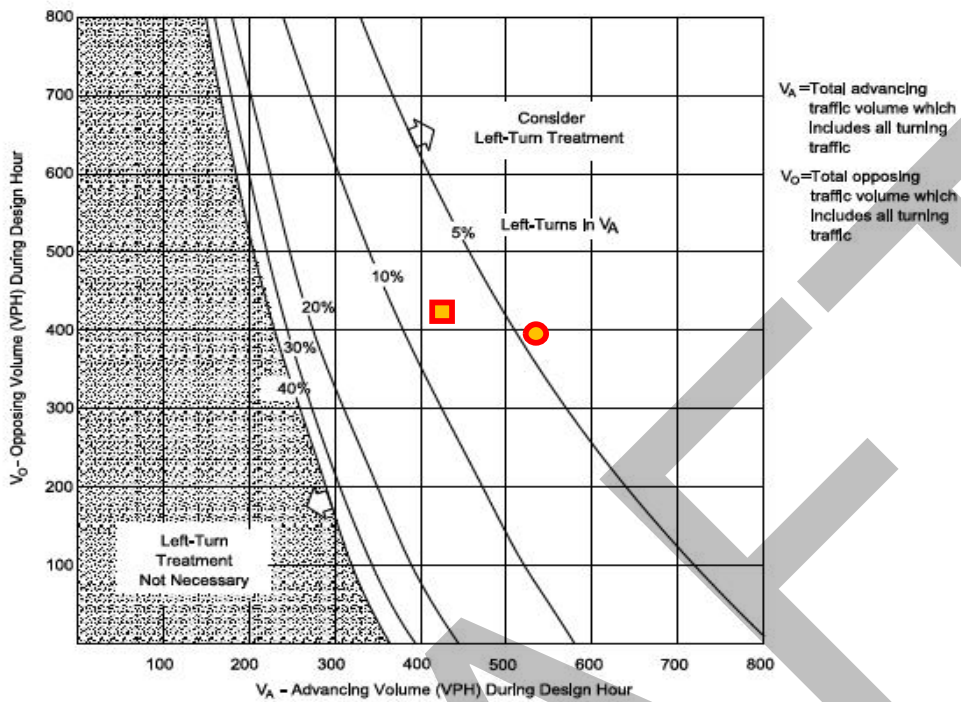
Instructions:

1. The family of curves represents the percent of left turns in the advancing volume (V_A). The designer should locate the curve for the actual percentage of left turns. When this is not an even increment of 5, the designer should estimate where the curve lies.
2. Read V_A and V_O into the chart and locate the intersection of the two volumes.
3. Note the location of the point in #2 relative to the line in #1. If the point is to the right of the line, then a left-turn lane is warranted. If the point is to the left of the line, then a left-turn lane is not warranted based on traffic volumes.

VOLUME GUIDELINES FOR LEFT-TURN LANES AT UNSIGNALIZED INTERSECTIONS ON TWO-LANE HIGHWAYS (40 mph)
Figure 9.5-G

WK Alston Drive at RSLA Bus Access

Northbound	Condition	Va	Vo	LTs	LT %
■	2030 AM Build	351	359	58	16.5%
●	2030 PM Build	505	310	19	3.8%



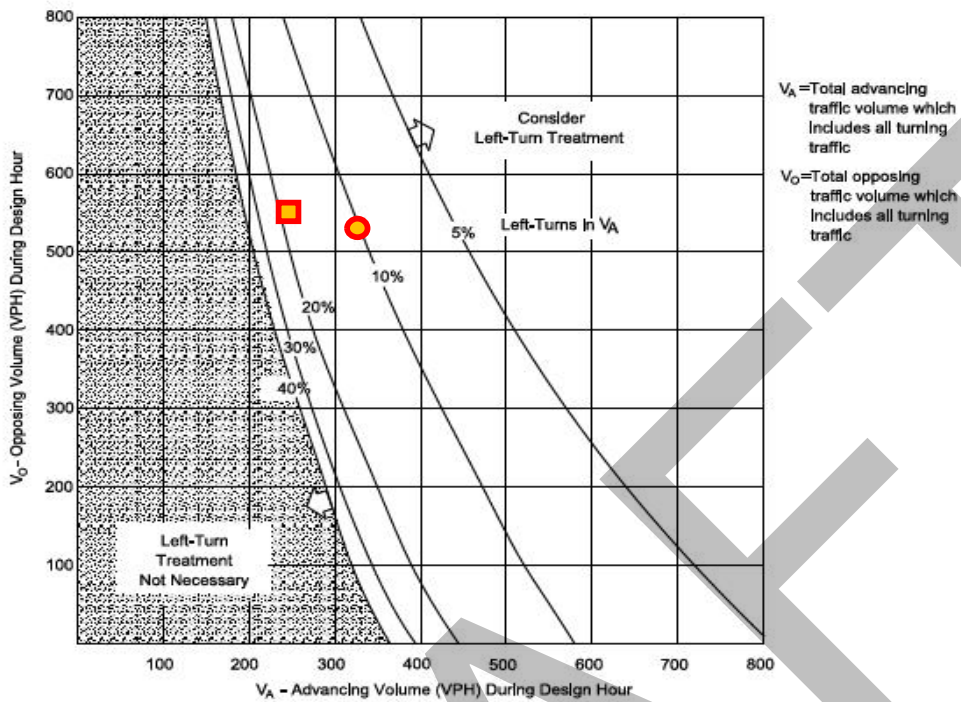
Instructions:

1. The family of curves represents the percent of left turns in the advancing volume (V_A). The designer should locate the curve for the actual percentage of left turns. When this is not an even increment of 5, the designer should estimate where the curve lies.
2. Read V_A and V_O into the chart and locate the intersection of the two volumes.
3. Note the location of the point in #2 relative to the line in #1. If the point is to the right of the line, then a left-turn lane is warranted. If the point is to the left of the line, then a left-turn lane is not warranted based on traffic volumes.

VOLUME GUIDELINES FOR LEFT-TURN LANES AT UNSIGNALIZED INTERSECTIONS ON TWO-LANE HIGHWAYS (40 mph)
 Figure 9.5-G

WK Alston Drive at FSED Site Access #1

Northbound	Condition	Va	Vo	LTs	LT %
■	2030 AM Build	436	416	89	20.4%
●	2030 PM Build	535	397	53	9.9%



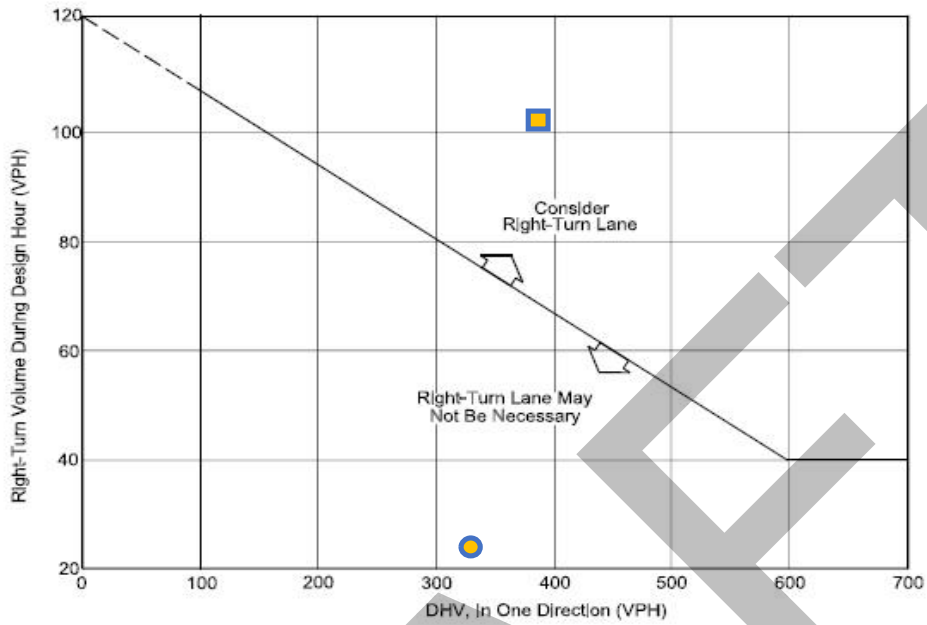
Instructions:

1. The family of curves represents the percent of left turns in the advancing volume (V_A). The designer should locate the curve for the actual percentage of left turns. When this is not an even increment of 5, the designer should estimate where the curve lies.
2. Read V_A and V_O into the chart and locate the intersection of the two volumes.
3. Note the location of the point in #2 relative to the line in #1. If the point is to the right of the line, then a left-turn lane is warranted. If the point is to the left of the line, then a left-turn lane is not warranted based on traffic volumes.

VOLUME GUIDELINES FOR LEFT-TURN LANES AT UNSIGNALIZED INTERSECTIONS ON TWO-LANE HIGHWAYS (40 mph)
 Figure 9.5-G

WK Alston Drive at Broad River Boulevard

Northbound	Condition	Va	Vo	LTs	LT %
■	2030 AM Build	241	567	90	37.3%
●	2030 PM Build	311	524	50	16.1%



Note: For highways with a design speed below 50 miles per hour with a DHV < 300 and where right turns > 40, an adjustment should be used. To read the vertical axis of the chart, subtract 20 from the actual number of right turns.

Example



Given: Design Speed = 35 miles per hour
 DHV = 250 vehicles per hour
 Right Turns = 100 vehicles per hour

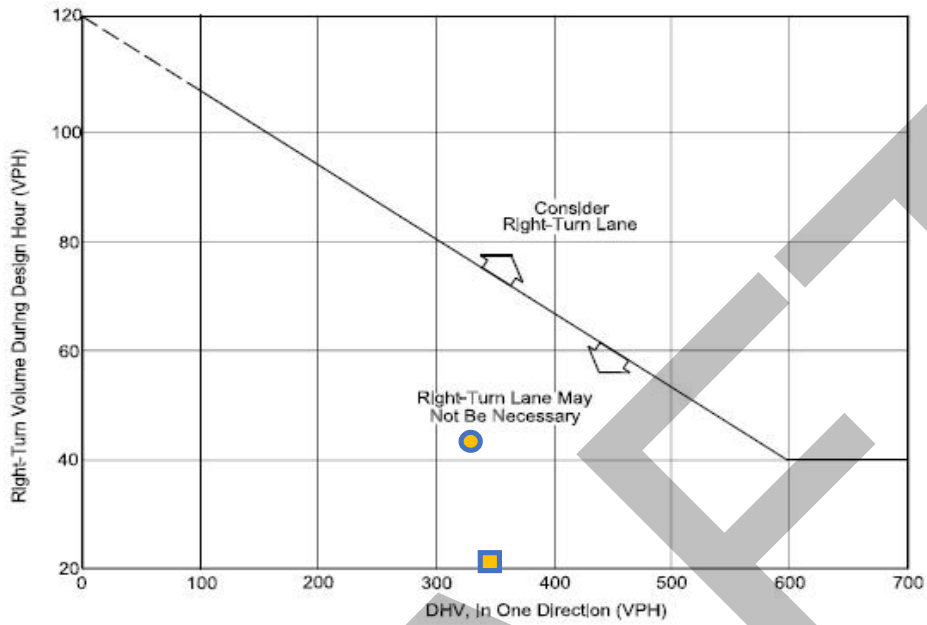
Problem: Determine if a right-turn lane is necessary.

Solution: To read the vertical axis, use $100 - 20 = 80$ vehicles per hour. The figure indicates that a right-turn lane is not necessary, unless other factors (e.g., high crash rate) indicate a lane is needed.

GUIDELINES FOR RIGHT-TURN LANES AT UNSIGNALIZED INTERSECTIONS ON TWO-LANE HIGHWAYS
 Figure 9.5-A

WK Alston Drive at RLSA Northern Access

Southbound	Condition	DHV	RTs
	2030 AM Build	393	106
	2030 PM Build	319	26



Note: For highways with a design speed below 50 miles per hour with a DHV < 300 and where right turns > 40, an adjustment should be used. To read the vertical axis of the chart, subtract 20 from the actual number of right turns.

Example



Given: Design Speed = 35 miles per hour
 DHV = 250 vehicles per hour
 Right Turns = 100 vehicles per hour

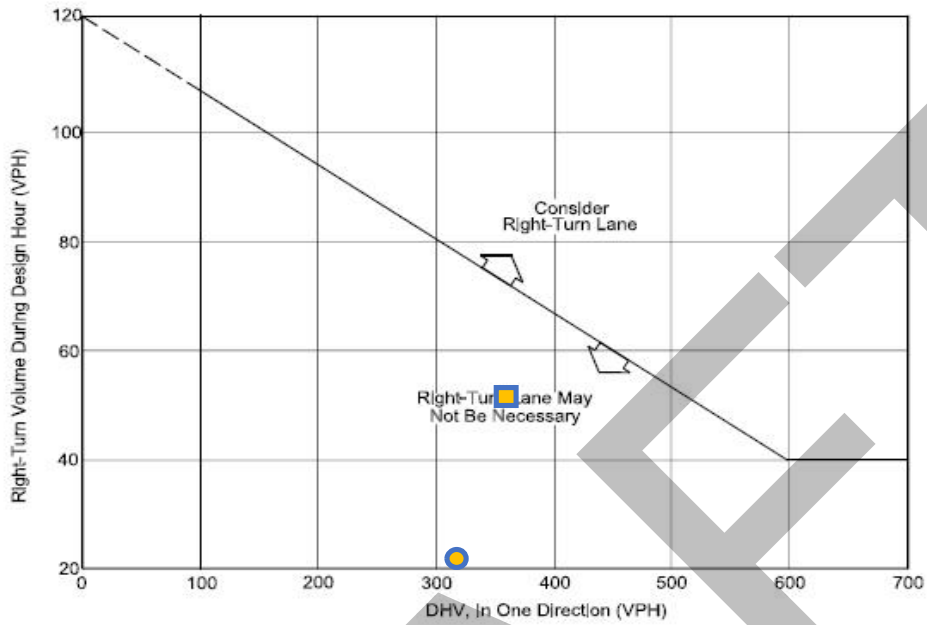
Problem: Determine if a right-turn lane is necessary.

Solution: To read the vertical axis, use $100 - 20 = 80$ vehicles per hour. The figure indicates that a right-turn lane is not necessary, unless other factors (e.g., high crash rate) indicate a lane is needed.

GUIDELINES FOR RIGHT-TURN LANES AT UNSIGNALIZED INTERSECTIONS ON TWO-LANE HIGHWAYS
 Figure 9.5-A

WK Alston Drive at RLSA Full-Movement Access

Southbound	Condition	DHV	RTs
	2030 AM Build	347	21
	2030 PM Build	324	46



Note: For highways with a design speed below 50 miles per hour with a DHV < 300 and where right turns > 40, an adjustment should be used. To read the vertical axis of the chart, subtract 20 from the actual number of right turns.

Example

Given: Design Speed = 35 miles per hour
 DHV = 250 vehicles per hour
 Right Turns = 100 vehicles per hour

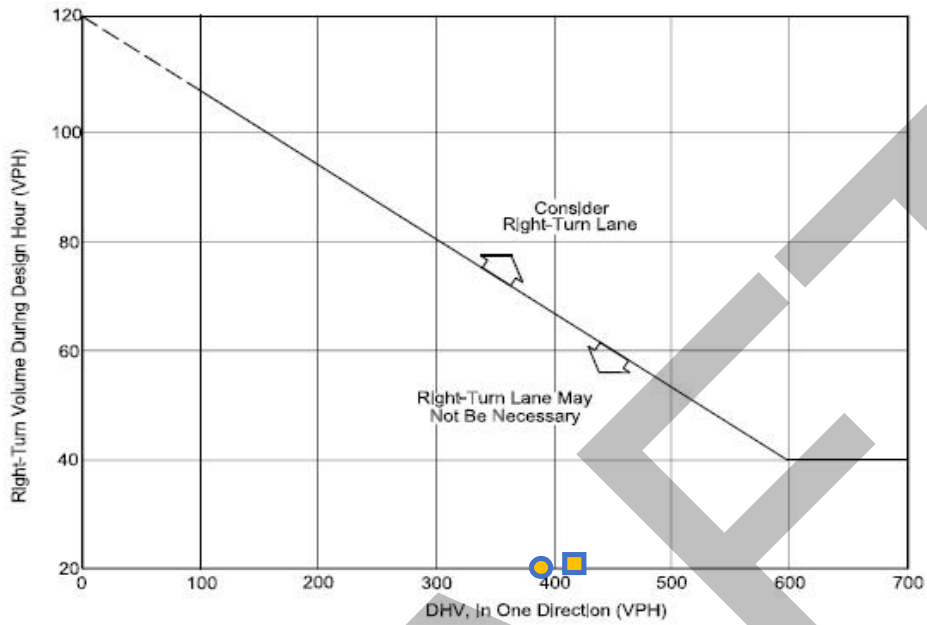
Problem: Determine if a right-turn lane is necessary.

Solution: To read the vertical axis, use $100 - 20 = 80$ vehicles per hour. The figure indicates that a right-turn lane is not necessary, unless other factors (e.g., high crash rate) indicate a lane is needed.

GUIDELINES FOR RIGHT-TURN LANES AT UNSIGNALIZED INTERSECTIONS ON TWO-LANE HIGHWAYS
 Figure 9.5-A

WK Alston Drive at RLSA Bus Access

Southbound	Condition	DHV	RTs
■	2030 AM Build	359	52
●	2030 PM Build	310	19



Note: For highways with a design speed below 50 miles per hour with a DHV < 300 and where right turns > 40, an adjustment should be used. To read the vertical axis of the chart, subtract 20 from the actual number of right turns.

Example

Given: Design Speed = 35 miles per hour
 DHV = 250 vehicles per hour
 Right Turns = 100 vehicles per hour

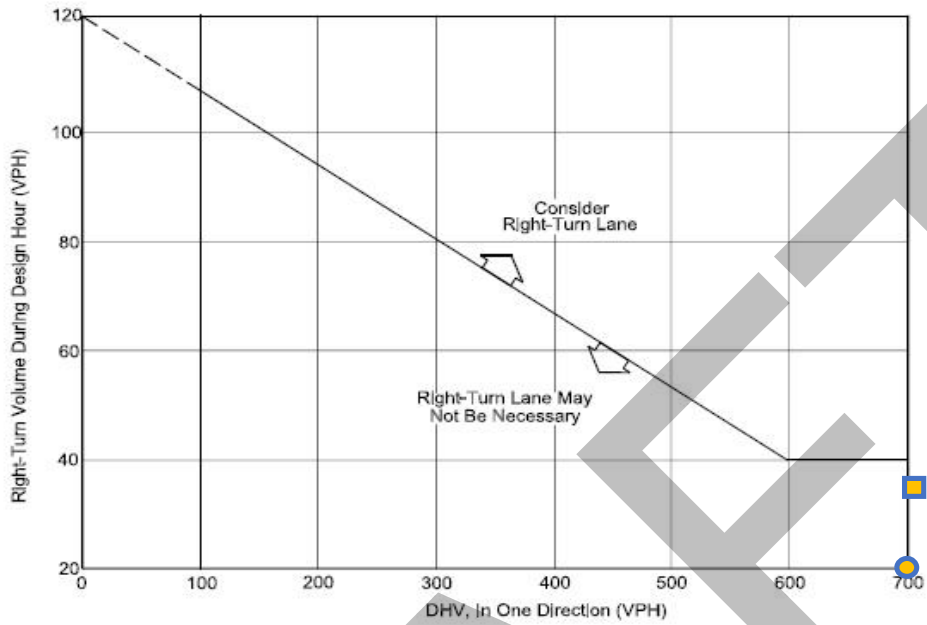
Problem: Determine if a right-turn lane is necessary.

Solution: To read the vertical axis, use $100 - 20 = 80$ vehicles per hour. The figure indicates that a right-turn lane is not necessary, unless other factors (e.g., high crash rate) indicate a lane is needed.

GUIDELINES FOR RIGHT-TURN LANES AT UNSIGNALIZED INTERSECTIONS ON TWO-LANE HIGHWAYS
 Figure 9.5-A

WK Alston Drive at FSED Site Access #1

Southbound	Condition	DHV	RTs
■	2030 AM Build	416	18
●	2030 PM Build	397	10



Note: For highways with a design speed below 50 miles per hour with a DHV < 300 and where right turns > 40, an adjustment should be used. To read the vertical axis of the chart, subtract 20 from the actual number of right turns.

Example



Given: Design Speed = 35 miles per hour
 DHV = 250 vehicles per hour
 Right Turns = 100 vehicles per hour

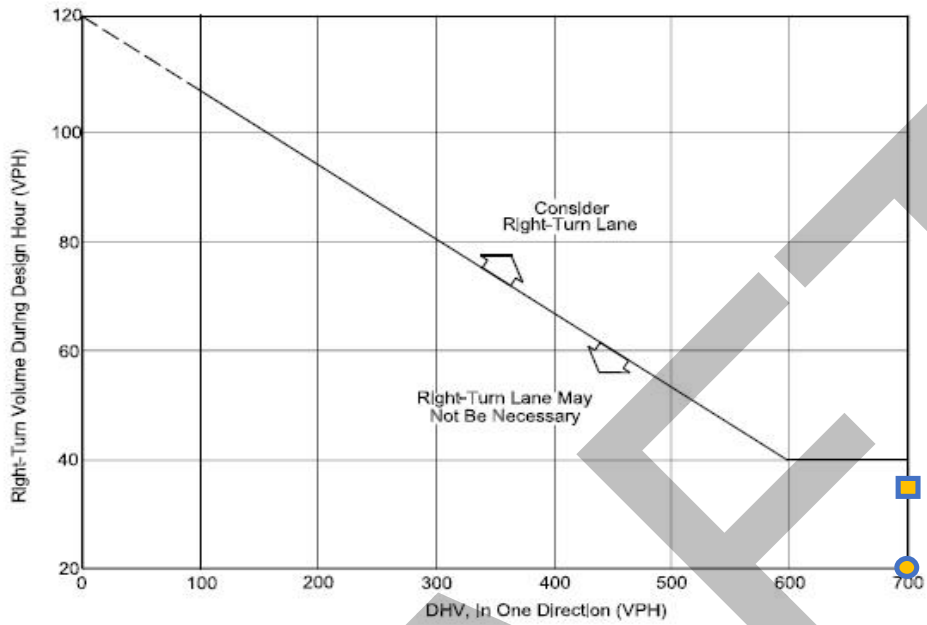
Problem: Determine if a right-turn lane is necessary.

Solution: To read the vertical axis, use $100 - 20 = 80$ vehicles per hour. The figure indicates that a right-turn lane is not necessary, unless other factors (e.g., high crash rate) indicate a lane is needed.

GUIDELINES FOR RIGHT-TURN LANES AT UNSIGNALIZED INTERSECTIONS ON TWO-LANE HIGHWAYS
 Figure 9.5-A

SC 170 at FSED Site Access #2

Westbound	Condition	DHV	RTs
	2030 AM Build	791	36
	2030 PM Build	1214	21



Note: For highways with a design speed below 50 miles per hour with a DHV < 300 and where right turns > 40, an adjustment should be used. To read the vertical axis of the chart, subtract 20 from the actual number of right turns.

Example

Given: Design Speed = 35 miles per hour
 DHV = 250 vehicles per hour
 Right Turns = 100 vehicles per hour

Problem: Determine if a right-turn lane is necessary.

Solution: To read the vertical axis, use $100 - 20 = 80$ vehicles per hour. The figure indicates that a right-turn lane is not necessary, unless other factors (e.g., high crash rate) indicate a lane is needed.

GUIDELINES FOR RIGHT-TURN LANES AT UNSIGNALIZED INTERSECTIONS ON TWO-LANE HIGHWAYS
 Figure 9.5-A

SC 170 at FSED Site Access #3

Westbound	Condition	DHV	RTs
■	2030 AM Build	765	36
●	2030 PM Build	1242	21

Public Hearing

-

909 CHURCH STREET

**Rezoning request for a dual-zoned
(T4-N / T4-HN) parcel**



STAFF REPORT: PLANNING COMMISSION (PC) – REZONING

909 CHURCH STREET

DATE: April 20, 2026

GENERAL INFORMATION		
Applicant:	Kelly Lamsens (Homeowner)	
Site Address / Tax PIN:	909 Church Street; R120 004 000 0275 0000	
Applicant's Request:	Applicant is requesting a Rezoning approval for 909 Church Street. Due to a mapping error during the 2019 unconsolidation and subdivision of the parent parcel, the property currently appears as dual-zoned T4-N / T4-HN. The 2019 subdivision was processed in error under the applicable planning requirements, and the rezoning corrects this error by applying a single T4-HN designation to the entire parcel.	
Current Zoning:	T4-N / T4-HN	
Current Land Use:	Undeveloped	
Flood Zone/Base Flood:	Flood Zone: X, Elevations: 17 – 19 ft. (Base-Flood Elevation: > 13 ft.)	
Existing Landmark/Specimen Trees:	No trees are affected by this request.	
ZONING DISTRICT INFORMATION		
T4-HISTORIC NEIGHBORHOOD DISTRICT (T4-HN)		
Minimum Lot Width:	40 ft min; 60 ft min in The Point	
Minimum Lot Size:	4,000 sf min; 6,000 sf min in The Point	
Max Impervious Lot Coverage:	55% (+10% for total impervious coverage)	
Front Setback	“Average Prevailing Setback on Block”	
Side Setback	Corner/Alley: 5 ft min, no max Interior: 6 ft min (10 ft min in The Point)	
Rear Setback	15 ft min; 0 ft from Alley	
Building Height:	3 stories max	
SURROUNDING ZONING, LAND USE AND REQUIRED BUFFERS		
Adjacent Zoning	Adjacent Land Uses	Required Buffer type(s) - see Sec. 5.4.1; 5.4.2; 5.4.3)
North: T4-N	Residential Structure – 1114 Greene St (Contributing Structure, ca. 1905)	N/A
South: T4-HN	Residential Structure – 905 Church St (Noncontributing Structure, ca. 1960)	N/A
East: T4-HN	Residential Structure – 1110 Greene St (Noncontributing Structure, ca. 1910; altered)	N/A
West: T4-HN	Residential Structure – 1202 Greene St (Contributing Structure, ca. 1910)	N/A

Background:

The applicant is requesting a Rezoning approval pursuant to Code Section 9.16. The applicant’s property, 909 Church Street, was formerly part of the parcel known as 1114 Greene Street. In June 2019, 1114 Greene Street was unconsolidated and later subdivided in November 2019, creating the new lot now identified as 909 Church Street. However, the subdivision and associated plat approval in 2019 were processed in error and did not comply with applicable planning requirements, and the current request serves to correct that procedural mistake. Additionally, the zoning district boundaries were not updated to match the new lot configuration and instead continue to reflect the previous lot lines, resulting in two residential properties that appear to be dual-zoned T4-N and T4-HN. 1114 Greene Street was, and remains, T4-N, while the newly created 909 Church Street received T4-HN designation. The applicant is requesting a rezoning to apply T4-HN to the correct, full bounds of 909 Church Street.

As shown on the current zoning map (see below), the zoning district boundaries still follow the former property lines between 1114 Greene Street and 909 Church Street. These boundaries were never updated after the lots were realigned and after 909 Church Street was expanded to include the rear portion of the original 1114 Greene Street parcel, which remained zoned T4-N.



Applicant’s Stated Reason(s) for Requesting Rezoning:

“The subject property currently contains split zoning (T4-HN and T4-N) that does not align with the existing parcel configuration due to a prior lot consolidation. This request is intended to establish a consistent zoning designation across the entire parcel in accordance with City planning principles.

Rezoning the property to T4-HN will eliminate the existing inconsistency, allow for orderly development of the property, and align with the surrounding neighborhood context.

The proposed use is a single-family residence with an accessory dwelling unit, which is compatible with the character and development pattern of the area.”

The Planning Commission shall study the proposed amendment, considering the following factors, in accordance with Code Sec. 9.16.3.C.2:

- a. Consistency (or lack thereof) with the Comprehensive Plan and Civic Master Plan.
 - ✓ Staff analysis: Consistency with the Comprehensive Plan and Civic Master Plan will be increased.
- b. Compatibility with the present zoning, with conforming uses of nearby property, and with the character of the neighborhood.
 - ✓ Staff analysis: Compatibility with present zoning is increased with the Rezoning request.
- c. Suitability of the property that would be affected by the amendment.
 - ✓ Staff analysis: Property will become more suitable with proper zoning district bounds.
- d. Compatibility with the natural features of — and any archaeological or cultural resources on — the property.
 - ✓ Staff analysis: Compatibility with the natural features of the property will be maintained. No archaeological or cultural resources are affected by this request.
- e. Marketability of the property that would be affected by the amendment; and
 - ✓ Staff analysis: Marketability of the property will be increased due to the property having its correct zoning district bounds.
- f. Availability of roads, sewer, water, and stormwater facilities generally suitable and adequate for the affected property.
 - ✓ Staff analysis: Infrastructure is not affected by this request.

FINDINGS AND RECOMMENDATIONS

Findings: Rezoning request meets all Code requirements and factors.

Staff Recommendation: **Approve Rezoning request.**

Location of Subject Property on City of Beaufort Zoning Map



4/14/2026, 4:17:09 PM

City of Beaufort Road Classifications Layer ■ T4-HN

— STATE,PAVED

Address Numbers

□ CityOfBeaufort_Parcels

□ City Limits Public

City of Beaufort Zoning

■ T4-N

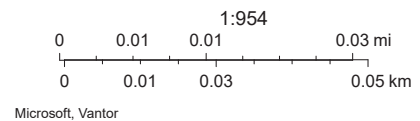
World Imagery

Low Resolution 15m Imagery

High Resolution 60cm Imagery

High Resolution 30cm Imagery

Citations



**BEAUFORT COUNTY
HISTORIC SITES SURVEY - 1997**

RECONNAISSANCE LEVEL BUILDING INVENTORY FORM

Statewide Survey Site Form
State Historic Preservation Office
South Carolina Department of Archives and History
Columbia, SC

Site Number: U- 13 - 1088
Access County Site #
USGS Quad: 025 Beaufort
Doc. Level: Reconnaissance Level

Historic name(s): Ed Pruitt's Grocery Store Map Ref.: BFT 09 (TL) Tax Number: R120 004 000 0274 0000
Common name(s): _____ City Block Ref.: 85 Island: Port Royal Is.
Address/location: 1114 Greene St. City/Vicinity of (vic.) Beaufort
Date: 1905 ca. Alteration date: _____
Ownership: 1. private 2. city 3. county 4. state 5. federal 6. unknown
Category: 1. building 2. site 3. structure 4. object
Historic use(s): commercial - store/shop Current uses: vacant

Notes: Rectangular 1 story 3 x 3 bay frame store w/front-facing gable roof

National Register Status: _____ Date: _____ Listing Name: _____ NRIS # _____
National Register Historic District (NHL, 11/73) 12/17/69 Beaufort Historic District 69000159

SHPO National Register Evaluation: Contributes to listed district Name: Beaufort Historic District

Consultant Recommendation: _____ Name: _____

Previous Survey: _____ Reference: _____ Notes: _____
 H.A.B.S. Other _____
 Feiss-Wright (1969) _____
 Historic Resources of the Lowcountry (1979) 38 BU 1089 The Neighborhood
 Milner Historic District Inventory (1979) _____
 A Guide to Historic Beaufort (1995 ed.) _____

Photograph:



Photographs:
 prints
 slides
 negatives

Date: 8/1/97

Recorder: D. Schneider, Historic Beaufort Fdn.

Roll # Neg. View of:
S-10 9 N facade & W elev., fac. SE



REQUEST TO UNCONSOLIDATE LOT APPLICATION

Department of Planning & Development Services
1911 Boundary Street, Beaufort, South Carolina, 29902
p. (843) 525-7011 / f. (843) 986-5606
www.cityofbeaufort.org

Application Fee: \$25

Receipt # [REDACTED]

OFFICE USE ONLY: Date Filed: 6/14 Application #: 18373 Zoning District: TC-1-N

Pursuant to Section 6-29-1145 of the South Carolina Code of Laws, is this tract or parcel restricted by any recorded covenant that is contrary to, conflicts with, or prohibits the activity described in this application? Yes No

\$ PAID
6/14 CAH

Applicant, Owner and Property Information

PROPERTY IDENTIFICATION NO.: 120 004 000 0274/2075

Applicant Name: The Bismark Co

Applicant Address: 820 Bay St.

Applicant Phone: [REDACTED]

Property Owner Name: The Bismark Co / John Mark III

Please unconsolidate lots as shown on the attached plat.

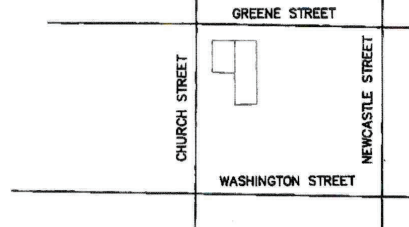
Applicant's Signature: [Signature] Date: 6/14/19

Approved By: [Signature] Date: 6/14/19

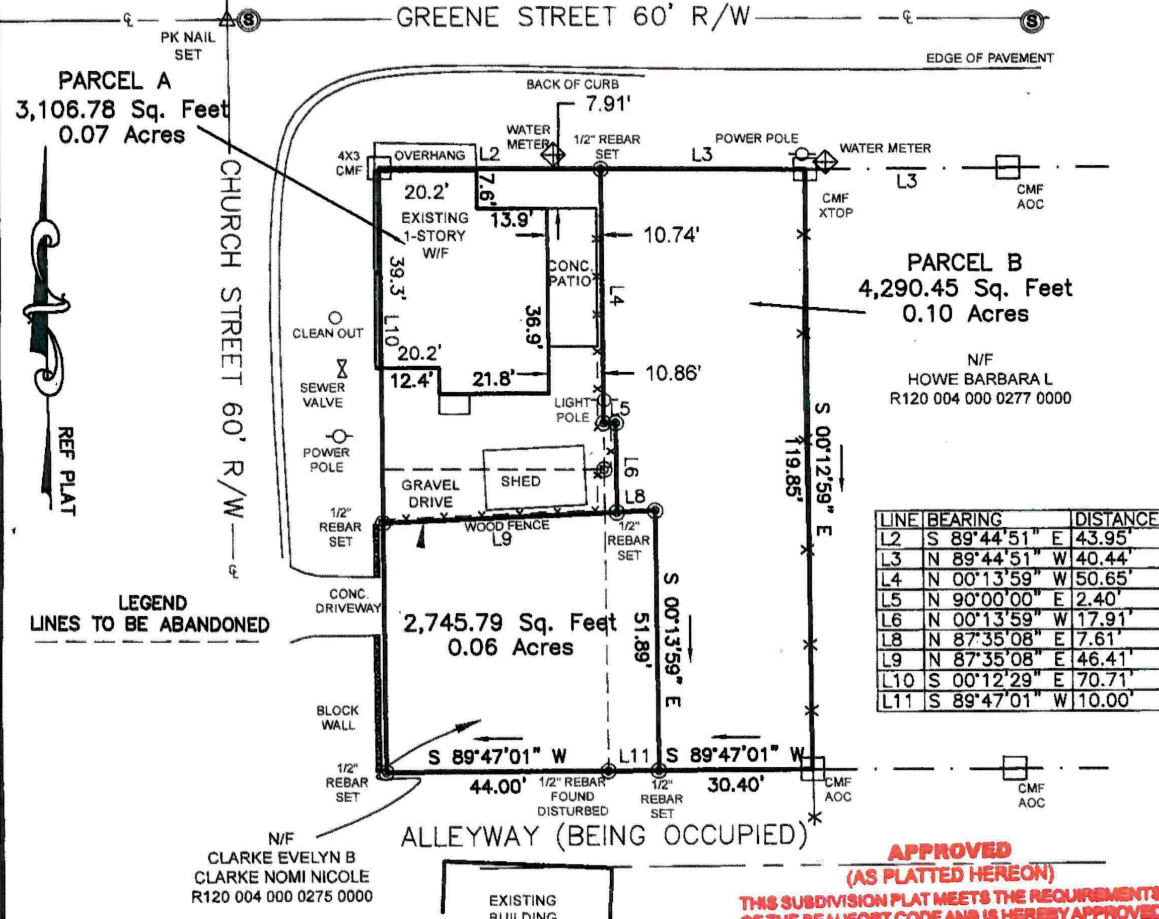


GASQUE & ASSOCIATES INC.
LAND SURVEYORS & PLANNERS

28 PROFESSIONAL VILLAGE CIRCLE, BEAUFORT, S.C.
 P.O. BOX 1363, BEAUFORT, S.C.
 PHONE (843) 522-1798



VICINITY MAP NOT TO SCALE



LINE	BEARING	DISTANCE
L2	S 89°44'51" E	43.95'
L3	N 89°44'51" W	40.44'
L4	N 00°13'59" W	50.65'
L5	N 90°00'00" E	2.40'
L6	N 00°13'59" W	17.91'
L8	N 87°35'08" E	7.61'
L9	N 87°35'08" E	46.41'
L10	S 00°12'29" E	70.71'
L11	S 89°47'01" W	10.00'

APPROVED
 (AS PLATTED HEREON)
 THIS SUBDIVISION PLAT MEETS THE REQUIREMENTS OF THE BEAUFORT CODE AND IS HEREBY APPROVED FOR RECORDING WITH THE BEAUFORT COUNTY REGISTER OF MEANS CONVEYANCES.

Certified by: *[Signature]*
 Date: 6/14/19

LOT LINE REVISION SURVEY PREPARED FOR JOHN TRASK III

THIS BEING PORTIONS OF BLOCK 85 CITY OF BEAUFORT BEAUFORT COUNTY SOUTH CAROLINA. SET FORTH ON A PLAT OF THE TOWN BY THE SOUTH CAROLINA U.S. DIRECT DISTRICT TAX COMMISSIONERS. LOCATED IN THE CITY OF BEAUFORT, BEAUFORT COUNTY, SOUTH CAROLINA. T.M.S. R120 004 000 0274 & 0275.

THE CERTIFIER HAS NOT INVESTIGATED OR BEEN INSTRUCTED TO INVESTIGATE THE EXISTENCE OR NONEXISTENCE OF ANY OVERLAY DISTRICTS, SUCH AS; AIRPORT, MILITARY, NOISE, CRASH POTENTIAL OR ENVIRONMENTAL ISSUES.

CERTIFICATIONS ARE NOT TRANSFERABLE TO ADDITIONAL INSTITUTIONS OR SUBSEQUENT OWNERS. THIS PLAT IS COPYRIGHTED AND IS INTENDED ONLY FOR THE ENTITY OR PERSON(S) SHOWN HERE ON. THIS PLAT REPRESENTS A SURVEY BASED ON THE LISTED REFERENCES ONLY AND IS NOT THE RESULT OF A TITLE SEARCH.

SETBACKS SHOWN AS PER PLAT OF RECORD AND MUST BE VERIFIED WITH OFFICIAL AGENCY BEFORE PURCHASE AND OR CONSTRUCTION ON SUBJECT PROPERTY.

BEFORE ANY DESIGN WORK OR CONSTRUCTION ON THIS SITE IS STARTED FLOOD ZONE INFORMATION MUST BE VERIFIED BY PROPER BUILDING CODES OFFICIAL.

THIS PROPERTY APPEARS TO LIE IN FLOOD ZONE C AS DETERMINED BY FEMA FIRM COMM-PANEL NUMBER 450026 0005 D, DATED SEPTEMBER 29 1986.

THE UNDERSIGNED DOES NOT CERTIFY THAT THE PROPERTY SHOWN HEREON COMPLIES WITH ANY RESTRICTIVE CONVEYANCES OR ANY CITY AND/OR COUNTY ORDINANCES.

I, DAVID E. GASQUE, HEREBY CERTIFY TO JOHN TRASK III THAT TO THE BEST OF MY KNOWLEDGE, INFORMATION AND BELIEF, THE SURVEY SHOWN HEREON WAS MADE IN ACCORDANCE WITH THE REQUIREMENTS OF THE MINIMUM STANDARDS MANUAL FOR THE PRACTICE OF LAND SURVEYING IN SOUTH CAROLINA, AND MEETS OR EXCEEDS THE REQUIREMENTS FOR A CLASS A SURVEY AS SPECIFIED THEREIN. ALSO THERE ARE NO ENCROACHMENTS OR PROJECTIONS OTHER THEN SHOWN. THIS SURVEY IS NOT VALID UNLESS IT BEARS THE ORIGINAL SIGNATURE AND HAS AN EMBOSSED SEAL. AREA DETERMINED BY COORDINATE METHOD.



DATE: 11/13/2018

[Signature] 5-31-19
 DAVID E. GASQUE, R.L.S. JOB # 49760
 S.C. REGISTRATION NUMBER 10506 FB#939/TT DSGN#7
 THIS PLAT IS NOT BINDING UNLESS ACCOMPANIED BY AN ORIGINAL SIGNATURE AND AN EMBOSSED SEAL.



MINOR SUBDIVISION APPLICATION
SUBDIVISIONS LESS THAN 10 ACRES

Community & Economic Development Department
1911 Boundary Street, Beaufort, South Carolina, 29902
p. (843) 525-7011 / f. (843) 986-5606 | www.cityofbeaufort.org

PAID
11-20 CA
Application Fee: \$50
Receipt # 129254

OFFICE USE ONLY: Date Filed: 11/20 Application #: 91,30 Zoning District: _____
Total Acreage: _____ # Of Lots: _____

Applicant and Property Information

Applicant Name: The Bismark Co/ LLC TRASE LLC

Applicant Address: 920 BAY ST Beaufort SC

Applicant E-mail: _____ Applicant Phone Number: _____

Applicant Title: Homeowner Tenant Architect Engineer Developer

Property Owner (if other than the Applicant): same

Property Owner Address: _____

Property Owner E-Mail: _____ Property Owner Phone Number: _____

Property Address: _____

Property Identification Number (Tax Map & Parcel Number): R 120 00400 0274 + 0275

Required Application Information

Submittal Requirements:

1. Applicant Fee - \$50.00
2. Water and Sewer Certification form signed by Beaufort-Jasper Water & Sewer Authority that water and sewer is available to the lots
3. Encroachment permit from S.C. Department of Transportation (843) 524-7255.
4. Five (5) copies of the plat.

Subdivision Plat Shall Include:

- Label as "Subdivision Plat"
- North arrow, graphic scale
- Vicinity map, specifying that the location is within the City
- Boundaries in bearing and distances
- Street Name(s)
- City limits line (if applicable)
- Floodplain information
- AICUZ Disclosure Statement (if applicable)
- Reference City of Beaufort Codes Office and Beaufort Code
- Name of owner
- All existing structures
- Building setbacks
- All trees >8" DBH on the lot(s) to be developed
- Surveyor's raised seal

Applicant's Signature: [Signature] Date: 11/20/19

Owner's Signature: [Signature] Date: 11/20/19

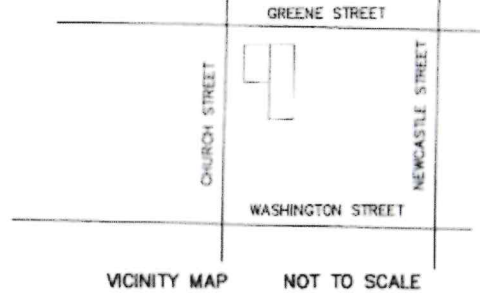
Approved By: [Signature] Date: NOV 22 2019



GASQUE & ASSOCIATES INC.
LAND SURVEYORS & PLANNERS

28 PROFESSIONAL VILLAGE CIRCLE, BEAUFORT, S.C.
 P.O. BOX 1363, BEAUFORT, S.C.
 PHONE (843) 522-1798

CITY OF BEAUFORT
 PLANNING DEPARTMENT
 1911 BOUNDARY STREET
 BEAUFORT, SC 29902

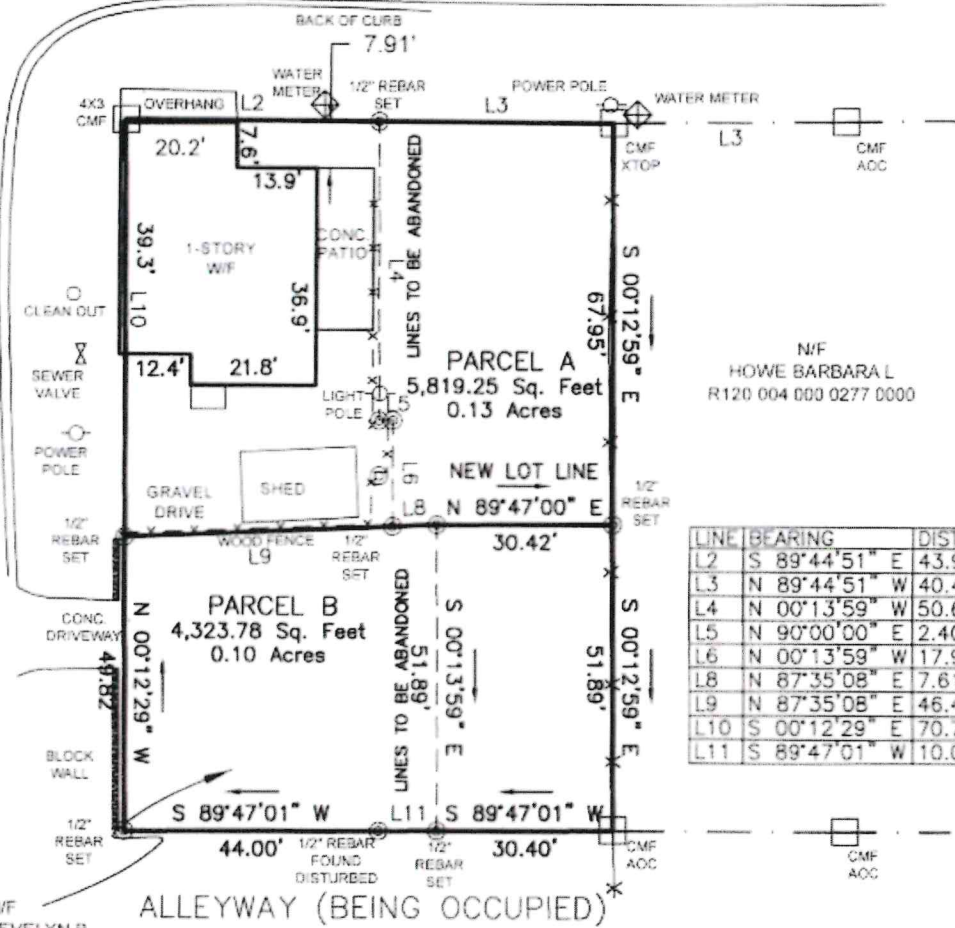


VICINITY MAP NOT TO SCALE

PK NAIL SET ——— GREENE STREET 60' R/W ——— S



CHURCH STREET 60' R/W



LINE	BEARING	DISTANCE
L2	S 89°44'51" E	43.95'
L3	N 89°44'51" W	40.44'
L4	N 00°13'59" W	50.65'
L5	N 90°00'00" E	2.40'
L6	N 00°13'59" W	17.91'
L8	N 87°35'08" E	7.61'
L9	N 87°35'08" E	46.41'
L10	S 00°12'29" E	70.71'
L11	S 89°47'01" W	10.00'

LEGEND
 LINES TO BE ABANDONED

N/F
 CLARKE EVELYN B
 CLARKE NOMI NICOLE
 R120 004 000 0275 0000

ALLEYWAY (BEING OCCUPIED)

APPROVED
 (AS PLATTED HEREON)
 THIS SUBDIVISION PLAT MEETS THE REQUIREMENTS OF THE BEAUFORT CODE AND IS HEREBY APPROVED FOR RECORDING WITH THE BEAUFORT COUNTY REGISTER OF MEANS CONVEYANCES.

Certified by: AM Spade
 Date: November 22 2019

LOT LINE REVISION SURVEY PREPARED FOR
JOHN TRASK III

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Public Hearing

**Beaufort Development Code Changes –
Amendment to Chapter 4
(Building Design / Infill Standards)**

4: BUILDING DESIGN AND INFILL STANDARDS

4.1: PURPOSE AND INTENT

4.1.1 PURPOSE

The purpose of this section is to:

- A. Protect and enhance Beaufort's unique aesthetic character.
- B. Support quality streets and public spaces.
- C. Encourage architecture that blends harmoniously with the natural and built environment.
- D. Carefully preserve the character of Beaufort's National Historic Landmark District while permitting appropriate growth.

4.1.2 INTENT

The standards and guidelines in this section are not meant to stifle innovative design or creativity. Instead, they are intended to serve as the minimum standards and guidelines necessary to ensure that new development and redevelopment meets the purposes described above.

4.2: APPLICABILITY

4.2.1 TERMINOLOGY

- A. **Shall:** This section establishes both binding standards and discretionary guidelines. Binding standards are typically signified the word "shall." They are specific and precise standards that are required to be met in the submitted design.
- B. **Should, Encouraged and Discouraged:** Discretionary guidelines are typically signified by the words "should," "encouraged," and "discouraged." They are general design objectives that are applied and interpreted by the appropriate Design Review Body in the review of development applications. "Shoulds" are the goal, but the Design Review Body has flexibility as to how they are administered.
- C. **May:** Discretionary guidelines with a greater degree of flexibility are signified by the word "may." They are ideal guidelines and a project following these guidelines would result in an outcome that is very compliant with the City's goals and objectives; however, they are not specifically required to be met.

4.2.2 APPLICABILITY

- A. **Applicability:** The standards apply to all construction
- B. **Exemptions:** Where these standards are exempt, as listed below, this section may still be used as design guidelines for new construction as they outline building practices that are traditional to the Beaufort area.

-
1. Modification to existing structures, unless they require design review as specified in Sections 9.8 and 9.9, are exempt from the requirements of this section.
 2. All construction in LI districts are exempt from all standards except for the provisions in 4.6.4.A, where the structure is visible from a Primary Street - See Street Hierarchy Diagram in Appendix C.

4.2.3 RELATIONSHIP TO HISTORIC DISTRICT STANDARDS

- A. **Applicability:** In addition to the standards and guidelines in this article, any development located within the Beaufort Historic District is subject to the standards, guidelines, and procedures established in Section 9.10.
- B. **Conflicts:** In the event of a conflict between the requirements of this article and the requirements and guidance provided by the documents listed in 9.10.2.B, the later shall take precedence.

4.2.4 RELATIONSHIP TO LAND USE PROVISIONS

- A. **Applicability:** Article 2, and Article 3 (Land Use Provisions) identify lot standards and use allowances by district and establish additional standards applicable to specific uses. These provisions shall also apply in conjunction with the standards outlined in this article.
- B. **Conflicts:** In the event of a conflict between the requirements of this article and the requirements of Article 2, and Article 3, the most stringent requirement, as determined by the Administrator, shall apply.

4.3: CONTEXTUAL DESIGN GUIDELINES

4.3.1 PURPOSE AND APPLICABILITY

All buildings possess a number of common elements that combine to express a structure both as an entity and as a part of the larger community. No building is so insulated from its surroundings as to avoid an impact on the surrounding context. Applications that require Major Design Review (Section 9.8.2) and Certificate of Appropriateness, Major (Section 9.10.2), are subject to the additional contextual design guidelines of this section. These guidelines shall be used by the Design Review Body as applicable, to evaluate the appropriateness of the proposed construction to its immediate context and the character of the broader community.

- A. **Design Guidelines.** The following design principles serve as criteria for staff and the Design Review Body for decision-making as they relate to the City of Beaufort's Lowcountry Vernacular in the review of proposed development and redevelopment throughout the Town. The goal of the design standards is to maintain the traditional small city/town feel of Beaufort as it continues to grow. The purpose of this section is to:
 - a. Protect and enhance Beaufort's unique aesthetic character
 - b. Support high quality streets and public spaces
 - c. Encourage architecture that respects the historical context and/or blends harmoniously with the natural surroundings and neighboring development
 - d. Safeguard property values and long-term economic assets
 - e. Carefully preserve the character of Beaufort's National Historic Landmark District while permitting appropriate growth within and beyond the Historic District.
- B. **Intent.** The standards and guidelines in this Section are not meant to stifle innovative design or creativity. Instead, they are intended to serve as the minimum standards and guidelines necessary to ensure that new development and redevelopment meets the purposes described above. Designs should respond to the Lowcountry's climate, landscape, and cultural heritage, promoting human-scaled, pedestrian-oriented

development that complements Beaufort’s established patterns of streets, blocks, and buildings. The use of authentic materials, simple building forms, and balanced proportions is essential to achieving the visual harmony and timeless quality characteristic of Beaufort’s architecture.

The intent is also to guide new development in a manner that:

- a. Strengthens the sense of continuity between historic and new structures through compatible scale, rhythm, and detail;
- b. Enhances the pedestrian experience and fosters vibrant, active streetscapes;
- c. Encourages site and building design that responds to the Lowcountry environment—including the use of porches, shade, and natural ventilation;
- d. Promotes variety within a cohesive visual framework, avoiding repetition or standardized corporate imagery; and
- e. Ensures that every project contributes positively to Beaufort’s enduring identity as a walkable, gracious, and resilient coastal community.

C. **Lowcountry Vernacular.** The architectural vernacular of Beaufort embodies the qualities found throughout the historic Lowcountry, prior to the advent of air conditioning,—simple in form, functional in design, and authentic in material expression. These buildings were constructed with practical intent and shaped by local design and craftsmanship. These designs required a deep understanding of the local climate, building materials, sun and wind angles, and flood levels to ensure buildings could survive and remain comfortable in a hot, humid environment. Future development should continue this tradition by drawing inspiration from the simplicity, proportion, and detail of Beaufort’s vernacular heritage, ensuring that new construction strengthens the city’s distinct sense of place. By incorporating these principles, new construction becomes inherently more sustainable, resilient, and connected to the land.

4.3.2 SPECIFIC GUIDELINES

- A. **Conformity to Civic Master Plan:** The Civic Master Plan provides site-specific guidance for the development of many parcels within the city. The intent of this Code is to facilitate the vision and ensure the visions are permitted. In instances where the Civic Master Plan provides guidance for building and site design standards on a particular parcel, development applications on that parcel should meet the general intent of such guidance to the extent practicable, as determined by the Design Review Body.
- B. **Rhythm of Development on the Street:** Monolithic massing that disrupts the predominant building pattern of the neighborhood and corridor is strongly discouraged. Traditionally, structures were placed and arranged so that the most public portion of the building (e.g., the entrance) orients outward toward the public realm (i.e., a street, park, plaza, or private courtyard area connecting to a street). The front of buildings must be arranged to create a strong and discernible connection with the public realm.



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- C. **Architectural Theme.** All buildings and structures within a proposed development, including canopies, shall utilize a unifying architectural theme and be uniform in character in order to create a harmonious whole. However, it is not to be inferred that buildings must look alike to achieve a harmony of style. The scale of buildings and accessory structures (including canopies) shall be appropriate to the scale of structures located in the surrounding area. Canopies designed as domineering or overpowering architectural features are not allowed.
- D. **Simplicity of Massing and Form.** Traditional Lowcountry buildings are characterized by clarity of form and restraint in composition. Historically, structures were built by hand using straightforward methods, which naturally resulted in simple, functional shapes rather than elaborate or decorative massing. This economy of form produced buildings that were both efficient and elegant, reflecting practicality, proportion, and human scale. New construction should continue this tradition by employing simple, well-composed masses, typically organized as one or more complementary ~~rectangular~~ volumes. Complex geometries, excessive articulation, or irregular forms are inconsistent with the Lowcountry vernacular and should be avoided. Each building element should contribute to a coherent overall form that reflects craft, simplicity, and purpose.
- E. **Legible Hierarchy and Order of Elements.** Traditional Lowcountry buildings express a clear hierarchy—primary forms are emphasized, while secondary elements are simplified. The most important portions of a building receive greater attention to proportion and detail, while supporting components remain subordinate. New buildings should maintain this order through distinct primary, secondary, and tertiary masses, ensuring a coherent composition where each element’s importance is visually legible.
- F. **Harmonious Proportions.** Lowcountry buildings are composed with a clear hierarchy that expresses both order and human scale. Primary forms—such as main building volumes or central masses—are vertically oriented and proportioned to feel balanced and approachable. Secondary and connecting elements are simpler, allowing the primary forms to remain legible and grounded in the landscape.

This hierarchy of massing, proportion, and detail ensures that buildings maintain a comfortable vertical rhythm and a scale perceptible to pedestrians, reinforcing the clarity and character of traditional Lowcountry architecture.

1. Human scale: Prioritize vertical proportions, so that architectural features appear taller than they are wide, using historical proportions. This reinforces the human scale of the streetscape, rather than a monolithic or oversized horizontal one.
 2. Narrow frontages: Like Charleston's single houses, narrow street frontages allow for a greater variety of unique building fronts along the sidewalk, making the pedestrian experience more engaging.
 3. Mixed-use appearance: Multi-family and mixed-use buildings should maintain the character of large single-family residences. Any commercial elements should be expressed through simple, compatible modifications to the residential form.
- G. **Consistent Arrangement of Elements.** Traditional architecture arranges openings, columns, and elements of the building in a manner that is based on practicality, ease of construction, and time efficiency. Window openings must be arranged regularly and symmetrical along a building face.
- H. **Top, Middle, and Base.** Vertical elements must be composed of a clearly visible top, middle, and base. These three separate concepts are rooted in classical patterns, provide order, and give articulation to a building. Building elevations must clearly illustrate a visually pronounced first floor or base level, a middle ground consisting of all floors above the base level, and an articulated top consisting of the final floor or top level with defined cornice or sloped roof profile.
- I. **Relationship to the Street.** Traditionally, structures were placed and arranged so that the most public portion of the building (e.g., the entrance) orients outward toward the public realm (i.e., a street, park,

plaza, or private courtyard area connecting to a street). The front of buildings must be arranged to create a strong and discernible connection with the public realm.

- J. **Relationship to Environs.** Lowcountry vernacular architecture responds to and connects with the natural environment and regional climate. Building placement, orientation to the sun, natural light and ventilation, materiality, and shading all lend toward developing a strong sense of character that is unique to Beaufort Lowcountry Vernacular. New construction must incorporate elements that relate well to the natural setting and climate.
1. Porches and arcades: Provide shelter for pedestrians using elements like arcades, colonnades, marquees, or awnings. Like traditional piazzas, these features offer shade and protect people from the elements.
 2. Raised foundations: Lowcountry buildings should incorporate proportional raised foundation(s) or water table, often of stucco or brick, to protect against flooding and accommodate the low-lying terrain.
 3. Southern orientation: Buildings should be oriented to take advantage of prevailing breezes and river views, using porches and piazzas to provide shade.
 4. Maximizing air flow through single-room wide floor plans.
- K. **Detailing.** Primary building materials were limited to those readily available in the area—predominantly wood and brick. Roofs generally feature a hipped or gabled roof, often with broad eave overhangs and exposed rafter tails or fascia trim. Boxed eaves, when present, feature a traditional pedimented gable or return detail. Roofing materials are historically metal standing seam or 5-V crimp and later shingles. Windows are abundant for light and ventilation. Window shapes and panes are taller than wide. Windows occur as singles, pairs or, in some instances, triples with a single, wide “picture frame” trim. Decorative trim and detailing is generally limited to the pedestrian areas of the building, such as display windows and entry elements, although some simple variations of cornice trim may be found at the eaves and in the gables. Large porches are common at the primary entries, with square wood columns that are often chamfered or simple round Tuscan columns.
1. Tactile details at street level: The ground floor of commercial buildings should feature high-quality, durable materials and detailed craftsmanship. This can be achieved by using a different material or a slight setback to define a distinctive “base” for the building.
 2. Surface articulation: Use architectural features like columns, pilasters, and canopies to divide large wall areas into smaller, visually interesting components, as opposed to a single, flat facade.
 3. Refined craftsmanship: Echo the tradition of expert craftsmanship found in Beaufort's simpler historic buildings by protecting and celebrating distinctive materials, features, finishes, and construction techniques.
- L. **Vista and View Shed.** Lowcountry Vernacular structures are generally framed within their settings, often incorporating large trees and roadways into a visual experience. Where it is reasonably practical, proposed structures shall not impede scenic views from the main road, from existing structures, or from natural settings. Structures must strengthen and maintain vistas and view sheds.
- M. **Relationship to Adjacent Structures.** Although maximum site densities and special site requirements defined for particular use districts shall be preserved, proposed structures shall not dominate, in an incompatible manner, an adjacent building, nor in surrounding general development substantially in compliance with this chapter.
- N. **Cohesive Design.** Cohesive design elements create harmony through related massing, scale, proportion, detail, materials, color, site planning, and landscaping.

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1. All elevations of a structure shall be in harmony with one another in terms of scale, proportion, detail, material, and color. The side and rear elevations of buildings shall be as visually attractive as the front elevation. Rooflines and architectural detailing shall present a consistency in quality design.
 2. All buildings and structures within a proposed development, including canopies, shall utilize a unifying architectural theme and be uniform in character to create a harmonious whole. However, it is not to be inferred that buildings must look alike to achieve a harmony of style. The scale of buildings and accessory structures (including canopies) shall be appropriate to the scale of structures located in the surrounding area. Canopies designed as domineering or overpowering architectural features are not allowed.
 3. The architecture, landscaping, and building siting must work in concert to create a unified appearance that contributes to the existing/developing streetscape.

O. Architectural Compatibility.

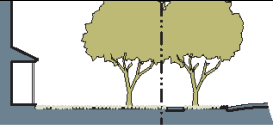
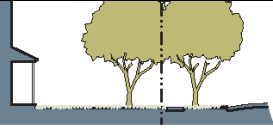
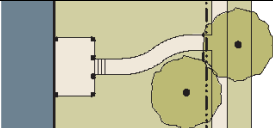
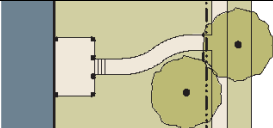
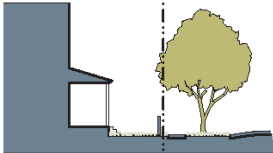
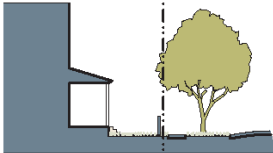
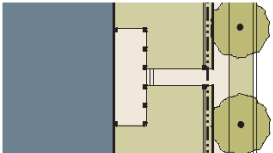
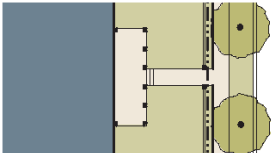
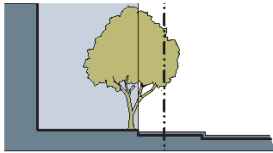
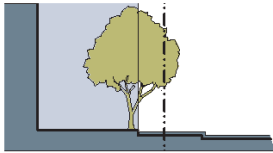
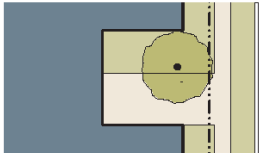
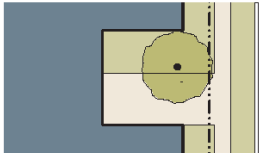
1. The architectural design and material finish of buildings, signage, gasoline pump canopies, and other necessary structures shall be compatible with one another, and with adjacent and surrounding structures where such structures are substantially in compliance with this section.
2. In reviewing a proposed structure, specific considerations shall be given to its compatibility with adjacent structures.
3. Proposed structures shall be shown to scale, in a streetscape view with adjacent buildings, by using elevations or, in certain instances, photographs, of adjacent structures.

P. Local Design Quality. Structures that are of symbolic design for reasons of advertising, corporate identity or corporate continuity shall be highly discouraged; however, the Planning Commission or staff may work with applicants to incorporate limited signature design elements into buildings, to allow recognition of certain corporate or signature identity, while still retaining the ability to achieve a project design unique to Beaufort. Similarly, prototype architecture is prohibited; however, the Planning Commission or staff may work with applicants to incorporate limited signature design elements into buildings, to allow recognition of certain corporate or signature identity, while still retaining the ability to achieve a project design unique to Beaufort. The need for a building's program to be laid out in a specific manner congruent with a company's typical business model cannot override the need to alter the design to reflect Beaufort's traditional vernacular architecture.




Q. Consideration for Contemporary Architectural Treatments. Contemporary architectural treatments shall not be prohibited where it maintains the traditional form, massing, design elements, materials, and adherence to design principles of Beaufort Lowcountry Vernacular, and arranges the components in a new and/or modern way such that the basic nature and form of the traditional architecture is clearly visible and compatible with surrounding structures and sites. Commercial buildings should reflect and incorporate traditional characteristics and patterns with simple modifications made to accommodate emerging market trends and commercial activities.

R. Consideration for Taller Buildings. Structures that are excessively wide and tall are not consistent with Beaufort Lowcountry Vernacular tradition. Vernacular structures were generally no taller than 38-40 feet, given the sum of heights of the base, floor-to-floor levels, and roof of a traditional two-story structure. Therefore, modern buildings generally taller than 3 stories and 45 feet should employ traditional design principles to mitigate excessive massing and scale of building elements and relate to traditional form and models.





4.4: PRIVATE FRONTAGE TYPES

	SECTION		PLAN		PERMITTED ZONING DISTRICT(S)
	Lot - Private Frontage	R.O.W. Public Frontage	Lot - Private Frontage	R.O.W./ Public Frontage	
<p>A. Front Yard: This is a planted Frontage, wherein the facade is set back substantially from the frontage line. The front yard created may be fenced or unfenced (see Section 2.5.6 H.). The deep setback provides a buffer from the higher speed thoroughfares.</p>					T3
<p>B. Porch: This is a frontage wherein the facade is set back from the frontage line with an attached porch permitted to encroach (see Section 2.5.6). Porches may be constructed in front of the minimum required setback but shall not extend into the right-of-way. Porches shall have a minimum depth of 8 feet and a minimum width of 25% of the primary facade. Porches may be screened; however, if screened, all architectural expression (columns, railings, pickets, etc.) must occur on the outside of the screen. Porch frontages may be fenced or unfenced (see Section 2.5.6 H.).</p>					T3, T4
<p>C. Forecourt: This is a frontage wherein a portion of the building's massing is close to the frontage line and the central portion is set back to create a void in the building's form. They serve as transitional areas between private and public space and may be used in residential buildings to provide entry yards and/or shared garden space, or in commercial and mixed-</p>					T4, T5, RMX, IC


- 4: BUILDING DESIGN AND INFILL STANDARDS
4.4: PRIVATE FRONTAGE TYPES

<p>use buildings for outdoor dining, display of merchandise, entries to individual tenants, or vehicular drop-off areas. Where provided, forecourts shall be a minimum of 16 feet in depth and 16 feet in width. This type should be allocated in conjunction with other frontage types. Breezeways in any form do not constitute a forecourt and are not to be interpreted as such. A forecourt's form must continue up all floors, not just the ground floor.</p>		
<p>D. Stoop: This is a frontage wherein the facade is aligned close to the frontage line with the first story elevated from the sidewalk sufficiently to secure privacy for the windows. The entrance is usually an exterior stair and landing with a covered or recessed entry door. This type is recommended for ground-floor residential use. Stoops may be constructed in front of the minimum required setback but shall not extend into the right-of-way. Stoops shall have a minimum depth and width of 4 feet. Stoops may be shared by 2 attached units. Stoop stairs may run to the front or to the side.</p>		<p>T4, T5, RMX, IC</p>
<p>E. Balcony: This is a frontage wherein the facade is aligned close to the frontage line with an attached cantilevered or bracketed balcony(ies) above at least 50% of the facade, including major entries. This type is conventional for apartment houses and live-work/mixed-use buildings. The balcony shall be no less than 3 feet deep and must be visually supported. French balconies (ones that are flush with the building) may be used, but are not considered balcony frontage, and would need</p>		<p>T4, T5, RMX, IC, LI</p>




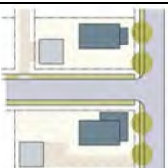
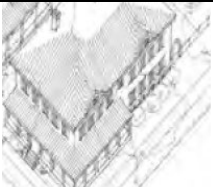

- 4: BUILDING DESIGN AND INFILL STANDARDS
4.4: PRIVATE FRONTAGE TYPES

<p>to be combined with another frontage type.</p>		
<p>F. Shopfront/Awning: This is a frontage wherein the facade is aligned close to the frontage line with the building entrance at sidewalk grade. This type is conventional for retail use. It has a substantial glazing on the sidewalk level. This frontage may also be used in conjunction with forecourt, Gallery/Colonnade, or balcony frontage types. Where an awning exists, it shall be a minimum of 5 feet deep and 8 feet above the sidewalk. Awnings shall be made of fabric or metal, but high-gloss and plasticized fabrics are prohibited.</p>		<p>T4, T5, RMX, IC, LI</p>
<p>G. Gallery/Colonnade: This is a frontage wherein the facade is aligned close to the frontage line with an attached cantilevered shed or a lightweight colonnade overlapping the sidewalk. This type is conventional for retail use. The gallery shall be a minimum of 10 feet wide and may overlap the sidewalk to within 2 feet of the curb.</p>		<p>T4, T5, RMX, IC, LI</p>
<p>H. Arcade: This is a colonnade supporting habitable space that overlaps the sidewalk, while the facade at sidewalk level remains at or behind the frontage line. This</p>		<p>T4, T5, RMX, IC, LI</p>


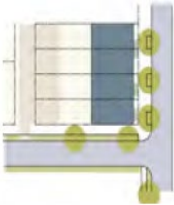

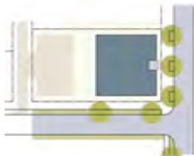

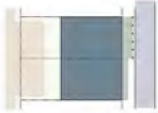
- 4: BUILDING DESIGN AND INFILL STANDARDS
 4.4: PRIVATE FRONTAGE TYPES

<p>type is conventional for retail use. The Arcade shall be a minimum of 12 feet wide and may overlap the sidewalk to within 2 feet of the curb.</p>		
<p>I. Formal Entry Courtyard: This is a frontage defined by an outdoor space that is partially enclosed by the form of the building (typically enclosed on three sides), and functions as a private or semi-private open space for light, air, recreation, circulation, or landscaping. This frontage type is primarily used for multi-unit apartment buildings, in which several units face into this courtyard. Windows are to be placed in units that are visible from the courtyard. The formal entry courtyard serves as the primary entrance point for a building, in which the main entrance is clearly defined at the geometric center of the courtyard.</p>		<p>T4, T5, RMX, IC, LI</p>

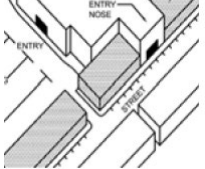
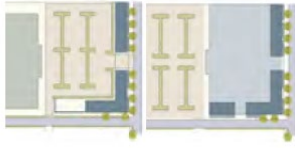
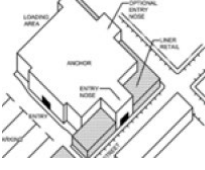
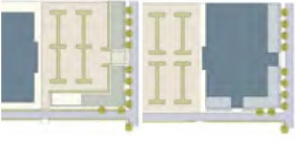
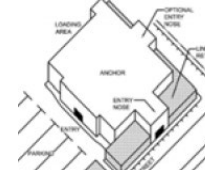

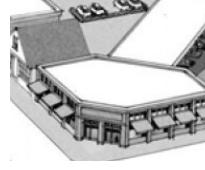

4.5: BUILDING TYPES

	SKETCH	PLAN	PERMITTED ZONING DISTRICT(S)
<p>A. Carriage House: This is an accessory structure that provides small, flexible living spaces adjacent to a main house (a.k.a. Accessory Dwelling Unit [ADU], Granny Flat). It is often used for rental housing, and may be free standing, or located above a garage or parking area.</p>			T3, T4, T5, RMX, IC
<p>B. Single-Family Detached House: This is the predominant residential building type in Beaufort. This house sits on a private lot and can vary in size from a small cottage to a large mansion. Setbacks and frontage types vary by transect zone.</p>			T3, T4
<p>C. 2-3 Unit House: This is a house-form that seamlessly fits into a predominantly single-family neighborhood but contains multiple dwelling units. The units may be side-by-side or stacked. They typically have separate entrances from the street but may share a common entrance. They are located under one roof and do not have parapets dividing the units.</p>			T3, T4, T5, IC

- 4: BUILDING DESIGN AND INFILL STANDARDS
4.5: BUILDING TYPES

	SKETCH	PLAN	PERMITTED ZONING DISTRICT(S)
<p>D. Rowhome/Townhouse: This is an attached residential building type that is part of a series of other rowhomes with more than 3 units in a row. The combination of 3 or fewer units is considered a 2-3 unit building. Rowhomes may have parapet walls dividing the units or be combined under one roof form. They are elevated above the street a minimum of 3 feet for privacy and are typically accessed by stoops. They may also utilize the forecourt and porch frontage types.</p>			T4-N, T5, RMX, IC
<p>E. Apartment House: This is a larger-scale building (a.k.a. Multi-Family Building) that contains 4 or more dwelling units. They often occupy corner lots or are located in combination to create their own blocks. The ground floor is raised a minimum of 18 inches for privacy.</p>			T4, T5, RMX, IC
<p>F. Livework/Mixed-Use: This is a building that contains commercial space, typically at grade, with office or residential living, typically located on the upper level(s). They are typically attached but may be freestanding structures. The ground floor has a substantial amount of glazing and often utilizes the shopfront frontage type.</p>			T4, T5, RMX, LI, IC

- 4: BUILDING DESIGN AND INFILL STANDARDS
4.5: BUILDING TYPES

	SKETCH	PLAN	PERMITTED ZONING DISTRICT(S)
<p>G. Liner Building: This is a shallow structure, typically 25-30 feet deep, which is placed close to the street frontage. It is typically used in conjunction with other Liner Buildings to conceal surface or structured parking, or large-scale commercial buildings. These buildings may contain a variety of uses, including ground floor commercial, and upper-level offices or residential.</p>			T4, T5, RMX, IC
<p>H. Large Footprint Building: This is a building that has a footprint greater than 20,000 square feet. It is a type often used by big-box, national retailers. In the T4 and T5 transect zones, they shall be integrated into the streetscape or screened with Liner Buildings.</p>			T4, T5, RMX, IC, LI
<p>I. Structured Parking: This is a multi-level parking deck that may take up all or a significant portion of a block.</p>			T4, T5, RMX, IC, LI
<p>J. Gas/Fuel Station & Car Wash: This is a building type that contains a series of fuel pumps in conjunction with a convenience or retail store. Car washes feature pull-through lanes in which customers may or may not exit their vehicles.</p>			T5, RMX
<p>K. Drive-Thru Facilities: This is a commercial building type that includes a vehicular lane along a portion of the building to serve the public through a drive-thru window or other area in which customers do not leave their vehicles.</p>			T5, RMX

<p>L. Civic Buildings: This is a civic, institutional or religious building in which program, massing, and height vary. For religious buildings such as churches, heights and frontages are to be determined by the Code Administrator.</p>		<p>T3, T3-N, T4, T4-N, T4-HN, T5-UC, T5-DC, RMX, IC</p>
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4.5.1 GENERAL

- A. The transect provides a framework for determining where specific types of buildings can be located. This section gives an overview of the most common building types in Beaufort, and where and how they are permitted based on their transect zone. Some building types are permitted in multiple zones, but have different requirements based on their district. In case of a conflict between this section and Article 2, whichever requirement is stricter shall take precedence.
- B. This section is not intended to limit the variety of buildings or stifle creativity. Other building types may be permitted by the Design Review Body, as specified in Article 9, if the building meets the guidelines and requirements of this Article.

4.5.2 APPLICABILITY

- A. The requirements of the districts, as stated in Article 2, shall apply to all building types, unless a more stringent requirement is listed as part of this section.
- B. Some of the building types listed have specific uses implied (e.g., Gas/Fuel Station) while others may be used flexibly for multiple uses.

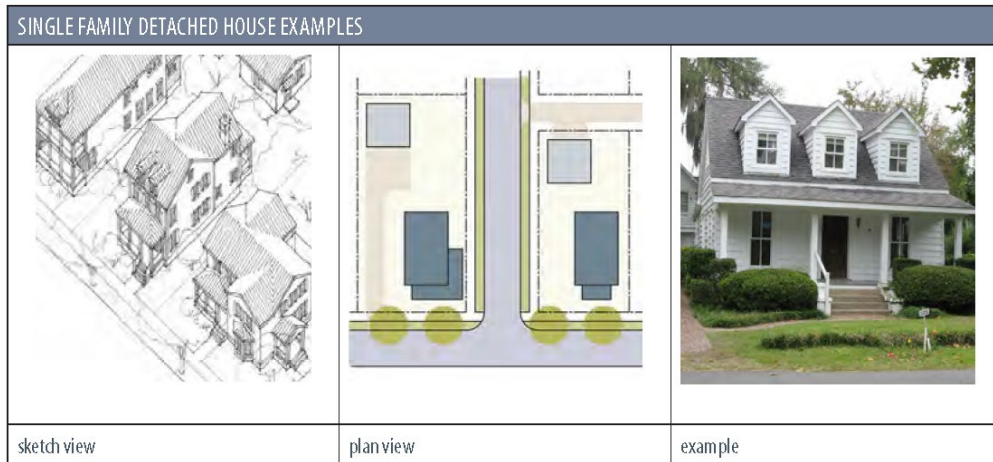
4.5.3 CARRIAGE HOUSE



- A. **Description:** This is an Accessory Structure that provides small, flexible living spaces adjacent to a main house (a.k.a. Accessory Dwelling Unit [ADU], Granny Flat). It is often used for rental housing, and may be free standing, or located above a garage or parking area.
- B. **Special Requirements:**

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1. **Infrastructure:** The lot shall be served with public water and sewer.
 2. **Number allowed:** 1per lot, an additional ADU may be allowed as a Special Exception (see Section 9.13)
 3. **Placement on the Lot:** The carriage house shall be located to the rear of the primary structure, or to the side as a secondary option, with the following exceptions:
 - a. Units may be placed at the front of a lot where the front of the primary structure is not the street, and the structure has clearly been designed to take advantage of unique site amenities, such as location on the water.
 - b. Units may be placed in the front of the lot where the prevailing character of the neighborhood has other similarly-placed units.
 4. **Frontage Type:** No frontage type is prescribed unless the building is close to the street; in which case, appropriate frontage types are: porch, stoop.
 5. **Maximum Number of Bedrooms:** 2.
 6. **Minimum Size:** 240 square feet in total area.
 7. **Maximum Size:** The footprint shall not exceed 50% of the footprint of the primary building, or 1,500 square feet, whichever is smaller. Conversions of existing accessory structures that exceed this maximum may be permitted if the administrator determines that there is no adverse impact on surrounding property.
 8. **Compatibility with Primary Structure:** Architectural details, including color, siding, roof pitch, window detailing, roofing materials, height, and foundation, shall be compatible with the primary dwelling unit.
 9. **Parking:** 1 parking space per Carriage House is required and shall be clearly defined. See Section 7.3 for additional parking standards.
 10. **Timing:** The carriage house(s) shall be permitted to be built prior to the primary structure on the lot if the following requirements are met:
 - a. A sketch plan showing the potential build-out, including parking, of the site is required;
 - b. The size(s) must be appropriate to permit a primary structure without exceeding the maximum lot coverage; and
 - c. Materials of the future primary structure must coordinate with the carriage house.

4.5.4 SINGLE FAMILY DETACHED HOUSE



- A. **Description:** This is the predominant residential building type in Beaufort. This house sits on a private lot and can vary in size from a small cottage to a large mansion. Setbacks and frontage types vary by Transect Zone.
- B. **Special Requirements:**
1. **Frontage Types:** The allowed frontage types are: Common Yard, Porch, Stoop.
 2. **Housing Model Diversity:** For any ***NEW*** development containing at least four single-family detached dwellings, there shall be at least two distinctly different building designs. For any such development containing more than eight separate single-family detached dwellings, there shall be at least three distinctly different building designs. For any such development containing more than sixteen separate single-family detached dwellings, there shall be at least four distinctly different building designs. The Administrator will determine the required amount of building designs for any development greater than 24 single-family structures. For all developments, there shall be no similar buildings placed next to each other along a street or street-like private drive. Building designs shall be considered similar unless they are distinguished by colors, massing elements of some combination of uniquely designed entryways, porches, windows and roof lines, such that they plainly loom different from one another. Square footage does not have to be different.
 3. **General Architectural Design.** All dwellings and accessory structures shall provide quality architectural design that takes into consideration building massing and style, roof lines, window and door placement, exterior materials and colors and other architectural features. Fluorescent or intense colors shall not be used on any wall or roof of any dwelling or accessory structure. Side or rear elevations shall not be a solid blank wall with no articulation; structures shall be designed so as to break up long walls by the use of offsets no less than 24" deep, shadow lines, façade treatments and the architectural theme on the front of the house shall be continued on the sides and rear of the structure to maintain the integrity of design throughout the structure.
 4. **Lowcountry Design Elements:** Single family detached houses shall reflect traditional Beaufort Lowcountry architectural elements as described in Section 4.3.2 and materials as described in Sections 4.6.3 and 4.6.4.
 5. **Elevation:** New construction shall be elevated at least eighteen inches in height above final grade
 6. **Corner Lots.** Facades on corner, end, or double frontage units visible from a public right-of-way, internal alley, or open space such as a park shall include architectural features including transparent fenestration,

frontage types such as porches or stoops, and other elements that match those found on the primary frontage. Blank facades with no articulation within the wall plane are prohibited.

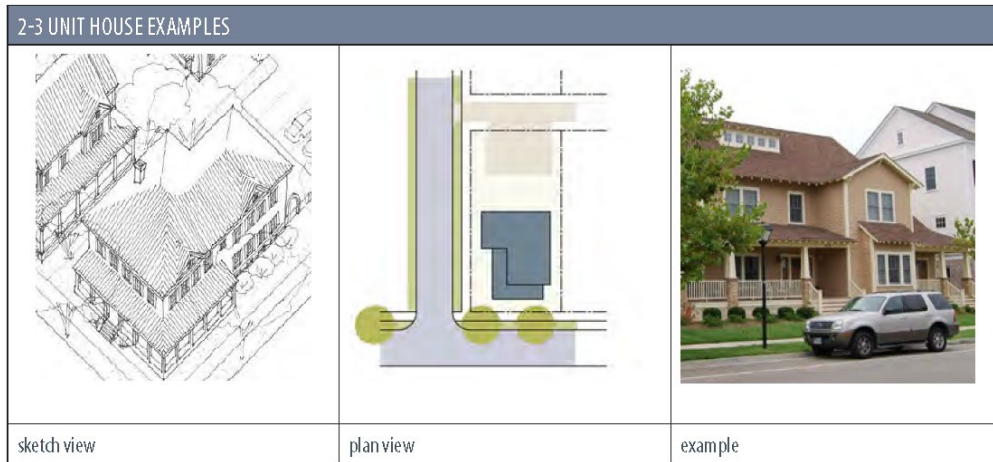
7. **Garages.**

- a. The preferred form of new single-family detached housing development is one whereby parking (apart from occasional on-street parking and including garages) is accessed from a rear alley. If an applicant can demonstrate to the satisfaction of the Code Administrator (or if not him/her, then the Planning Commission) that the relatively small, narrow or irregular size of a new subdivision development, and/or other configuration of the land, and/or environmental constraints, and/or location of abutting streets, and/or design of abutting subdivision development are such that rear, alley-fed parking, including garages, are impractical because of the resulting lot layouts and yields, then front-entry, non-alley-fed garages (but in no event front parking wholly apart from a garage) may be allowed.
- b. If front-entry garages are allowed under the above circumstances, such garages and the entryways to the house shall be designed with the following features:
 - i. If attached to the house, a front-entry garage shall be recessed a minimum of 5 feet from the front facade of the house. If entirely detached from the house or if accommodating more than 2 regular-sized (meaning non-golf cart) vehicles, it shall be located no closer to the front of the house than the rear wall of the house. Any portion of such garage accommodating more than 2 such vehicles shall also not be visible from the front street.
 - ii. Every front-entry, as opposed to rear, alley-accessed garage, shall have one garage door (not exceeding 12 feet in width) per vehicle, meaning double or triple sized garage doors are prohibited.
 - iii. Front-entry garage doors shall contain a minimum of two of these design features: Garage doors shall also utilize architectural elements such as elevated windows, a color other than white, and architectural elements (such as raised panels and applied trim) that are not incompatible with the design of the house.
 - iv. The front facade of the house shall be at least as wide as the front of the garage, and the entryway to the house shall be prominent to the house and the principal focal point of it.

8. **Front Yard Landscaping.** Front yard (or front and side yard if on a corner) landscaping shall visually integrate the adjacent natural landscape where applicable with the following standards:

- a. The entire front yard shall be landscaped, except for that portion covered by a paved driveway. Landscaping shall mean any combination of trees, shrubs, vines, ground cover, turf, natural grass, decorative gravel, stones, decomposed granite, mulch or other hardscape materials.
- b. Monoculture and/or plant palettes including five or less species are discouraged.
- c. Vegetation species for groundcover, (ornamental) grasses, shrubs, trees that are native to the South Carolina coast (Lowcountry) as well as regionally sourced or manufactured landscape construction materials, such as lumber, mulches, pavers, groundcover, quarried gravel/cobbles/stone or other hardscape materials, shall be used.
- d. Trees must be planted ten feet away from utility lines extending to the residence.
- e. Each front yard should have at least one ornamental tree. Front yard trees should be of at least two and one-half-inch caliper in size.

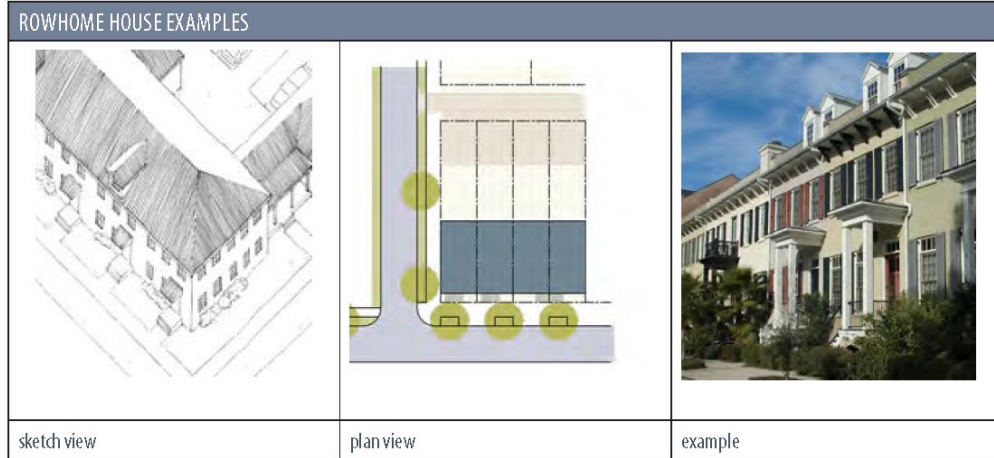
4.5.5 2-3 UNIT HOUSE



- A. **Description:** This house-form seamlessly fits into a predominantly single-family neighborhood but contains multiple dwelling units. The units may be side-by-side or stacked. They typically have separate entrances off of the street but may share a common entrance. They are located under one roof and do not have parapets dividing the units.
- B. **Special Requirements:**
1. **Frontage Types:** The allowed frontage types are: Common Yard, Porch, Stoop, Forecourt.
 2. **Parking:** Parking must be well defined, located behind the building, and accessed off a side street or rear alley. On-site parking for all vehicles, including boats, must not be in front of the building. No variances to this provision are permitted.
 3. **Location, Specific to T3-S:** A 2-3 unit building is permitted in the following areas:
 - a. On a corner lot; or
 - b. Where alley access is provided; however, no more than 2 per block are permitted.
 4. **Housing Model Diversity.** For any *NEW* development containing at least three duplexes or triplexes and not more than five buildings (excluding clubhouses/leasing offices), there shall be at least two distinctly different building designs. For any such development containing more than five separate duplexes or triplexes, there shall be at least three distinctly different building designs. For all developments, there shall be no similar buildings placed next to each other along a street or street-like private drive. Building designs shall be considered similar unless they are distinguished by colors, massing elements of some combination of uniquely designed entryways, porches, windows and roof lines, such that they plainly loom different from one another. Square footage does not have to be different.
 5. **General Architectural Design:** All dwellings and accessory structures shall provide quality architectural design that takes into consideration building massing and style, roof lines, window and door placement, exterior materials and colors and other architectural features. Fluorescent or intense colors shall not be used on any wall or roof of any dwelling or accessory structure. Side or rear elevations shall not be a solid blank wall with no articulation; structures shall be designed so as to break up long walls by the use of offsets no less than 24" deep, shadow lines, façade treatments and the architectural theme on the front of the house shall be continued

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6. **Lowcountry Design Elements:** 2-3 unit houses shall reflect traditional Beaufort Lowcountry architectural elements as described in Section 4.3.2 and materials as described in Sections 4.6.3 and 4.6.4.
 7. **Elevation:** New 2-3 unit houses shall be elevated at least 18" in height above final grade.
 8. **Corner Lots.** Facades on corner, end, or double frontage units visible from a public right-of-way, internal alley, or open space such as a park shall include architectural features including transparent fenestration, frontage types such as porches or stoops, and other elements that match those found on the primary frontage. Blank facades with no articulation within the wall plane are prohibited.
 9. **Garages**
 - a. Garages shall be placed behind the main building, or to the side of the building behind the main building's primary frontage if site constraints prohibit a rear-accessed garage. Side garage facades shall be placed a minimum of 5 feet behind the primary frontage wall of the main house.
 - b. Garages shall be accessed from a rear alley wherever possible. If a garage cannot be accessed from a rear alley, driveways along the side of a building are permitted, but the garage shall be placed so as to be hidden from the public right-of-way.
 - c. Garages may be attached to a building through a breezeway or small corridor. Attached garages that are incorporated into the massing of the main house are prohibited.
 - d. Garage doors shall not be visible from a public right-of-way wherever possible.
 - e. Side loaded garages are allowed a reduced 10 feet setback in T3-S districts.
 - f. Large garage doors that span wider than a single vehicular bay are prohibited. Single garage doors shall not be wider than 12 feet.
 - g. Garage doors shall also utilize architectural elements such as flat/raised panels composed of applied trim – Stamped metal, vision- lites, and faux wood grain textures are not allowed.
 - h. Refer to additional garage standards in Section 4.6.1.E.
 10. **Front Yard Landscaping:** Front yard (or front and side yard if on a corner) landscaping shall visually integrate the adjacent natural landscape where applicable with the following standards:
 - a. The entire front yard shall be landscaped, except for that portion covered by a paved driveway. Landscaping shall mean any combination of trees, shrubs, vines, ground cover, turf, natural grass, decorative gravel, stones, decomposed granite, mulch or other hardscape materials.
 - b. Monoculture and/or plant palettes including five or less species are discouraged.
 - c. Vegetation species for groundcover, (ornamental) grasses, shrubs, trees that are native to the South Carolina coast (Lowcountry) as well as regionally sourced or manufactured landscape construction materials, such as lumber, mulches, pavers, quarried gravel/cobbles/stone or other hardscape materials, shall be used.
 - d. Trees must be planted ten feet away from utility lines extending to the residence. *(canopy standard conflict?)
 - e. Each front yard should have at least one ornamental tree. Front yard trees should be of at least two and one-half-inch caliper in size (is this already addressed somewhere else?).

4.5.6 ROWHOME/TOWNHOUSE



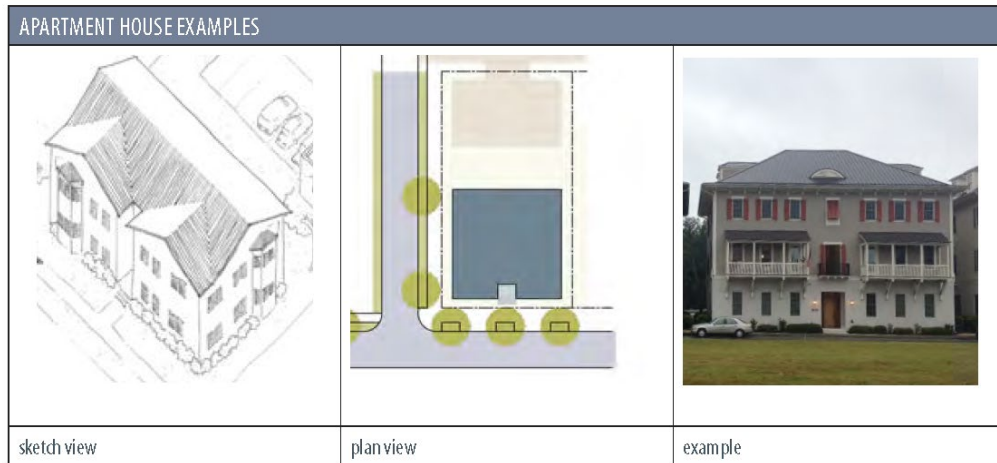
- A. **Description:** This is an attached residential building type that is part of a series of other rowhomes/townhouses with more than three units in a row. Three or fewer is considered a 2-3 unit building. Rowhomes/Townhouses may have parapet walls dividing the units or be combined under one roof form. They may also utilize the stoop or the forecourt and-porch frontage types.
- B. **Special Requirements:**
1. **Frontage Types:** The allowed frontage types are: Porch, stoop, forecourt.
 2. **Parking:** Parking must be well defined, located behind the building, and accessed off a side street or rear alley. On-site parking for all vehicles, including boats, must not be in front of the building.
 3. **Specific to T4:**

Rowhomes are not permitted in the Historic District, except in the Bladen Street Redevelopment District.
 4. **Housing Model Diversity.** For any development containing at least three rows of rowhomes/townhouses (i.e. each row of multiple townhouses/rowhomes is considered a building) and not more than five buildings (excluding clubhouses/leasing offices), there shall be at least two distinctly different building designs. For any such development containing more than five separate rows or townhouse buildings (excluding clubhouses/leasing offices), there shall be at least three distinctly different building designs. For all developments, there shall be no similar buildings placed next to each other along a street or street-like private drive. Building designs shall be considered similar unless they vary significantly in footprint size and shape. Building designs shall be further distinguished by including unique architectural elevations and unique entrance features, within a coordinated overall theme of roof forms, massing proportions and other characteristics. Such variation among buildings shall not consist solely of different combinations of the same building feature.
 5. **General Architectural Design.** All dwellings and accessory structures shall provide quality architectural design that takes into consideration building massing and style, roof lines, window and door placement, exterior materials and colors and other architectural features. Fluorescent or intense colors shall not be used on any wall or roof of any dwelling or accessory structure. Side or rear elevations shall not be a solid blank wall with no articulation; structures shall be designed so as to break up long walls by the use of offsets no less than 24" deep, shadow lines, façade treatments and the architectural theme on the front of the house shall be continued on the sides and rear of the structure to maintain the integrity of design throughout the structure.

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6. **Lowcountry Design Elements:** 2-3 unit houses shall reflect traditional Beaufort Lowcountry architectural elements as described in Section 4.3.2 and materials as described in Sections 4.6.3 and 4.6.4.
 7. **Elevation.** A duplex or triplex new construction shall be elevated at least 3 feet in height above grade. In areas with natural topography changes, the Design Review Body may allow an exception of 18 inches in elevation.
 8. **Corner Lots:** Facades on corner, end, or double frontage units visible from a public right-of-way, internal alley, or open space such as a park shall include architectural features including transparent fenestration, frontage types such as porches or stoops, and other elements that match those found on the primary frontage. Blank facades with no articulation within the wall plane are prohibited.
 9. **Garages.**
 - a. Garages shall be placed behind the rowhome/townhouse. Apart from occasional on-street parking, all parking, including garages and carports, if any in new rowhome/townhouse developments shall be located in the rear of the rowhomes/townhouses and shall be accessed off a rear alleyway.
 - b. If a garage cannot be accessed from a rear alley, driveways along the side of a building are permitted, but the garage shall be placed so as to be hidden from the public right-of-way.
 - c. Up to three detached garages may be attached to one another in a row along a rear alley. The length of the three attached garages shall not exceed 55 feet.
 - d. Garage doors shall not be visible from a public right-of-way or internal "street" wherever possible. Landscaping or appropriate buffering may be required by the Administrator or Planning Commission to accomplish this requirement.
 - e. Articulation. At a minimum, a vertical trim detail that subdivides the overall siding pattern shall be provided at intervals not to exceed two internal parking stalls (approximately 20-24 feet).
 - f. Rear Walls of Multi-family Garages. To add visual interest and avoid the effect of a long blank wall with no relation to human size, accessibility needs or internal divisions within the building, the following standards for minimum wall articulation shall apply:
 - (i) *Length.* Any garage located with its rear wall along the perimeter of a development and within 65 feet of a public right-of-way or the property line of the development site shall not exceed 55 feet in length. A minimum of seven feet of landscaping must be provided between any two such perimeter garages.
 - (ii) *Articulation.* No rear garage wall that faces a street or adjacent development shall exceed 30 feet in length without including at least one of the following in at least two locations:
 - a. Change in wall plane of at least six inches,
 - b. Change in material or masonry pattern,
 - c. Change in roof plane,
 - d. Windows,
 - e. Doorways,
 - f. False door or window openings defined by frames, sills and lintels, and/or

g. an equivalent vertical element that subdivides the wall into proportions related to human scale and/or the internal divisions within the building.

4.5.7 APARTMENT HOUSE



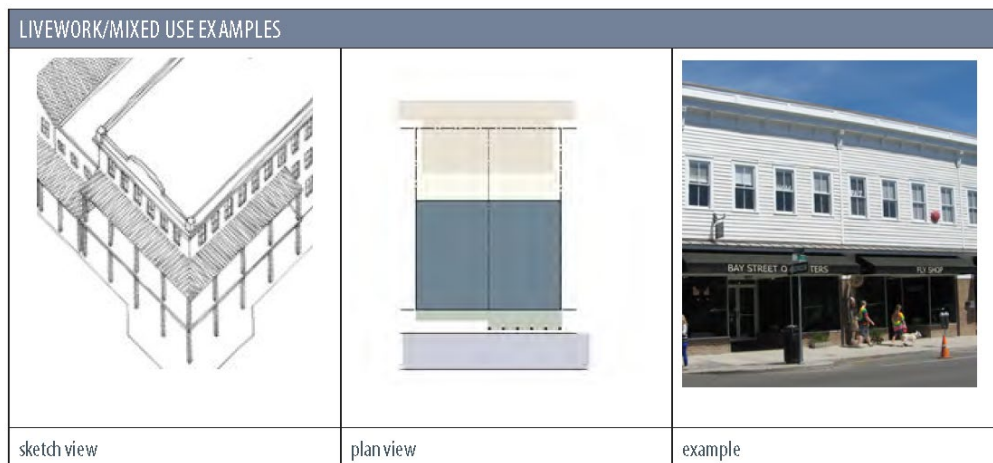
- A. **Description:** This is a larger-scale building (a.k.a. Multi-Family Building) that contains 4 or more dwelling units. They often occupy corner lots or are located in combination to create their own blocks. This building type applies to all new multi-family developments of 4 or more units, mixed-use buildings with residential units above or behind ground-floor commercial spaces.
- B. **Special Requirements:**
1. **Frontage Types--The allowed frontage types are:** Porch, Formal Entry Courtyard, Balcony.
 2. **Building Entries:** All ground floor units facing a frontage line, formal entry courtyard, or park/open space designated for use by the occupants shall have individual entrances to access those units. Specifically, those units facing a frontage line or internal park/open space shall utilize a porch, stoop, or awning frontage type.
 - a. Breezeways, internal corridors, or passageways that open to the street shall not be used as a frontage type, either in apartment homes with only residential units, or in apartment homes with a mix of commercial and residential spaces. In general, breezeways are strongly discouraged in apartment homes, even if only used as secondary circulation and not as a frontage type. External treatments such as garden walls, landscaping, benches and public seating, etc. shall not constitute a breezeway be interpreted as a forecourt or formal entry courtyard.
 - b. At least 70% of a building's frontage buildout façade shall utilize one of the frontage types mentioned above.
 2. **Specific to T4:**
 - a. In the Historic District, these are limited to 6 dwelling units per building.
 - b. Multi-family dwellings are not permitted in T4-NA.
 3. **Parking.** Head-in parking spaces along primary frontages (two primary frontages in the case of corner buildings) are prohibited. Parallel parking is permitted along these frontages. Any additional on-site parking shall be located behind the building and accessed off a rear alley. Detached garages

are permitted to the rear of an apartment building and access off a rear alley or internal parking lot. Refer to the garage requirements listed in this Section.

4. **General Architectural Design:** All dwellings and accessory structures shall provide quality architectural design that takes into consideration building massing and style, roof lines, window and door placement, exterior materials and colors and other architectural features. Fluorescent or intense colors shall not be used on any wall or roof of any dwelling or accessory structure. Side or rear elevations shall not be a solid blank wall with no articulation; structures shall be designed so as to break up long walls by the use of offsets no less than 24" deep, shadow lines, façade treatments and the architectural theme on the front of the building shall be continued on the sides and rear of the structure to maintain the integrity of design throughout the structure.
5. **Lowcountry Design Elements:** Apartment houses shall reflect traditional Beaufort Lowcountry architectural elements as described in Section 4.3.2 and materials and architectural standards as described in Sections 4.6.3 and 4.6.4. Furthermore, the intent and design goal of any apartment building within City of Beaufort limits shall address the items listed in this Section.
6. **Elevation:** New construction shall be elevated at least 3 feet in height above final grade. In areas with natural topography elevation changes, the Design Review Body may allow an exception of 18 inches in elevation.
7. **Height:** Buildings shall not exceed 3 stories in height at the street frontage. Where 4 stories are allowed, upper floors shall be set back from the primary façade plane so that the street elevation reads as 2-3 stories with a recessed upper story.
8. **Mixed-Use Program:** Ground floor commercial space or designated commercial buildings at a significant intersection within the site shall be required at designated public corridors at the discretion of the Design Review Body. Refer to the requirements listed in Section 4.5.8.
9. **Width:** The building width shall not exceed 100 feet along any frontage.
10. **House Module Approach:** Apartment home buildings shall reduce the apparent scale of multi-family buildings so they read as a collection of houses rather than a single large block.
 - a. Every building façade visible from a public right-of-way must be expressed as a set of discrete modules that read like individual houses, separated by varying forms, vertical breaks, offsets, and/or roof changes
 - b. Modules shall be between 18 feet-30 feet in width.
 - c. Buildings shall use varied roof forms such as front gables, cross gables, low-pitch hip roofs and small dormers to break up the overall massing of a building. Large roof forms such as hip roofs that encompass a whole building or uninterrupted flat parapet roofs are prohibited, unless broken into modules that support the house module approach as outlined in this section. The use of excessive hips, gables, shed, or parapet roofs with the singular goal of creating complex roof systems is strongly discouraged – the appropriate roof forms mentioned above should be used to create simple roof forms that do not detract from the appearance of the individual modules.
 - d. Primary roof ridgelines should generally be perpendicular or at shallow angles to the street to emphasize smaller forms.
 - e. No interrupted façade shall exceed 40 feet without a vertical modulation (offset, porch, material change, or recess).
 - f. For facades longer than 60 feet, there shall be at least 2 strong compositional changes (e.g., projecting gable, recessed courtyard, mid-bay break).
11. **Garages.**

- a. Garages shall be placed behind the apartment buildings. Garages or parking internal to the block shall be minimally visible from any public rights-of-way or internal “streets.”
- b. Garages shall be accessed from a rear alley wherever possible.
- c. Up to three detached garages may be attached to one another in a row along a rear alley or internal parking lot. The length of the three attached garages shall not exceed 55 feet.
- d. Garage doors shall not be visible from a public right-of-way or internal “street” wherever possible.
- e. Any garage wall visible from a public right-of-way shall include fenestration that matches the rhythm and detailing of the fenestration in the apartment building. Blank facades with no articulation or fenestration are not permitted where visible from a public right-of-way.
- f. Large garage doors that span wider than a single vehicular bay are prohibited. Single garage doors shall not be wider than 12’.
- g. Garage doors shall also utilize architectural elements such as flat/raised panels composed of applied trim – Stamped metal, vision-lites, and faux wood grain textures are not allowed.
- h. Refer to additional garage standards in Section 4.6.1.E.

4.5.8 LIVEWORK/MIXED-USE



- A. **Description:** This is a building that contains commercial space, typically at grade, with office or residential living, typically located on the upper level(s). They are typically attached but may be freestanding structures. The ground floor has a substantial amount of glazing and often utilizes the shopfront frontage type.
- B. **Special Requirements:**
 - 1. **Frontage Types:** The allowed frontage types are: Balcony, Shopfront/Awning, Gallery/Colonnade..
 - 2. **Parking:** Parking must be located behind the building and accessed off a rear alley.
 - 3. **Specific to T4-N and T4-NA:** When no retail frontage overlay exists, the ground floor is limited to office, artisan and trail-related uses only.
 - 4. **Specific to LI:** Drive-thru facilities are prohibited.

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5. **General Architectural Design:** All liner buildings shall provide quality architectural design that takes into consideration building massing and style, roof lines, window and door placement, exterior materials and colors and other architectural features. Fluorescent or intense colors shall not be used on any wall or roof of any dwelling or accessory structure. Side or rear elevations shall not be a solid blank wall with no articulation; structures shall be designed so as to break up long walls by the use of offsets no less than 24" deep, shadow lines, façade treatments and the architectural theme on the front of the building shall be continued on the sides and rear of the structure to maintain the integrity of design throughout the structure.
 6. **Lowcountry Design Elements:** Liner buildings shall reflect traditional Beaufort Lowcountry architectural elements as described in Section 4.3.2 and materials and architectural standards as described in Sections 4.6.3 and 4.6.4. Furthermore, the intent and design goal of any liner building within City of Beaufort limits shall address the items listed in this Section.
 7. **Mixed Use Program:** ~~5.~~**Ground-floor active uses are required** on designated mixed-use corridors. Where commercial is required, maintain residential direct entries adjacent or integrated — ground-floor residential entries cannot be completely separated from commercial frontage by service or parking.
 8. **Height:** Live-work/mixed use buildings shall be a minimum two stories tall, with at least one story of residential units above a ground floor commercial level. Commercial ground floors shall be a minimum of 12 feet tall, while upper units must be a minimum of 8 feet tall.
 9. **Depth:** Commercial ground floor units shall be a minimum 30 feet deep. Upper residential floors may be stepped back.
 10. **Width:** Commercial units shall be a minimum of 25 feet wide.
 11. **Live-Work/Mixed Use Special Architectural Requirements:**
 - a. Individual storefronts shall be visually identifiable as separate storefronts.
 - b. Residential entry points shall be separate from any commercial spaces; residents shall not be forced to enter units through commercial areas.
 - c. Ground floor commercial areas may have different frontage types than residential areas.
 - d. Commercial units shall be a minimum of 25 feet wide.
 - e. Any service areas shall be located to the rear of the building and shall not be visible from a public right-of-way.
 - f. Fenestration shall be arranged in an organized manner on all sides of the building. Storefronts or appropriately-sized single or ganged window are appropriate. Fenestration shall be transparent per the definition in Section 4.6.3.C.1 and 4.6.4.D.1.
 - g. Building entrances shall be clearly defined. Multiple entrances may be allowed if the building is on a corner or fronts to both a public right-of-way and internal parking.
 - h. Public frontage/pedestrian activation is strongly encouraged through the use of outdoor dining areas, wraparound porches or awnings, lighting, and/or large expanses of transparent fenestration. Porches/patios along public rights-of-way shall be a minimum of 12 feet in depth. These areas shall not encroach into any required sidewalks or street buffers.
 - i. The architectural treatment of the primary facade shall be continued, in its major features, around all visibly exposed sides of a building visible from adjacent residential and commercial zoned districts. Blank walls at side and/or rear elevations visible to the general public are prohibited adjacent to any residential or commercial zoned district unless the Codes Administrator or Design

Review Body determines there are adequate building or landscape features to conceal the view of the blank wall or service area.

- j. Walls shall not have an uninterrupted length exceeding 20 feet. Pilasters, transparent fenestration, stepping of the wall plane, and/or awnings or porches are required to satisfy this standard.
- k. Changes in massing are strongly encouraged. Box-like structures with no changes in façade planes are inappropriate, even if the design utilizes elements such as pilasters, transparent fenestration, or other elements mentioned in this Section. Long facades shall be broken into smaller, individually expressed modules reflective of traditional shopfront widths.

12. Wall articulation.

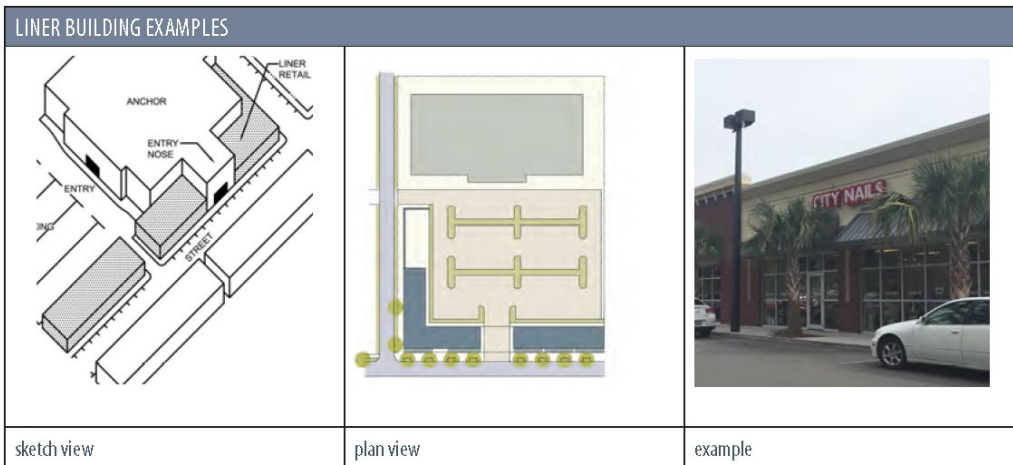
Walls shall not have an uninterrupted length exceeding 35 feet. Pilasters, texture transitions, windows, stepping of the wall plane at 24 inches, are required to satisfy this standard;

- a. Blank wall or service area treatment of side and/or rear elevations visible from to the general public is not allowed unless the Codes Administrator or Design Review Body determines there are adequate building features to conceal the view of the blank wall.
- b. All exterior elevations shall maintain the integrity of the adjacent dwellings architectural character and detailing.
- c. Continuous cornice lines or eaves are encouraged between adjacent buildings.
- d. Buildings with flat roofs shall provide a parapet with an articulated cornice.

13. Facade treatment. The architectural treatment of the front facade shall be continued, in its major features, around all visibly exposed sides of a building visible from adjacent residential and commercial zoned districts. Blank walls at side and/or rear elevations visible to the general public are prohibited adjacent to any residential or commercial zoned district.

14. Windows. Windows shall be vertically proportioned wherever possible and follow fenestration/transparency requirements found in this article.

4.5.9 LINER BUILDING



- A. **Description:** This is a shallow structure, typically 25—30 feet deep, which is placed close to the street frontage. It is typically used in conjunction with other Liner Buildings to conceal surface or structured parking, or large-scale commercial buildings. These buildings may contain a variety of uses, including ground floor commercial, and upper-level offices or residential. Liner buildings maintain a strong

frontage with minimal gaps between buildings to ensure visually coherent block face along any public rights-of-way.

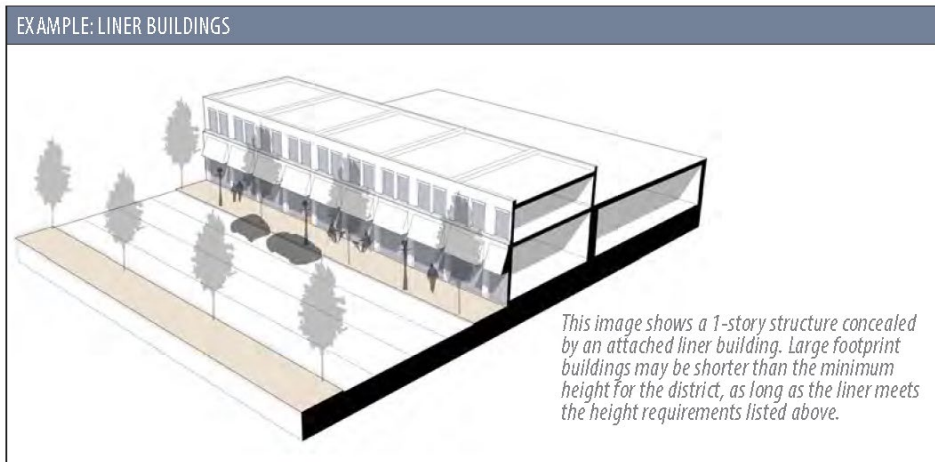
- B. **Requirements:** This is a shallow structure, 25—30 feet deep, which is placed close to the street frontage between the Large Footprint Building and the Primary Street frontage and required in the RMX, T-4 and T-5 UC districts. It is typically used in conjunction with other Liner Buildings to conceal surface or structured parking, or large-scale commercial buildings. These buildings may contain a variety of uses, including ground floor commercial, and upper-level offices or residential.
1. **Frontage Types:** Balcony, Shopfront/Awning, Gallery/Colonnade, Arcade.
 2. **Frontage Build-Out:** 75% minimum.
 3. **Parking:** Parking shall be located behind the building and accessed off an internal drive.
 4. **General Architectural Design:** All liner buildings shall provide quality architectural design that takes into consideration building massing and style, roof lines, window and door placement, exterior materials and colors and other architectural features. Fluorescent or intense colors shall not be used on any wall or roof of any dwelling or accessory structure. Side or rear elevations shall not be a solid blank wall with no articulation; structures shall be designed so as to break up long walls by the use of offsets no less than 24" deep, shadow lines, façade treatments and the architectural theme on the front of the building shall be continued on the sides and rear of the structure to maintain the integrity of design throughout the structure.
 6. **Lowcountry Design Elements:** Liner buildings shall reflect traditional Beaufort Lowcountry architectural elements as described in Section 4.3.2 and materials and architectural standards as described in Sections 4.6.3 and 4.6.4. Furthermore, the intent and design goal of any liner building within City of Beaufort limits shall address the items listed in this Section.
 7. **Height:** The minimum height for Liner Buildings is 16 feet or 1st story; however, when attached to another structure, it must be tall enough to conceal the building it is screening. Liner buildings shall be 2 stories tall if located within the area defined in Section 2.6.3.
 8. **Connection:** Liner Buildings may either be detached from or attached to the building they are lining.
 9. **Depth:** Detached Liner Buildings shall have a 25 foot minimum depth: Attached Buildings, shall have a 12 foot minimum depth, or shall present a unique design façade differentiating it from the building they are lining, which shall be approved by the approved Design Review Body.
 10. **Liner Building Special Architectural Requirements:**
 - a. Walls shall not have an uninterrupted length exceeding 20 feet. Pilasters,—transparent fenestration, stepping of the wall plane at 24 inches, and awnings or porches are required to satisfy this standard.
 - b. Lowcountry roof forms such as simple gable and hip roofs with overhanging eaves are strongly encouraged on liner buildings. Buildings with flat roofs shall provide a parapet with an articulated cornice. In the case of parapet walls, façade planes shall not have an uninterrupted length exceeding 15 feet. If flat roofs are to be used, the use of other roof forms are strongly encouraged.
 - c. Changes in massing are strongly encouraged. Box-like structures with no changes in façade planes are inappropriate, even if the design utilizes elements such as pilasters, transparent fenestration, or other elements mentioned in this Section. Long facades shall be broken into smaller, individually expressed modules reflective of traditional shopfront widths.

- d. Fenestration shall be arranged in an organized manner on all sides of the building. Storefronts or appropriately-sized single or ganged windows are appropriate. Fenestration shall be transparent per the definition in Section 4.6.3.C.1 and 4.6.4.D.1.
- e. Building entrances shall be clearly defined. Multiple entrances may be allowed if the building is on a corner or fronts to both a public right-of-way and internal parking.
- f. Public frontage/pedestrian activation is strongly encouraged through the use of outdoor dining areas, wraparound porches or awnings, lighting, and/or large expanses of transparent fenestration. Porches/patios along public rights-of-way shall be a minimum of 18'-24' in depth. These areas shall not encroach into any required sidewalks or street buffers.
- g. The architectural treatment of the primary facade shall be continued, in its major features, around all visibly exposed sides of a building visible from adjacent residential and commercial zoned districts. Blank walls at side and/or rear elevations visible to the general public are prohibited adjacent to any residential or commercial zoned district unless the Codes Administrator or Design Review Body determines there are adequate building or landscape features to conceal the view of the blank wall or service area.
- h. Corner Lots: In the case of corner units, any architectural elements applied to the main façade along the public right-of-way shall be continued onto the side elevation along the corner.

APPROPRIATE



INAPPROPRIATE



- 11. Facade treatment. The architectural treatment of the front facade shall be continued, in its major features, around all visibly exposed sides of a building visible from adjacent residential and commercial zoned districts. Blank walls at side and/or rear elevations visible to the general public are prohibited adjacent to any residential or commercial zoned district.
- 12. Windows. Windows shall be vertically proportioned wherever possible and follow fenestration/transparency requirements found in this article.

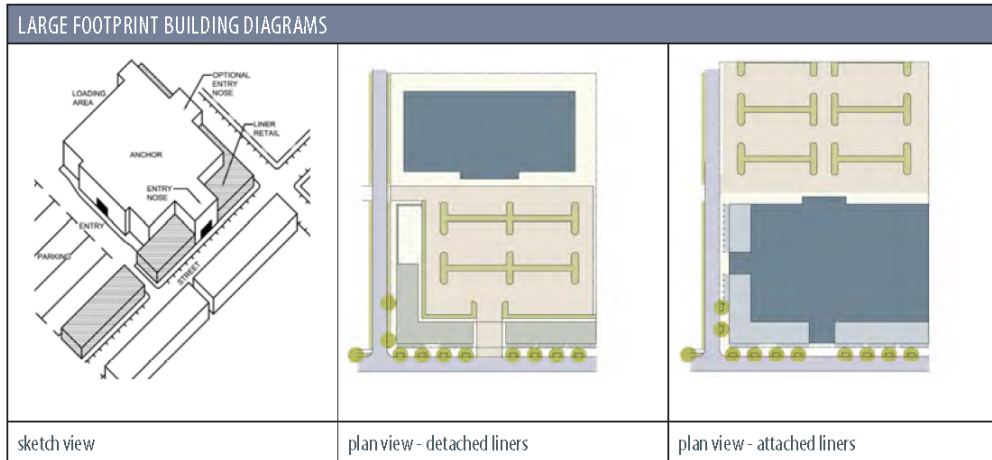
C. Alternative Frontage Build-Out with Patio

Where a minimum frontage build-out of seventy-five percent (75%) of the lot width is required, the Planning Commission or Administrator may permit a reduction in frontage build-out to **no less than sixty percent (60%) of the lot width** when the remaining frontage area is designed as an active pedestrian patio or courtyard integrated with the building frontage.

Such patio or courtyard shall meet the following requirements:

1. Minimum Dimension – The patio shall have a minimum depth of 12 feet measured from the build-to line.
2. Pedestrian Activation – The patio shall function as an extension of the adjacent building use and shall include features such as outdoor seating, café space, gathering areas, or similar pedestrian activity.
3. Direct Access - The patio shall be directly accessible from the public sidewalk.
4. Visibility and Transparency – The building façade facing the patio shall include entrances and transparent windows meeting the ground-floor transparency requirements of Chapter 4.
5. Street Openness - The patio shall remain visually open to the street and shall not be enclosed by opaque fencing or walls exceeding three (3) feet in height.
6. Prohibited Uses - The patio area shall not be used for vehicle parking, loading, trash storage, or service functions.
7. Architectural Integration - The patio shall be designed as an architectural extension of the building frontage through consistent materials, paving, lighting, and façade articulation. The patio shall include design features such as seating, planters, furnishings, or other programmatic elements consistent with pedestrian outdoor gathering spaces.

4.5.10 LARGE FOOTPRINT BUILDING

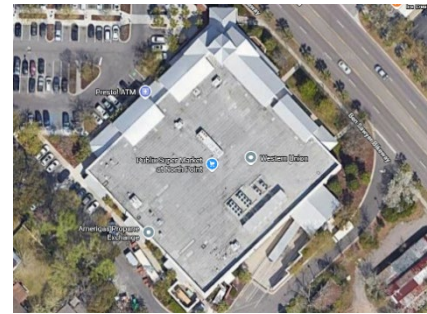


- A. **Description:** This is a building that has a footprint greater than 20,000 square feet. It is a type often used by big-box, national retailers. In the RMX, T4, and T5 transect zones, they shall be integrated into the streetscape or screened with Liner Buildings (see Section 4.5.9).
- B. **Special Requirements:**

-
1. **Frontage Types: The allowed frontage types are:** Shopfront/Awning, Gallery/Colonnade, —and/or Balconies may be used if there is true habitable space on the upper floor(s). A frontage type shall extend a minimum of 40% along the large footprint building’s primary frontages as well as any facades visible from ingress/egress lanes serving the development and internal parking areas that are used by customers.
 2. **Parking Location:**
 - a. **Specific to T4 and T5:** A minimum of 75% of all parking shall be located behind the primary building or a Liner Building.
 - b. **All other districts:** The majority of the parking spaces shall be located to the side or rear of the building, or behind a Liner Building.
 3. **Liner Buildings:** Liner Buildings (Section 4.5.9) are required in the RMX, T4 and T5 districts between the Large Footprint Building and the Primary Street frontage. They may be used in other districts to fulfill the requirements of this Code, such as height, entrances on the street, and parking location.
 - a. **Exceptions:** When a Large Footprint building provides an entry on the primary street frontage, and a minimum of 40% clear and unobstructed glazing along that street, liners are not required.
 4. **Height:** Buildings may be 1 story in height on the interior of the block but should be at least 2 stories in height along all Primary Street frontages (see Street Hierarchy Diagram in Appendix C). This may be accomplished with multi-story buildings, higher ceiling heights, parapets, and/or separate Liner Buildings. See Section 2.6.3 for more details on two-story building requirements.
 5. **Size:** Floor area of buildings shall not cantilever over public rights-of-way.
 6. **Entrances:** See Section 2.5.1 D.
 7. **Location:** In the Historic District, except along Boundary Street, these are permitted by special exception only.
 8. **General Architectural Design:** All large footprint buildings shall provide Lowcountry architectural design that takes into consideration building massing and style, roof lines, window and door placement, exterior materials and colors and other architectural features. Fluorescent or intense colors shall not be used on any wall or roof of any dwelling or accessory structure. Side or rear elevations shall not be a solid blank wall with no articulation; structures shall be designed so as to break up long walls by the use of offsets no less than 24” deep, shadow lines, façade treatments and the architectural theme on the front of the building shall be continued on the sides and rear of the structure to maintain the integrity of design throughout the structure.
 9. **Lowcountry Design Elements:** Large footprint buildings shall reflect traditional Beaufort Lowcountry architectural elements as described in Section 4.3.2 and materials and architectural standards as described in Sections 4.6.3 and 4.6.4. Furthermore, the intent and design goal of any large footprint building within City of Beaufort limits shall address the items listed in this Section.
 10. **Large Footprint Buildings Special Architectural Requirements:**
 - a. Walls shall not have an uninterrupted blank walls exceeding 135 feet. Pilasters, transparent fenestration, stepping of the wall plane, and/or awnings or porches are required to satisfy this standard.
 - b. Fenestration shall meet all requirements as listed in Section 4.6.3 or 4.6.4. Interior layouts shall not be a governing reason to inhibit the use of transparent fenestration. A minimum of 40% transparency is required on any internal façade that fronts a primary parking area or internal “street.”

c. Long expanses of wall on the same plane are not allowed. Walls shall change plane by at least 24" a minimum of every 30 feet to provide visual interest and avoid long, flat walls. These changes in wall plane shall also be carried to the roofline, where changes in roof forms are required.

d. The use of parapet walls and a flat roof are allowed on large footprint buildings. However, the outside perimeter of a large footprint building that is visible from a public right-of-way (including ingress/egress lanes serving the development and internal parking areas that are used by customers) shall include varied roof forms that are reflective of Beaufort's Lowcountry architecture. Parapet roofs are to be limited to rear service areas and/or the discretion of the Design Review Body. Parapets shall have an articulated cornice.



EXAMPLE: USE OF VARIED ROOF FORMS ALONG STREET FRONTAGE. ROOF PLAN SHOWING PERIMETER ROOF FORMS WITH INTERNAL FLAT ROOF.

- e. Blank wall or service area treatment of side and/or rear elevations visible from to the general public is not allowed unless the Codes Administrator or Design Review Body determines there are adequate building or landscape features to conceal the view of the blank wall.
- f. All exterior elevations shall maintain the integrity of the adjacent dwellings architectural character and detailing.
- g. Continuous cornice lines or eaves are encouraged between adjacent buildings.
- h. Fenestration shall be arranged in an organized manner on all sides of the building. Storefronts or appropriately-sized single or ganged window are appropriate. Fenestration shall be transparent per the definition in Section 4.6.3.C.1 and 4.6.4.D.1.
- i. The architectural treatment of the front facade shall be continued, in its major features, around all visibly exposed sides of a building visible from adjacent residential and commercial zoned districts. Blank walls at side and/or rear elevations visible to the general public are prohibited adjacent to any residential or commercial zoned district.

LARGE FOOTPRINT BUILDING EXAMPLE



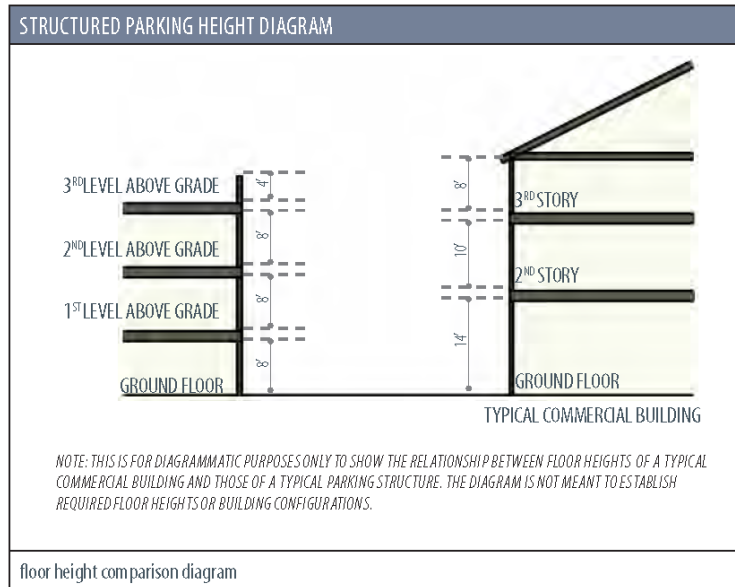
The large footprint building in the photo above is sited close to the sidewalks and incorporates frequent windows and doors. These techniques help to reinforce the urban character of the street and encourage use by pedestrians.

4.5.11 STRUCTURED PARKING

STRUCTURED PARKING DIAGRAMS		
sketch view	plan view - attached liners	example

- A. **Description:** This is a multi-level parking deck that may take up all or a significant portion of a block.
- B. **Applicability:** These standards apply to all above-ground parking structures that front a public street or right-of-way.
- C. **General Requirements:**
 1. **Screening Required:** Where an above-ground parking structure fronts a public street, the ground level shall be screened in such a way that cars are not visible from the street.
 2. **Materials:** Parking structure facades along pedestrian-oriented streets or primary pedestrian paths of travel shall be treated with high-quality materials, such as brick, cast stone, louvers, landscaped trellises, and/or crafted ornamental metal screens, to visually screen cars. The proportion should be broken down into bays that reflect the surrounding context.
 3. **Entries:** Pedestrian entries into the parking structure shall be clearly visible from the primary pedestrian paths of travel.

4. **Bicycle Parking:** Parking structures shall provide bicycle parking within the structure. It shall be located on the level closest to the street and/or a primary building entrance.
 5. **Liner Buildings:** If liners exist, they should be designed according to the standards set out in Subsection E. below.
- D. **Height:** Each above-ground level counts as 75% of a story regardless of its relationship to habitable stories.
1. **Specific to T5-DC:** The maximum height at the property line shall not exceed 35 feet to the top of the parapet.



E. **Special Requirements Specific to Transect Zones:**

1. **Disposition:** Parking structures shall meet the required building setbacks except where Liner Buildings are required. In that case, parking structures should be set back from the property lines of the adjacent Primary Street(s) a minimum of 25 feet to reserve room for Liner Buildings between parking structures and the lot frontage— see Street Hierarchy Diagram in Appendix C for Primary and Secondary streets.
2. **Frontage Types:** Shopfront/Awning, Gallery/Colonnade, Arcade — Balconies may be used if there is true habitable space on the upper floor(s).
3. **Liner Buildings:**
 - a. Liner Buildings (Section 4.5.9) are required in the T4 and T5 districts along Primary street frontage(s). Where parking structures face more than 1 street, requirements for Liner Buildings at secondary frontages are at the discretion of the Design Review Body. They may be used in other districts to fulfill the requirements of this Code, such as height and parking location.
 - b. Exceptions to this requirement may be granted when lot width or depth is less than 140 feet. Where no Liner Buildings exist, along pedestrian-oriented streets, standards in Section 4.5.11 C.1. shall apply.

- c. Liner Buildings may be constructed concurrently with the parking garage, or space may be reserved for their construction in the future.

4.5.12 GAS/FUEL STATIONS AND CAR WASHES



A. **Description:** This is a building type that contains a series of fuel pumps in conjunction with a convenience or retail store. Car washes feature pull-through lanes in which customers may or may not exit their vehicles.

B. Building and Fueling Station Requirements

a. Specific to T5-UC

- i. The number of pumps shall be limited to 3 — a maximum of six fueling positions — in the Historic District and 6 pumps elsewhere.
- ii. The fueling stations shall be located behind the convenience store or a Liner Building. Outside of the Historic District, the fueling stations may be located to the side of the convenience store or Liner Building with the following provisions:
 - 1. The building shall be located on a corner
 - 2. The pumps shall be set back a minimum of 10 feet from the front line of the building
 - 3. The short side of the pumps shall be parallel to the Primary Street — see Street Hierarchy Diagram in Appendix C
 - 4. The frontage build-out percentage shall be met by the convenience store and/or Liner Building
 - 5. Fueling stations shall not be located between the building and the adjacent street
- iii. One or more pedestrian entries shall be located along the Primary Street frontage.

C. General Architectural Design: All large footprint buildings shall provide quality architectural design that takes into consideration building massing and style, roof lines, window and door placement, exterior materials and colors and other architectural features. Fluorescent or intense colors shall not be used on any

wall or roof of any dwelling or accessory structure. Side or rear elevations shall not be a solid blank wall with no articulation; structures shall be designed so as to break up long walls by the use of offsets no less than 24 inches deep, shadow lines, façade treatments and the architectural theme on the front of the building shall be continued on the sides and rear of the structure to maintain the integrity of design throughout the structure.

- D. **Lowcountry Design Elements:** Large footprint buildings shall reflect traditional Beaufort Lowcountry architectural elements as described in Section 4.3.2 and materials and architectural standards as described in Sections 4.6.3 and 4.6.4. Furthermore, the intent and design goal of any large footprint building within City of Beaufort limits shall address the items listed in this Section.

E. **Site Planning & Orientation**

1. **Street Relationship:**

- a. Buildings must front the primary street, with a pedestrian entry oriented directly toward the sidewalk or public right-of-way.
- b. Automobiles shall not pass between the building and the street.
- c. Outdoor seating, small plazas, or shaded porches are encouraged to activate the street frontage.
- d. The bay door(s) to the garage or car wash bay(s) shall not be visible from the Primary Street frontage.
- e. For a stand-alone car wash not associated with a fuel station, the opening of the bay door shall either face away from the Primary Street or be screened from the street with a Liner Building (Section 4.5.9).

2. **Scale & Proportion:**

- f. Break down long façades with bays, porches, pilasters, or window groupings to create rhythm and human scale.
- g. Buildings should appear as a collection of smaller volumes, not a single large box.
- h. Use vertical proportions for windows and doors typical of Beaufort’s traditional buildings.

3. **Fenestration & Transparency**

Transparency Requirement:

- a. A minimum of 40% transparent fenestration is required on all pedestrian-facing façades.
- b. Transparency shall be measured as vision glass that allows direct interior visibility — not tinted, mirrored, or spandrel glass.

4.5.13 DRIVE-THRU FACILITIES

- A. **Description:** This is a commercial building type that includes a vehicular lane along a portion of the building to serve the public through a drive-thru window or other area in which customers do not leave their vehicles.
- B. **The allowed frontage types are:** Shopfront/Awning, Gallery/Colonnade, Arcade.

-
- C. **General Architectural Design:** All drive-thru facilities shall provide quality architectural design that takes into consideration building massing and style, roof lines, window and door placement, exterior materials and colors and other architectural features. Fluorescent or intense colors shall not be used on any wall or roof of any dwelling or accessory structure. Side or rear elevations shall not be a solid blank wall with no articulation; structures shall be designed so as to break up long walls by the use of offsets no less than 24 inches deep, shadow lines, façade treatments and the architectural theme on the front of the building shall be continued on the sides and rear of the structure to maintain the integrity of design throughout the structure.
- D. **Lowcountry Design Elements:** Drive-thru facilities shall reflect traditional Beaufort Lowcountry architectural elements as described in Section 4.3.2 and materials and architectural standards as described in Sections 4.6.3 and 4.6.4. Furthermore, the intent and design goal of any liner building within City of Beaufort limits shall address the items listed in this Section.
- E. **Height:** The minimum height for a Drive-Thru Facility is 16 feet. or 1 story. Drive-thru facilities shall be 2 stories tall if located within the area defined in Section 2.6.3. Upper floors must either be used as habitable space or be a lofted one-story space with the appearance of a two-story space from the exterior.
- F. **Drive-Thru Facilities Special Site Requirements:**
- a. Buildings must front the primary street frontage. A pedestrian entry point must be located on the primary street frontage with direct access from a sidewalk or public right-of-way, even if there are entrances points from another area such as an internal parking area.
 - b. Drive-thru lanes shall be located to the side or rear of the building.
 - c. In no instance shall an automobile pass between the building and the street, regardless of whether the automobile lane is for exiting the drive-thru area or not.
 - d. Circulation around all four sides of a building is prohibited.
 - e. Drive-thru windows and canopies shall be concealed from direct view of the street through architectural placement, fencing, or landscaping.
 - f. Queuing lanes shall be designed to minimize visual impact from public rights-of-way.
 - g. Adequate internal stacking lanes shall be provided to avoid spillover into public rights-of-way. A stacking study shall be made to substantiate the proposed stacking design.
 - h. A TIA is required as per Section 7.3.2.
 - i. Pedestrian walk-up windows are encouraged.
 - j. Public frontage/pedestrian activation is strongly encouraged through the use of outdoor dining areas, wraparound porches or awnings, lighting, and/or large expanses of transparent fenestration. Porches/patios along public rights-of-way shall be a minimum of 12' in depth. These areas shall not encroach into any required sidewalks or street buffers.
- G. **Drive-Thru Facilities Special Architectural Requirements:**
- a. Fenestration shall meet all requirements as listed in Section 4.6.3 or 4.6.4. Interior layouts shall not be a governing reason to inhibit the use of transparent fenestration. A minimum of 40% transparency is required on any internal façade that fronts a primary parking area or internal "street."
 - b. Per Section 4.3.2.P "Local Design Quality," architectural designs that are used to represent corporate identity are strongly discouraged within the City of Beaufort. Corporate identity must

be adapted to lowcountry vernacular. Designs should reflect local design principles as noted in Section 4.3.2, 4.5.13.D, and 4.5.12.E. Corporate logos and branding shall be expressed only through signage, not through building form, materials, or colors.

- c. Corporate neon or LED border lighting is prohibited.
- d. Walls shall not have an uninterrupted length exceeding 20 feet. Pilasters, transparent fenestration, stepping of the wall plane, and/or awnings or porches are required to satisfy this standard.
- e. All exterior elevations shall maintain the integrity of the adjacent dwellings architectural character and detailing.
- f. Continuous cornice lines or eaves are encouraged between adjacent buildings.
- g. Lowcountry roof forms such as simple gable and hip roofs with overhanging eaves are strongly encouraged on liner buildings. Buildings with flat roofs shall provide a parapet with an articulated cornice. In the case of parapet walls, façade planes shall not have an uninterrupted length exceeding 15 feet. If flat roofs are to be used, the use of other roof forms are strongly encouraged.
- h. Changes in massing are strongly encouraged. Box-like structures with no changes in façade planes are inappropriate, even if the design utilizes elements such as pilasters, transparent fenestration, or other elements mentioned in this Section. Long facades shall be broken into smaller, individually expressed modules reflective of traditional storefront widths.
- i. Fenestration shall be arranged in an organized manner on all sides of the building. Storefronts or appropriately-sized single or ganged window are appropriate. Fenestration shall be transparent per the definition in Section 4.6.3.C.1 and 4.6.4.D.1.
- j. Public frontage/pedestrian activation is strongly encouraged through the use of outdoor dining areas, wraparound porches or awnings, lighting, and/or large expanses of transparent fenestration. Porches/patios along public rights-of-way shall be a minimum of 12' in depth. These areas shall not encroach into any required sidewalks or street buffers.
- k. The architectural treatment of the primary facade shall be continued, in its major features, around all visibly exposed sides of a building visible from adjacent residential and commercial zoned districts. Blank walls at side and/or rear elevations visible to the general public are prohibited adjacent to any residential or commercial zoned district unless the Codes Administrator or Design Review Body determines there are adequate building or landscape features to conceal the view of the blank wall or service area.
- l. Corner Lots: In the case of corner units, any architectural elements applied to the main façade along the public right-of-way shall be continued onto the side elevation along the corner.

4.5.14 CIVIC BUILDINGS

- A. **Description:** Civic buildings contain uses of special public importance and may be located in any zoning district. They require special design standards as described below but should also be compatible with the surrounding context. Civic buildings may include, but are not limited to, municipal buildings, churches, libraries, schools, hospitals, and public recreation facilities. Museums may sometimes be considered civic buildings. Civic buildings do not include day care facilities, retail buildings, residential buildings, and privately-owned office buildings.
- B. **Placement:** In order to provide greater flexibility to create a special architectural statement, civic buildings are not subject to minimum or maximum setback requirements. Instead, building placement should be designed with consideration of the following guidelines.

1. Placement of civic buildings, depending upon program and site, can often benefit from being set back from the adjacent build-to lines of private development. This allows the scale of the building to have more visual emphasis and can create a public space in the foreground. The amount of this setback should be carefully determined, based on the urban design objectives of the particular site.
2. Buildings should be oriented toward the public realm (streets, squares and plazas) in a very deliberate way.
3. Civic buildings and their primary architectural elements should be placed at the termination of public vistas to provide an appropriate level of visual importance.

EXAMPLE: CIVIC BUILDING TERMINATING VISTA



4. Entrances should always be located on the most prominent facade(s). Avoid entrances that are at the rear or are visually concealed.
5. Parking shall be located at the rear and/or side of the building. In general, parking location standards shall match the zoning district.

B. Massing:

1. The primary massing of civic buildings should be symmetrical in form. The appearance of a balanced design increases the level of formality, which is appropriate to the public use.

EXAMPLE: CIVIC BUILDING SYMMETRY



2. Massing of civic buildings, although often larger as a whole, should be divided into visually distinct sections. Massing divisions should provide visual order to the building and create vertical proportions within individual elements.

EXAMPLE: CIVIC BUILDING MASSING VISUAL ORDER



C. **Scale/Height:**

1. The scale of civic buildings should be larger and more monumental than corresponding buildings in order to be more prominent and visible across greater distances.
2. Floor-to-floor heights and architectural details should be proportionately larger than those of private buildings that exist or are anticipated within adjacent blocks.

EXAMPLE: CIVIC SCALE AND FLOOR-TO-FLOOR HEIGHTS



3. Prominent roof forms and additive elements, such as cupolas, should be used to visually extend the height of the building.

EXAMPLE: ROOF FORMS AND ELEMENTS THAT DISTINGUISH CIVIC BUILDINGS



D. Materials/Details:

1. Civic buildings should evoke a civic character and be carefully designed to reflect the architectural character of Beaufort.
2. Civic buildings should be made of durable, high-quality materials that create a sense of permanence and lend civic identity to the city. Preferred materials include brick, stone, and cast concrete. Stucco should only be utilized when applied over structural CMU.
3. Building details should be designed at 2 scales. At the larger scale, details should be robust, so as to be read from a distance. Nearer to the building, the details of the lower levels should include another measure of refinement that can only be seen up-close at a pedestrian scale.
4. Building design elements should be used which allow civic buildings to act as focal points of the community. Depending on the architectural style of the building, the following elements should be considered in the building design:
 - a. Pronounced window lintels/sills/mullions, etc.
 - b. Columns with a capital and base.
 - c. A water table constructed of high-quality masonry units (such as cast-in-place stone) that extends beyond the face of the facade.
 - d. Vertically oriented windows of at least a 2:1 ratio.
 - e. Cornice lines with significant depth and multiple levels of relief.
 - f. A monumental raised entryway.
 - g. A formal landscaped area or plaza.
 - h. A tower element with a turret, cupola, or similar treatment.

E. Special Requirements for Church and Campus Style Uses:

1. Frontage Types: Gallery/Colonnade, Porch. Frontage defined at the

2. Height: Building height may vary depending on the program needed, such as the use of upper balconies, tall ceiling heights, use of steeples/cupolas, etc.

3. Entrances: Building entrances shall be clearly defined and articulated. More than one entrance is permitted from multiple facades.

4. Street Orientation:

- a. Locate the principal façade and main entrance facing the public street or green, reflecting Beaufort’s historic civic patterns.
- b. Buildings should be sited close to the street, with front setbacks consistent with neighboring structures and landscaped forecourts or gathering lawns.

5. Pedestrian Connections:

- a. Provide direct pedestrian paths from the public sidewalk to the main entry.
- b. Parking areas shall be located to the side or rear and connected by shaded walkways.

6. Campus Form (for larger sites):

- a. For multi-building church campuses, organize buildings around courtyards or greens, not parking lots.
- b. Use tree canopies, garden walls, and covered walkways to unify the site and create shade.
- c. Service and vehicular areas should be visually screened and subordinate to pedestrian spaces.

7. Outbuildings:

- a. Parish halls, classrooms, or offices should reflect subordinate building forms—smaller scale, simpler rooflines, and consistent materials.

8. Architectural Character (Churches and Campus Style Buildings)

a. Form & Massing:

- i. Draw from Lowcountry vernacular precedents: simple rectangular plans, steeply pitched gable or hip roofs, and symmetrical façades.
- ii. Emphasize vertical proportion and rhythm—tall windows, doors, and ventilating louvers that express grace and lightness.
- iii. For prominent worship spaces, a central gable, steeple, or cupola may be used as an accent, not as a dominating tower.
- iv. Avoid over-scaled domes or monumental porticoes inconsistent with the city’s intimate scale.

b. Porches & Entries:

- i. Incorporate deep porches or porticos with slender columns, wood posts, or brick piers—echoing Beaufort’s tradition of shaded transitions between indoors and outdoors.
- ii. Entry steps and landings should be generous and welcoming, connecting seamlessly with sidewalks and gathering lawns.

c. Roof Forms:

- i. Preferred roofs: gable, hip, or cross-gable with moderate to steep pitch (6:12–12:12).
- ii. Use standing seam metal or architectural shingles in muted, coastal tones.
- iii. Broad eaves with exposed rafter tails or simple trim are encouraged.

d. Proportion & Rhythm:

- i. Articulate facades with bays, pilasters, or window groupings that reflect internal organization.
- ii. Maintain vertical emphasis; avoid long horizontal bands or large unbroken walls.
- iii. Larger sanctuaries should step down in height toward adjacent streets or neighborhoods to soften scale.

e. Fenestration & Transparency

1. Window Design:

- i. Use vertically proportioned, operable windows with divided lights, wood or wood-look frames, and trim of appropriate depth.
- ii. Window groupings may express the sacred interior through tall, narrow proportions or paired openings.
- iii. Clerestory or transom glazing encouraged for natural light and ventilation.

2. Transparency & Detailing:

- i. Ground-level façades facing public streets should maintain a minimum of 25% transparency to create visual connection and safety.
- ii. Clear glass preferred; lightly tinted acceptable. Avoid mirrored or opaque glass. Stained glass may be acceptable at the discretion of staff/board members.

f. Materials & Craftsmanship

1. Primary Materials:

- i. Painted wood siding, smooth fiber cement, traditional brick, or lime stucco.
- ii. Heavy timber, wood brackets, and hand-crafted detailing encouraged for authenticity.

2. Secondary Materials:

- i. Metal roofs and wood shutters typical of Lowcountry precedent.
- ii. Stone or precast limited to accent elements (sills, thresholds, steps).

g. Color Palette:

- i. Soft, coastal tones: whites, grays, creams, pale yellows, and greens.
- ii. Trim and shutters may contrast slightly for depth and expression.

h. Civic Expression

1. Steeples & Cupolas:

- i. Allowed where scaled to the building mass and used as focal points rather than monuments.
- ii. Details should be crafted and proportional, not prefabricated or exaggerated.
- c. Belfries or cupolas may serve as ventilation towers or daylight monitors.

2. Bell Towers & Crosses:

- i. Integrate into the building form, not as detached sculptural elements.
- ii. Lighting of crosses or towers shall be subdued, downward, and non-glaring.

3. Entrances & Gathering Spaces:

- i. Forecourts, gardens, or porches shall function as publicly inviting civic thresholds.
- ii. Integrate shade trees, benches, and low brick or tabby walls for enclosure and comfort.

i. Landscape & Site Features

1. Lowcountry Landscape Character:

- i. Emphasize native trees (live oak, magnolia, palmetto), shaded walks, and pervious surfaces.
- ii. Preserve existing canopy trees where possible.
- iii. Use lawn panels, tabby walls, and native plantings to define outdoor gathering spaces.

j. Parking & Circulation:

- i. Parking areas should be broken into smaller courts framed by trees and low walls.
- ii. Encourage shared parking agreements with adjacent uses to reduce impervious surfaces.

4.6: BUILDING DESIGN STANDARDS

4.6.1 GENERAL TO ALL APPLICABLE DISTRICTS (SEE 4.2.2)

- A. **Size, Mass and Scale:** The scale of buildings and accessory structures (including canopies) shall be appropriate to the scale of structures located in the neighboring context.
 - 1. Architectural elements intended solely to simulate an additional story without providing meaningful building depth, usable space, or architectural articulation shall not be used to satisfy the two-story requirement.
- B. **Proportions of Bays and Openings:** All building bays, including porches, colonnades and porticos as appropriate, shall be square or vertical in proportion, unless the design merits a horizontally-proportioned opening. Wall openings, with the exception of transoms and storefronts, shall be square or vertical in proportion. The facade of a proposed building should draw upon the proportion and number of bays in surrounding buildings, as defined by windows, doors, and column spacing, to establish a compatible scale.
- C. **Building Materials:** Building wall materials shall be combined on each facade only horizontally, unless the building is broken vertically by a change of plane, or a vertical architectural element. Heavier materials, such as masonry and stucco, shall be located below lighter materials, such as cement fiber or wood siding.
- D. **Roof Forms:** Where pitched roofs exist, primary roofs shall have a minimum slope of 4:12, while ancillary roof slopes may be no less than 2:12.
- E. **Garages and Accessory Structures:** Attached garages, detached garages, and other accessory structures shall be subordinate in height, footprint, and proportion to the primary structure on the site, and shall

be compatible with the principal structure in terms of roof form, materials, and color. Where a garage contains an accessory dwelling unit above, it may exceed the height of the primary structure only if the primary structure is one story tall and the accessory structure is not visible from a public right-of-way, or is placed as far away as possible in the case of a corner lot.

F. Trash, Mechanical, Utility and Service Equipment and Areas:

1. Loading docks, service areas, and trash disposal facilities (e.g., trash and recycling receptacles, compactors, dumpsters) shall be hidden or screened from view of streets, parks, squares, waterways, or significant pedestrian spaces. Loading areas may be oriented toward adjoining developed properties that are zoned for nonresidential uses, if such loading areas are screened from view. See Section 2.7.1 E. for specific provisions about these areas in the Historic District.
2. Mechanical equipment, such as ventilation systems, commercial exhaust fans, rooftop terminations, commercial cooling equipment, heating and air conditioning units, TV antennas, and satellite dishes, shall be hidden or screened from view. Lattice, open brick enclosures, or vegetation can be used to conceal mechanical equipment. Screening material shall be properly maintained. If vegetation is used for screening, the mature size of the vegetation shall be considered so that equipment air flow will not be compromised.

- G. Colors:** Buildings and additional site elements — e.g., bollards, arbors, drive-thru canopies, sign posts — shall not be painted in a color or pattern that expresses corporate identity, to the extent that the paint color or pattern is not consistent with the design standards of the district. Fluorescent colors, such as ASHTO safety yellow, are also prohibited except where required by a public agency. Building elements shall use soft, muted palettes (pastels, neutrals, weathered wood tones) with a maximum of three primary colors per façade. For all developments, there shall be no more than two similarly colored structures placed next to each other along a street or major walkway. *CLARIFY?*

H. Outdoor Display of Merchandise:

1. **Definition of "Merchandise":** For purposes of this section, merchandise is defined as any item that is for sale on the premises or is representative of an item that is for sale on the premises, regardless of whether or not that particular item is available for purchase.
2. **Arrangement, Location, and Type of Merchandise:**
 - a. **Arrangement:** Merchandise shall only be located on the property and not on a public right-of-way, and be arranged and spaced so as not to clutter the front of the property, as determined by the Administrator.
 - b. **Outdoor Merchandise:** Except in T5-DC per Section 4.6.1 H.2.d., automobiles, trucks, boats, trailers, outdoor landscape structures (garden sheds, arbors, gazebos, etc.), plant materials, agricultural products, lawn maintenance equipment, and outdoor furniture may be displayed outdoors per the provisions of this section. Outdoor merchandise may remain outdoors when the business is closed.
 - c. **Indoor Merchandise:** Merchandise other than that typically used and stored outdoors may be displayed outdoors within a maximum of 5 feet from, and directly in front of building or the tenant space. Indoor merchandise shall only be displayed during business hours.
 - d. **Specific to T5-DC:**
 - i. **Display Hours:** All merchandise shall be displayed outdoors during business hours only.
 - ii. **Location:** All merchandise displayed outdoors shall be within 5 feet of the building and only in front of the building or the tenant space.

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3. **Location:**
 - a. Merchandise shall not be placed on the public sidewalk or within the right-of-way without approval of the Administrator. If merchandise is displayed on any privately owned sidewalk, a minimum of 4 feet of the sidewalk as measured from the curb must remain open and unobstructed to facilitate safe pedestrian circulation.
 - b. Areas designated for vehicular parking may not be used as outdoor display areas.
 - c. Merchandise shall not be displayed on public street furniture or within public landscaping.
 4. **Designation in Plans:** plans for new developments or reuse of an existing space shall clearly designate any areas for outdoor display of outdoor merchandise. Outdoor display of merchandise shall only occur in areas designated for such display on the approved plan.
 5. **Vending Machines and Realty Advertising Racks:** Vending machines, except newspaper boxes, shall be screened from view from the street. Realty advertising racks, and other street furniture visible from the public right-of-way shall not display any advertising or other commercial message, except where such advertising or commercial message is counted as a sign under Article 6 (Signs). These racks shall be constructed of wood or metal.
 6. **Gifts to the Street:** Items such as benches, planters, or other decorative elements that enhance a shopfront, may be displayed so as not to clutter the street, and as long as they meet the clearance requirements of merchandise listed above. These elements must be properly maintained and may not contain advertising of any kind.
 7. **Screening Requirements:** Outdoor merchandise may be displayed on the site behind buffers meeting the requirements the Required Buffer Width and Planting Chart in Section 5.5.1.

4.6.2 T1 DISTRICT STANDARDS

There are no specific standards for this district. The Administrator will determine whether the proposed development is compatible with the surrounding context and may choose to permit the appropriate Design Review Body to review the project. In the Historic District, any development in T1 districts shall be reviewed by the Historic Review Board.

4.6.3 SPECIFIC TO TRANSECT-BASED DISTRICTS (SEE 4.2.2)

A. **Materials.** In Beaufort, building materials should be chosen for durability in the Lowcountry's humid, coastal climate, while also reflecting the area's historic aesthetic. Beaufort's architectural standards emphasize using materials that reflect the Lowcountry, with a preference for quality, authenticity, and durability.

1. Building walls shall be finished in 1 or more (but not more than 3) of the following materials:
 - a. Wood.
Styles: Traditional lap, shiplap, beaded lap, beveled, vertical board and batten
 - b. Fiber cement board siding,
 - c. Concrete masonry units with stucco (CBS).
 - d. Reinforced concrete with stucco.
 - e. Brick.
 - f. Tabby (or stucco with oyster shell aggregate, typ.).

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- gf. Other materials as approved by the Administrator, based on visual compatibility with listed approved materials.
2. Visible foundation walls and chimneys shall be finished in one of the following materials:
 - a. Brick.
 - b. Stucco.
 - c. Tabby.
 3. Columns may be constructed of the following materials and shall be sized proportionately to the building:
 - a. Wood.
 - b. Wood or fiber cement clad.
 - c. Cast iron.
 - d. Concrete with smooth stucco finish.
 - e. Cast stone with smooth finish.
 - f. Brick.
 - g. *Perma-cast.
 - h. *Other synthetic materials as approved by the Administrator, based on visual compatibility with listed approved materials.
 4. Railings and balustrades may be constructed of the following materials:
 - a. Painted wood.
 - b. Wrought iron.
 - c. *Aluminum.
 - e. *Other synthetic material as approved by the Administrator, based on visual compatibility with listed approved materials.
 5. Trim:
 - a. Wood
 - b. Composition board
 - c. Fiber cement board
 6. Doors shall be made of wood, metal, glass, or fiberglass*. Refer to transparency definitions in Section 4.6.3.C.1 regarding allowable glazing treatments.
 8. Roofs with visible pitches shall be constructed of the following materials:
 - a. 25-year architectural dimension shingles.
 - b. Tile (clay, cement, natural or manufactured stone).
 - c. Nonreflective pre-finished metal, such as standing seam or 5V.
 - d. Slate.
 - e. Reflective metal such as copper.

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- f. Other similar metals as approved by the Administrator.
9. The following materials are prohibited:
- a. Exposed concrete masonry units.
 - b. Vinyl Siding.
 - c. Exterior Insulation and Finish System (EIFS)
 - d. Continuous perforated soffit panels.
 - e. Plastic shutters.
 - f. Stone.
 - g. Metal panels.
 - j. Day-glo, luminescent, iridescent, neon or similar types of color finishes.
 - k. Mirrored glass with a reflectivity of 20% or more.
 - l. Roofing.
 - i. Plastic or PVC roof tile.
 - ii. Nontraditional colors such as orange or purple.
10. **Exceptions:** In the Historic District, materials indicated with an asterisk (*) may be permitted, on a context-specific basis, at the discretion of the Design Review Body. Certain materials may also not be allowed in the Historic District based on existing elements (e.g. composite columns may not be permitted as a replacement for historic wood columns), and are approved or denied on a context-specific basis at the discretion of the Design Review Body.

B. Roof Forms:

1. Specific to zones T4 and T5:

- a. Permitted roof types include gabled, hipped, shed, barrel vaulted, flat, mono-pitch, and domed. Applied and partial (less than 3 sides) mansard roofs are not typically permitted but may be permitted at the discretion of the Design Review Body based on compatibility with the surrounding context, size, mass and scale of the structure.
- b. Shed, flat, and mono-pitch roofs shall be concealed with parapets along the street frontage, except on porches, balconies, or building extensions.
- c. Downspouts and gutters should be galvanized steel, aluminum, or copper and shall match in materials and finish.
- d. Roof penetrations shall be hidden or painted to match the color of the roof.
- e. Skylights must be flat to the pitch of the roof. In the Historic District and T5 districts, they shall not be located on any sloped roof facing the primary frontage.

C. Windows and Doors:

1. **Fenestration/Transparency:** With the exception of residential uses, glass on a window of any type (including storefront) or door of any type that has no coatings (such as frosted, tinted or spandrel glass) or treatments, so that the building's interior is visible from the outside. Transparent fenestration shall mean clear or lightly tinted vision glass with a minimum visible light transmittance (VT) of 0.60 that provides direct, unobstructed views into an active interior space at least three feet deep from the interior face of glass. Opaque spandrel panels, reflective glass, back-

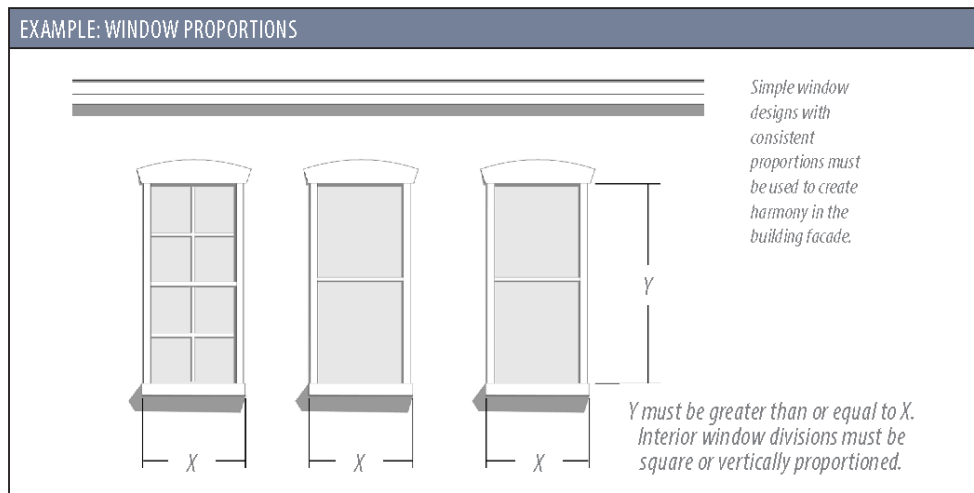
Painted glass, or any glazing with a solid wall, shelving, or storage directly behind it shall not be considered transparent fenestration. Low-E coatings suitable for Beaufort’s Lowcountry climate are considered appropriate. The Planning Commission may consider design exceptions of this requirement to include spandrel glass and/or higher percentage opacity in cases where the applicant proves these are critical functional security areas.

2. **Specific to zones T4 and T5:** Any commercial or mixed-use building elevation facing a street, waterway, or public space shall include a minimum of 40% of transparent fenestration (windows and doors) on the ground floor and 20% on upper floors. Apartment Houses shall conform to these requirements as much as possible, however the minimum percentage of transparent fenestration on the ground floor may be adjusted at the discretion of the Design Review Body. It shall not be reduced to lower than 30%.



3. **Proportion and Details:**
 - a. Windows shall not be flush with exterior wall treatments. Windows shall be provided with an architectural surround at the jambs and header, and a projecting sill.
 - b. Window openings may be grouped horizontally, but trim between windows shall be at least 3.5 inches wide. This does not apply to storefront windows.
 - c. Faux windows/openings are prohibited in any district.
 - d. There must be visual clearance behind the glass for a minimum of six feet. Three-dimensional display windows at least six feet deep are permitted and may be counted toward the transparent glass requirements.
 - e. Ground floor windows of commercial uses shall be kept clear at night, free from any window covering, with internal illumination. When ground floor glass conflicts with the internal function of the building, other means shall be used to activate the sidewalk, such as, display windows, public art, architectural ornamentation or detailing or other similar treatment.
 - f. **Specific to zones RMX, T4, T4-HN, and T5:**

- i. Facades should have several window sizes with smaller ones above. This is particularly important when buildings rise more than 2 stories.
- ii. Thin mullions or muntins shall be required on windows larger than 2 feet in any direction, except for shopfronts. The depth of the mullion shall not be less than the width.
- iii. In the historic district, windows shall have exterior-applied muntins and interior spacer bars (SDL) or be true divided lite (TDL) windows.
- iii. Windows outside of the historic district may be required to have exterior-applied muntins and interior spacer bars (SDL) if visible from significant corridors, to be determined at the discretion of the Design Review Body.



D. Architectural Details:

1. **Shutters (specific to zones RMX, IC, T4 and T5):** Shutters shall be sized and placed so as to equal the width that would be required to cover the window opening. Shutters shall be operable and include all necessary hardware. Bahama shutters are strongly discouraged. The use/language of shutters shall be consistent on all facades and floors.
2. **Column Bays:** Columns and piers shall be spaced no farther apart than they are tall. Column bays shall be of equal and precise proportions.
3. **Specific to zones RMX, T4 and T5:**
 - a. Cornices are required to delineate the tops of facades. Expression lines are required to delineate the divisions between the first floor and upper floors. Cornices and expression lines should either be a molding extending a minimum of 2 inches, or a jog in the surface plane of the building wall greater than 2 inches. Cornice or eave height shall be consistent with the dominant cornice or eave height of buildings on the same block.
 - b. The elevation of the first floor and floor-to-floor heights shall be compatible with the expression of floors in the facades buildings on the same block.

EXAMPLE: CORNICES



Cornices and expression lines articulate the building facade and break up the massing of larger buildings.

- c. Buildings shall have a base, wherein the bottom is articulated differently from the rest of the building, either by change of material or a setback. Material and craftsmanship on the base shall be as or more durable and of equal or higher quality than the rest above. For single-family detached dwellings, the base consists of the porch and/or primary entry.

4.6.4 SPECIFIC TO CONVENTIONAL DISTRICTS (SEE SECTION 4.2.2)

A. **Building Materials and Details:**

1. Building walls shall be finished in 1 or more (but not more than 3) of the following materials:
 - a. Wood, painted or natural.
 - b. Fiber cement board siding, smooth finish.
 - c. Concrete masonry units with stucco (CBS).
 - d. Reinforced concrete with stucco.
 - e. Brick.
 - f. Tabby (or stucco with oyster shell aggregate, typ.).
 - g. Corrugated metal.
 - h. Other compatible and authentic materials as approved by the Administrator.
2. **Specific to LI District:** Building walls not visible from a public right-of-way may be finished in metal or exposed CMU block, in addition to the materials listed above.
3. Visible foundation walls and chimneys shall be finished in 1 of the following materials:
 - a. Brick.
 - b. Stucco.
 - c. Tabby.
 - d. Specific to LI District: Painted CMU block is permitted.

B. **Roof Forms, Specific to RMX and IC Districts:**

1. In general, a slope of 4:12-12:12 is appropriate for primary roofs, while ancillary roof slopes may be 3:12-5:12.

2. Permitted roof types include gabled, hipped, shed, barrel vaulted, flat, mono-pitch, mansard, and domed. Applied and partial (less than 3 sides) mansard roofs are not typically permitted but may be at the discretion of the Design Review Body based on compatibility with the surrounding context, size, mass and scale of the structure.
3. Shed, flat, and mono-pitch roofs shall be concealed with parapets along the street frontage.
4. **Specific to IC District:**
 - a. Downspouts and gutters are to be galvanized steel, aluminum, or copper. Downspouts and gutters are to match in materials and finish.
 - b. Chimneys shall have a masonry exterior finish.
 - c. Roof penetrations shall be hidden or painted to match the color of the roof.
 - d. Skylights must be flat to the pitch of the roof and shall not be located on any sloped roof facing the primary frontage.

C. Windows and Doors:

1. **Transparency, Specific to RMX and IC Districts:** Glass on a window of any type (including storefront) or door of any type that has no coatings (such as frosted, tinted or spandrel glass) or treatments, so that the building's interior is visible from the outside. Transparent fenestration shall mean clear or lightly tinted vision glass with a minimum visible light transmittance (VT) of 0.60 that provides direct, unobstructed views into an active interior space at least three feet deep from the interior face of glass. Opaque spandrel panels, reflective glass, back-painted glass, or any glazing with a solid wall, shelving, or storage directly behind it shall not be considered transparent fenestration. Low-E coatings suitable for Beaufort's Lowcountry climate are considered appropriate.
2. Building elevations that face the street shall have at least 40% of the first-floor wall area consist of windows and/or doors, and 15% of upper floors.



D. Massing and Articulation:

1. Building footprints shall not be larger than a single block. Where there is not a defined block structure in the area, see Section 7.2.2 B. for additional standards.
2. Facades that are visible from streets, parks, squares, waterways, significant pedestrian spaces, or primary vehicular access points or parking areas:

- a. Shall not include expanses of blank walls (without any transparent windows or doors) of 20 linear feet or greater.
 - b. Shall be divided into architecturally distinct sections (or bays) with similar proportions. The bays should average no greater than 35 linear feet wide through the use of different architectural divisions or elements such as pilasters, columns, and/or changes in wall depths or floor and roof levels.
 - c. Shall be in harmony with each other in terms of scale, proportion, detailing, material, color, and design.
3. On corner lots, distinct roof or building elements shall be utilized, such as porches, canopies, Arcades, or towers. Buildings may be aligned to create public gathering spaces, while conforming to the building placement and orientation requirements set out in Section 2.5.1 C.

EXAMPLE: CORNER BUILDINGS ELEMENTS AND GATHERING SPACES



E. Architectural Proportions and Details:

1. All buildings shall have a clearly discernible base, body, and cap, with masonry soldier courses or other horizontal expression lines separating each element.

EXAMPLE: BASE, BODY, CAP (GOOD & BAD)



2. Cornices are recommended to delineate the tops of facades. Expression lines are recommended to delineate the divisions between the first floor and upper floors.

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3. **Colors:** Recommended colors are traditional historic colors, earth tones (greens, tans, light browns, terra cotta, grays, pale primary and secondary colors, and white and cream tones. Trademark colors are considered signage and will be counted as such unless they are consistent with the color palette as previously described.
- F. **Additional Guidelines Specific to IC District:** All buildings constructed as part of a campus may be subject to the guidelines for civic buildings as outlined in Section 4.5.13 or shall be held to standards similar to the requirements in the surrounding context.

4.6.5 MANUFACTURED HOME PARK DISTRICT STANDARDS

The following standards shall apply to all development within the Manufactured Home (MH) Park District:

- A. **Site Size:** Parcels shall be a minimum of 5 acres with a minimum width of 150 feet.
- B. **Infrastructure:** The site shall be served by public water and sewer facilities.
- C. **Maximum Density:** 7 manufactured home units per acre.
- D. **Minimum Space Per Unit:** 40 feet in width, and 4,000 square feet of area which shall be clearly defined.
- E. **Minimum Unit Spacing:** 20 feet of clearance between only those MHs parked end to end and any building located within the park.
- F. **Open Space:** Required dedication of open space shall be in accordance with the provisions of 7.4.
- G. **Subdivision:** All lots, regardless of whether they are proposed for sale or leases shall undergo a Subdivision Review process (see Section 9.9).
- H. **Posting:** The "Certificate of Occupancy" (CO) shall be conspicuously posted in the office or on the premises of the MH District.
- I. **Administrative Procedures:**
 1. A proposed Manufactured Home Park must be rezoned to the MH Park District through the amendment process in Section 9.16 (Amendments).
 2. Effect of Approval: If approved by city council, all information pertaining to the proposal shall be adopted as an amendment to this Code as the standards of development for the particular MH Park District.
- J. **Permanent Installation:**
 1. MHs shall be attached to a permanent foundation of brick, concrete, framing, or block that is unpierced, except for required ventilation and access or for flood hazard construction.
 2. Any tongue, axles, running lights, and removable towing apparatus must be removed prior to the issuance of a certificate of occupancy.
- K. **Entrances:**
 1. At the main entrance, there shall be an entryway transition, such as a porch or stoop, that is a minimum of 6 feet by 6 feet.
 2. Stairs, porches, entrance platforms, ramps, or other means of access shall be attached to the structure and anchored securely to the ground.
- L. **Roof Form and Materials:**
 1. Roofs shall have a minimum eave projection and roof overhang of 6 inches, not including the gutter.

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2. Roofs must be covered with a material that is customarily used on site-built dwellings. Aluminum or metal roofing is not permitted unless standing seam metal roofing or metal shingles are used.
- M. **Wall Materials:** Exterior siding shall be wood, vinyl, metal horizontal siding, brick, fiber cement board, stucco, or similar materials. Smooth, ribbed, or corrugated metal or plastic panels, exposed plywood, and materials with a high-gloss finish are not permitted.

4.7: HISTORIC DISTRICT INFILL DESIGN GUIDELINES

4.7.1 APPLICABILITY AND INTENT

The following principles shall be considered by the HRB for new construction within the Historic District overlay. The intent of these principles is to protect the integrity and coherence of the Historic District, and to provide clarity and consistency for developers, designers, and regulators.

4.7.2 PRINCIPLES FOR COMPATIBLE INFILL

- A. **The District is the Resource, Not Only Its Individual Parts:** Beaufort is comprised of a number of individually significant buildings. Additionally, Beaufort's historic areas are significant as a collective whole, and shall be considered as such and protected in their entirety. This is the primary, overarching principle.
1. New construction shall respond to and protect the integrity of the overall Historic District in much the same way as an addition does to a historic building.
 2. The integrity of the district — why, where and when a property is important — shall be upheld in all new construction and rehabilitation projects. Guidelines for determining integrity are as follows:
 - a. **Location:** This is the relationship between the property and its historical context.
 - b. **Design:** This is the combination of elements that create the feeling of a district or structure. These elements include building patterns, streetscapes, site elements, building size, mass and scale, spatial relationships, and specific architectural elements and details.
 - c. **Setting:** This is the physical environment of a property and should be evaluated on its context as well as on the historical role the property has played and continues to play. Important features include topography, vegetation, man-made features, and relationships between existing structures and their surroundings.
 - d. **Materials:** These are the physical elements that make up a property or district.
 - e. **Workmanship:** This is the physical evidence of the crafts of a particular culture or time period. This particularly applies to rehabilitation projects, but for new infill projects, workmanship of surrounding structures should be considered and respected. Retaining the details of the original craft and craftsman (i.e., wood, masonry, tabby etc.) of the original building ensures the historic fabric is retained and serves as an important component of the integrity and the patina of age of individual structures and the district as a whole.
 - f. **Feeling:** This is the property's expression of the aesthetic or historic sense of a particular period of time. This particularly applies to rehabilitation projects, but for new infill projects, the feeling of surrounding structures should be considered and respected.

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- g. **Association:** This is the direct link between an important historic event or person and a property. This particularly applies to rehabilitation projects, but for new infill projects, association of particular sites and neighborhoods should be considered.
- B. **New Construction Shall Reinforce the Historic Significance of the District:** Infill buildings should relate to and strengthen the core characteristics of the district, as identified in the National Register nomination's "Statement of Significance."
1. New construction should build upon the history and established pattern of the district through its design, landscape, use, and cultural expression. An understanding of the character and significance of the district should predicate any design or development activities.
 2. If applicable, cultural expressions and/or historic uses within the district may be considered in design or development activities.
- C. **New Construction Shall Complement and Support the District:** The Historic District has a distinct rhythm of massing, scale, and siting. Infill buildings should not deviate in a detracting manner from these elements but appear as complementary members of the district.
1. Lot size, massing, siting, floor area ratio, and height must correspond to the adjacent buildings that contribute to or complement the rhythm of the district.
 2. The use of buildings will be secondary to their design and integration into the district. However, newly introduced uses should not be detrimental to the historic fabric.
- D. **Infill Shall Be Compatible Yet Distinct:** New buildings should be identifiable as being of their period of construction; however, they should not be so differentiated that they detract from - or visually compete with - their historic neighbors. Within historic districts, compatibility is more important than differentiation.
1. Because the district is the resource, the reconstruction of buildings that existed within the district during the period of significance is allowed. Reconstructions shall be done in accordance with the Secretary of the Interior's "Standards for Reconstruction."
 2. Style is discouraged from being the primary indicator of differentiation.
 3. Means of differentiation may include materials, mechanical systems, and construction methods.
- E. **The Exterior Envelope and Patterning of New Buildings Shall Reflect District Characteristics:** Infill design elements, patterning, texture, and materials should reflect the aesthetic and historic themes of the district.
1. Patterns of fenestration, building divisions, setbacks, and landscapes that are characteristic of the district should inform the design of new buildings.
 2. Mechanical and automobile infrastructure should be appropriately concealed when not consistent with the district's character.
- F. **Contributing Buildings Should Not Be Demolished to Create Infill Opportunities:** Properties deemed contributing in the "1997 Beaufort County Above Ground Historic Sites Survey," or in the most recent historic resources survey, should not be removed or rendered non-contributing to make way for new construction.
- G. **Archeological Resources Shall Be Preserved in Place or Mitigated:** When new construction disturbs or affects archaeological resources, mitigation measures should be taken such that the history of the site can be traced. See Section 8.3 for archeological assessment requirements.
1. Archeological mitigation must conform to local, state, and federal laws and accepted professional standards.

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2. When appropriate, archaeological mitigation should be accessible to the general public in an educational capacity.
 3. Information yielded from archaeological mitigation should be interpreted in the new building and throughout the district.